

# SCOTTSDALE AIRPORT ADVISORY COMMISSION MEETING NOTICE AND AGENDA



Wednesday, February 17, 2021  
5:00 p.m.  
**Meeting will be held electronically and  
remotely**



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## AIRPORT ADVISORY COMMISSION

John Berry, Chair

Charles McDermott Vice-Chair

Larry Bernosky

Liz Kaplan

Cory Little

Peter Mier

Rick Milburn

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**Until further notice, Airport Advisory Commission meetings are being held electronically. While physical facilities are not open to the public, Airport Advisory Commission meetings are available on Scottsdale's YouTube channel to allow the public to virtually attend and listen/view the meeting in progress.**

1. Go to [ScottsdaleAZ.gov](http://ScottsdaleAZ.gov), search "live stream"
2. Click on "Scottsdale YouTube Channel"
3. Scroll to "Upcoming live streams"
4. Select the applicable meeting

**Spoken comment is being accepted on agenda action items. To sign up to speak on these please [click here](#). Request to speak forms must be submitted no later than 90 minutes before the start of the meeting.**

**Written comment is being accepted for both agendized and non-agendized items, and should be submitted electronically no later than 90 minutes before the start of the meeting. To submit a written public comment electronically, please [click here](#)**

**Call to Order**

**Roll Call**

**Aviation Director's Report**

The public body may not propose, discuss, deliberate or take legal action on any matter in the summary unless the specific matter is properly noticed for legal action.

## Approval of Minutes

Regular Meeting: January 20, 2021

### REGULAR AGENDA

### ITEMS 1-11

**How the Regular Agenda Works:** The Commission takes a separate action on each item on the Regular Agenda


1. Discussion and Possible Action to Recommend Adoption of Resolution No. 12075, Authorizing Lease Agreement with 3R Ranch, Inc. (2021-022-COS) for the lease of the North General Aviation Executive Box Hangar Space at the Scottsdale Airport. Staff contact: Carmen Williams, Aviation Finance & Administration Manager, 480-312-8475, [cawilliams@scottsdaleaz.gov](mailto:cawilliams@scottsdaleaz.gov)
2. Discussion and possible action regarding 1-GP-2021: Draft Scottsdale General Plan 2035, Staff contacts: Adam Yaron, Project Coordination Liaison, [ayaron@scottsdaleaz.gov](mailto:ayaron@scottsdaleaz.gov) and Taylor Reynolds, Project Coordination Liaison, [treynolds@scottsdaleaz.gov](mailto:treynolds@scottsdaleaz.gov)
3. Discussion and Possible Action to approve the Five-Year Capital Improvement Program for Fiscal Years 21/22 through FY25/26. Staff contact: Carmen Williams, Aviation Finance & Administration Manager, 480-312-8475, [cawilliams@scottsdaleaz.gov](mailto:cawilliams@scottsdaleaz.gov)
4. Discussion and Possible Action to modify the Airport Rules and Regulations. Article 1 Definitions, Article 2 Section 2.6. Aircraft Parking, Section 2.9 Aircraft shades and tiedowns, Section 2.15. Aviation Business Center and Airpark Rules and Regulations, Section 301, Landing and takeoff of helicopters.  
Staff contact: Gary P. Mascaro, Aviation Director, 480-312-7735, [gmascaro@scottsdaleaz.gov](mailto:gmascaro@scottsdaleaz.gov)
5. Discussion and input regarding Airport and Airpark Aeronautical Business Permit Additions, Cancellations and Revocations. Staff contact: Carmen Williams, Aviation Finance & Administration Manager, 480-312-8475, [cawilliams@scottsdaleaz.gov](mailto:cawilliams@scottsdaleaz.gov)
6. Discussion and input regarding Financial Report for December 2020. Staff contact: Carmen Williams, Aviation Finance & Administration Manager, 480-312-8475, [cawilliams@scottsdaleaz.gov](mailto:cawilliams@scottsdaleaz.gov)
7. Discussion and input regarding the Monthly Construction Report  
Staff contact: Chris Read, Assistant Aviation Director-Operations, 480-312-2674, [cread@scottsdaleaz.gov](mailto:cread@scottsdaleaz.gov)

8. Discussion and input regarding Monthly Operations Report  
Staff contact: Chris Read, Assistant Aviation Director-Operations, 480-312-2674, [cread@scottsdaleaz.gov](mailto:cread@scottsdaleaz.gov)
9. Discussion and input regarding Public Outreach Programs and Planning Projects  
Staff contact: Sarah Ferrara, Aviation Planning & Outreach Coordinator, 480-312-8482, [sferrara@scottsdaleaz.gov](mailto:sferrara@scottsdaleaz.gov)
10. Administrative report from the Aviation Director, or designee, regarding the status of pending aviation-related items. Staff contact: Gary P. Mascaro, Aviation Director, 480-312-7735, [gmascaro@scottsdaleaz.gov](mailto:gmascaro@scottsdaleaz.gov)
11. Discussion and possible action to modify the Airport Advisory Commission Meeting Schedule and Commission Item Calendar. Staff contact: Gary P. Mascaro, Aviation Director, 480-312-7735, [gmascaro@scottsdaleaz.gov](mailto:gmascaro@scottsdaleaz.gov)

### **Future Agenda Items**

Discussion and possible action to add Commissioner requested item on a future agenda.

### **Adjournment**

 Persons with a disability may request a reasonable accommodation by contacting Airport Administration (480-312-2321). Requests should be made 24 hours in advance or as early as possible to allow time to arrange accommodation. For TTY Users, the Arizona Relay Service (1-800-367-8939) may contact the Aviation Department (480-312-2321)



**COMMISSION INFORMATION REPORT**  
**APPROVAL OF MINUTES**

**Meeting Date:** 02/17/21

**Contact:** Gary P. Mascaro,  
Aviation Director

**Phone:** (480) 312-7735

**ACTION**

Approval of Minutes – Regular Meeting  
January 20, 2021

Attachment(s): 1. Draft of minutes of the January 20, 2021 Regular Meeting

Action taken:



**SCOTTSDALE AIRPORT ADVISORY COMMISSION  
PUBLIC MEETING**

**Scottsdale Airport Aviation Business Center  
Stearman/Thunderbird Meeting Room  
Second floor 15000 N. Airport Drive  
Scottsdale, Arizona**

**MEETING HELD ELECTRONICALLY  
Wednesday, January 20, 2021**

**DRAFT MINUTES**

**PRESENT:** Larry Bernosky, Chair  
John Berry, Vice Chair  
Liz Kaplan  
Charles McDermott  
Peter Mier  
Rick Milburn

**ABSENT:** Cory Little

**STAFF:** Gary Mascaro, Aviation Director  
Sarah Ferrara, Aviation Planning & Outreach Coordinator  
Carmen Williams, Aviation Finance & Administration Manager  
Tiffany Domingo, Staff Coordinator

**GUESTS:** Ryan Hamilton, Beck Lane Hangars, LLC  
Jessica Naor, GrandView Aviation, LLC  
Chad Verdaglio, Sawyer MX, LLC

**CALL TO ORDER**

Chair Bernosky called the meeting to order at 5:00 p.m.

**ROLL CALL**

A formal roll call confirmed the presence of Commissioners as noted above.

Reporter's Note: Due to poor audio quality, not all comments were able to be captured.

## **AVIATION DIRECTOR'S REPORT**

Gary Mascaro, Aviation Director, announced the official opening of Scottsdale Airport's third FBO, Jet Aviation, as well as the opening of Phase 1 of the Airport's north executive box hangar development. Phase 2, consisting of an additional seven executive box hangars, is currently in development to be completed in the fall. He thanked Chair Bernosky and Vice Chair Berry for their service on the Commission.

1. Regular Meeting: November 18, 2020

Chair Bernosky called for approval of the minutes.

COMMISSIONER MIER MADE A MOTION TO APPROVE THE REGULAR MINUTES OF NOVEMBER 18, 2020 AS PRESENTED. VICE CHAIR BERRY SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERNOSKY, VICE CHAIR BERRY, COMMISSIONERS KAPLAN, MCDERMOTT, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.

## **PUBLIC COMMENTS**

No comments were submitted.

## **REGULAR AGENDA            ITEMS 1-17**

1. Election of Officers

Mr. Mascaro reviewed the nomination process.

CHAIR BERNOSKY MADE A MOTION TO NOMINATE VICE CHAIR BERRY AS CHAIR. COMMISSIONER MILBURN SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERNOSKY, VICE CHAIR BERRY, COMMISSIONERS KAPLAN MCDERMOTT, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.

CHAIR BERNOSKY MADE A MOTION TO NOMINATE CHARLES MCDERMOTT AS VICE CHAIR. VICE CHAIR BERRY SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERNOSKY, VICE CHAIR BERRY, COMMISSIONERS KAPLAN, MCDERMOTT, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.

Upon the votes, the newly elected officers immediately assumed their roles.

2. Discussion and Possible Action Regarding Airport Advisory Commission By-Laws

Mr. Mascaro stated that the By-Laws are presented on an annual basis in anticipation of any changes or modifications desired by the Commission. No changes were recommended by Commission members.

3. Discussion and Possible Action Regarding Application for Airpark Aeronautical Business Permit for Beck Lane Hangars, LLC to Conduct Hanger/Shade Leading Services at the Scottsdale Airpark.

Carmen Williams, Aviation Finance & Administration Manager, noted that the applicant's business is located on Beck Lane with aircraft access through Gate 6. They have over 40,000 square feet of hangar office space and common area space. They have met the requirements of the permit and have submitted the proper documentation. Mr. Ryan Hamilton was in attendance to answer any questions.

VICE CHAIR MCDERMOTT MADE A MOTION TO APPROVE THE APPLICATION FOR AN AERONAUTICAL BUSINESS PERMIT FOR BECK LANE HANGARS, LLC, TO CONDUCT HANGAR/SHADE LEASING SERVICES AT THE SCOTTSDALE AIRPARK. COMMISSIONER KAPLAN SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERRY, VICE CHAIR MCDERMOTT, COMMISSIONERS BERNOSKY, KAPLAN, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.

4. Discussion and Possible Action Regarding Application for Airport Aeronautical Business Permit for GrandView Aviation, LLC, to Conduct Aircraft Charter Services at the Scottsdale Airport

Ms. Williams stated that GrandView's main offices are in Baltimore, Maryland. They have leased office space at Signature Flight Support's main facility and are basing an Embraer 505 Phenom jet at Scottsdale for their charter operations. They have met the requirements of the permit and have submitted the proper documentation. Ms. Jessica Naor, Chief Operating Officer, was in attendance to answer any questions.

COMMISSIONER MIER MADE A MOTION TO APPROVE THE APPLICATION FOR AN AERONAUTICAL BUSINESS PERMIT FOR GRANDVIEW AVIATION, LLC, TO CONDUCT AIRCRAFT CHARTER SERVICES AT THE SCOTTSDALE AIRPORT. COMMISSIONER MILBURN SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERRY, VICE CHAIR MCDERMOTT, COMMISSIONERS BERNOSKY, KAPLAN, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.

5. Discussion and Possible Action Regarding Application for Airport Aeronautical Business Permit for Sawyer MX, LLC., to Conduct Aircraft Maintenance and Repair Services at the Scottsdale Airport

Ms. Williams stated that Sawyer is a long-term tenant of the Airport, with flight school and charter operations under Sawyer Aviation. They have recently expanded their business to add aircraft maintenance services under the separate entity. They have office and hangar space at both the Ross Aviation and Jet Aviation facilities. They have met the requirements of the permit and submitted the proper documentation.

In response to a question from Chair Berry, Chad Verdaglio, President of Sawyer MX, LLC, stated that they will be servicing jet aircraft as well as piston.

In response to a question from Commissioner Milburn, Mr. Verdaglio said the maintenance shop is a stand-alone facility. They will also have 8,000 of floor space in Hangar 6 with Ross.

COMMISSIONER MILBURN MADE A MOTION TO APPROVE THE APPLICATION FOR AN AERONAUTICAL BUSINESS PERMIT FOR SAWYER MX, LLC., TO CONDUCT AIRCRAFT MAINTENANCE AND REPAIR SERVICES AT THE SCOTTSDALE AIRPORT. COMMISSIONER BERNOSKY SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERRY, VICE CHAIR MCDERMOTT, COMMISSIONERS BERNOSKY, KAPLAN, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.

6. Discussion and Possible Action to Approve Airport Advisory Commission Annual Report

Mr. Mascaro stated that all Board and Commission annual reports are forwarded to City Council in the month of February. He provided a brief overview of the Airport Commission's 2020 report. He reported that he received notice today from the FAA that the federal government has released grant funding to move forward with the Runway Rehab Project. Commission members are welcome to add any additional comments to the report to share with City Council. There were no requests for modifications.

VICE CHAIR MCDERMOTT MADE A MOTION TO APPROVE THE AIRPORT ADVISORY COMMISSION ANNUAL REPORT AS PRESENTED. COMMISSIONER MIER SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERRY, VICE CHAIR MCDERMOTT, COMMISSIONERS BERNOSKY, KAPLAN, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.

7. Discussion and Possible Action to Recommend Adoption of Resolution No. 12038, Authorizing Lease Agreement with Pantheon Global Realty for the Lease of North General Aviation Executive Box Hangar Space at the Scottsdale Airport

Ms. Williams stated that the first set of seven box hangars under Phase 1 is open. Two Phase 1 units opened in November, 2020. After a mutual lease termination, one vacant unit was leased to Ross Aviation. Pantheon Global Realty would like to lease the second unit. The lease will also require City Council approval at its February 2, 2021 meeting. The lease is for Unit A-5 for a term of 36 months. In response to a Commissioner question, Ms. Williams stated that the pricing was negotiated between the Airport's aviation real estate consultant and the tenant.

VICE CHAIR MCDERMOTT MADE A MOTION TO RECOMMEND ADOPTION OF RESOLUTION NO. 12038, AUTHORIZING LEASE AGREEMENT WITH PANTHEON GLOBAL REALTY FOR THE LEASE OF NORTH GENERAL AVIATION EXECUTIVE BOX HANGAR SPACE AT THE SCOTTSDALE AIRPORT. COMMISSIONER KAPLAN SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERRY, VICE CHAIR MCDERMOTT, COMMISSIONERS BERNOSKY, KAPLAN, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.



8. Discussion and Possible Action to Recommend Adoption of Resolution No. 12044, Authorizing Contract No. 2021-015-COS with DMD Real Estate Group, LLC., for Aviation Real Estate Brokerage Consulting Services

Ms. Williams stated that the current contract with DMD Real Estate Group ended last December. Over the years, the Airport has benefited from the real estate services provided by DMD Real Estate Group. The Aviation Department and City Purchasing reissued the request for proposals for brokerage services on October 15th and the proposals were due November 10th. The range of services include property analysis, market analysis, leasing services and consulting services. Two proposals were received and reviewed by an evaluation committee. The recommendation is to award the contract to the DMD Real Estate Group. The contract will go to City Council for approval in February.

Commissioner Milburn asked about the number of years under contract with DMD. Ms. Williams stated that the first five-year contract ended in December. The current contract is a one-year term with four one-year renewable extensions.

Commissioner Milburn asked about the extent of use of DMD's consulting services in the past. Ms. Williams stated that hourly consultant rates have been minimal. For the north GA Box Hangars, there was a lump sum to include time for the analysis. On occasion staff will consult with them on advice for a contract. The majority of services will most likely be commissions for finding tenants and for marketing.

Chair Berry recused himself as he had sat on the evaluation committee.

VICE CHAIR MCDERMOTT MADE A MOTION TO RECOMMEND ADOPTION OF RESOLUTION NO. 12044, AUTHORIZING CONTRACT NO. 2021-015-COS WITH DMD REAL ESTATE GROUP, LLC FOR AVIATION REAL ESTATE BROKERAGE CONSULTING SERVICES. COMMISSIONER BERNOSKY SECONDED THE MOTION, WHICH CARRIED 5/0 WITH VICE CHAIR MCDERMOTT, COMMISSIONERS BERNOSKY, KAPLAN, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES. CHAIR BERRY WAS RECUSED.

9. Discussion and Possible Action to Recommend that City Council Adopt Resolution 12045 Authorizing On-Call Engineering Services Contract Amendment 2018-055-COS-A1 with Mead & Hunt, Inc., Increasing the Annual Contract Expenditure Limit from \$1,000,000 to \$1,500,000 for Design and Construction Administration of Various Aviation Department Projects

Chris Read, Assistant Aviation Director – Operations, stated that Mead & Hunt provides the Airport with construction and administration services for large airfield projects. With larger projects, such as the box hangars and runway project, there will be a need to exceed the \$1 million limit.

Commissioner Milburn asked whether the contract would revert to the \$1 million limit at the end of the year and upon contract renewal. Mr. Read clarified that the amendment would continue through the end of the five-year contract.

In response to a question from Chair Berry, Mr. Read confirmed that a contract amendment would have to be executed in order for the contract to go above \$1.5 million. However, this is not expected to be necessary.

VICE CHAIR MCDERMOTT MADE A MOTION TO RECOMMEND ADOPTION OF RESOLUTION NO. 12045, AUTHORIZING CONTRACT NO. 2021-055-COS-A1 WITH MEAD & HUNT, INC., INCREASING THE ANNUAL CONTRACT FROM \$1,000,000 TO \$1,500,000 FOR DESIGN AND CONSTRUCTION ADMINISTRATION OF VARIOUS AVIATION DEPARTMENT PROJECTS. COMMISSIONER MIER SECONDED THE MOTION, WHICH CARRIED 6/0 WITH CHAIR BERRY, VICE CHAIR MCDERMOTT, COMMISSIONERS BERNOSKY, KAPLAN, MIER AND MILBURN VOTING IN THE AFFIRMATIVE WITH NO DISSENTING VOTES.

10. Discussion and Input Regarding the monthly Construction Report

Mr. Read reported that the first phase of the box hangars is complete. Phase 2 is just beginning, with pavement removal, underground utilities and building footers in progress. The Runway Rehabilitation project is in the preconstruction phase with this portion to be completed in February or March 2021. Construction is scheduled to commence in July, 2021.

In response to a question from Chair Berry, Mr. Read stated that the estimated time for completion of construction is 45 days.

11. Discussion Input Regarding Monthly Operations Report for November and December, 2020

Mr. Read stated that the based aircraft numbers for the period (195,852) surpassed the totals for the same period last year (186,514). IFRs were also 1.8 percent higher than the same period last year.

There were two Alert 2s for the time period. The most significant enforcement action for the period resulted in the issuance of a civil citation to a pedestrian, who entered the movement area without authorization. U.S. Customs revenue totaled \$93,900 with 168 uses and 112 U.S. visit flights. Fiscal year-to-date revenues surpassed the totals for the same period last year, as did total uses. There were 42 PPRs for Calendar Year 2020.

In response to a question from Commissioner Mier, Mr. Read stated that all buildings on the Airport perimeter are required to maintain a security plan. One gate was found to be not locking or closing and this was identified as a security plan violation.

12. Discussion and Input Regarding Airport and Airpark Aeronautical Business Permit Additions, Cancellations and Revocations

Ms. Williams noted that the three new permits from this agenda are included on the list. Jet Aviation's AVP was changed to a fixed-based operator, as they opened up as an FBO earlier in the month. Southern Sky Aviation Charters cancelled its charter permit. Mobile Inn cancelled its hangar shade AVP. November fuel revenues are up significantly over last year. For the November comparison, the jet gallons sold by FBOs are up 31 percent. AVGAS is down 1.5

percent. Airpark fuel is down 10 percent. Overall for November, gallons are up 17.1 percent. For fiscal year-to-date, jet fuel sales are up 25.3 percent for FBOs. AVGAS is up 3.3 percent and Airpark is down 3.3 percent. Actual gallons pumped year to date total 4.28 million, representing a 14 percent increase over last year. Customs activity continues to increase as the winter season progresses.

13. Discussion and Input Regarding Financial Report for November, 2020

Ms. Williams stated that the approved budget for revenues through November, 2020 was \$1.9 million. Actuals came in at \$2.2 million. The expense budget was \$1.1 million and actuals came in at \$906,000. Total revenues were \$491,842, compared with \$462,514 last year. Expenses were \$132,868 compared with \$301,912 last year. The Aviation Fund cash balance is \$6.4 million as of November 30th.

14. Discussion and Input Regarding Public Outreach Programs and Planning Projects

Ms. Ferrara reported on reviewing the Airport noise abatement program with new flight training company, Climb 250. Five voluntary curfew letters were sent out in November and nine in December. The Planning and Zoning reports listed one project within the Airport Influence Area in November and six in December. A listserv was sent out on the Jet Aviation opening and completion of Phase 1 of the box hangar project. A press release was issued on the city construction projects. There was coverage in local news and via Business Airport International. Fox 10 requested information on Airport operations and impacts due to the pandemic. Staff is in the process of updating collateral material to include the introduction of new FBO Jet Aviation. The Runway Rehab webpage is updated on a regular basis.

15. Discussion and Input Regarding Quarterly Noise Complaint Summary Report

Ms. Ferrara stated that there were 19 callbacks and one email response by staff. There were 24 new complaints and 33 repeat complaints, which is comparable to the same period last year. There were 10,074 complaints for 2020, due in large part to a single complainant submitting a total of 9,845 complaints. Removing the complaints from this single individual, the total complaints for the quarter was 229.

In response to a question from Commissioner Mier, Ms. Ferrara stated that staff has attempted to have a dialogue with the main complainant, however, the submissions have continued.

16. Administrative Report from the Aviation Director or Designee Regarding the Statute of Pending Aviation-Related Items

Mr. Mascaro reported that the amendments to the lease agreements were approved by City Council on consent on December 1st. The three items the Commission just voted to approve will go on consent for the February 2nd City Council meeting. There were no updates to the report on aviation-related items to the Planning Commission, Design Review Board or City Council.

17. Discussion and Possible Action to Modify the Airport Advisory Commission Meeting Schedule and Commission Item Calendar

There were no changes to the calendar.

**ADJOURNMENT**

With no further business to discuss, being duly moved by Commissioner Mier and seconded by Vice Chair McDermott, the meeting adjourned at 6:01 p.m.

AYES: Chair Berry, Vice Chair McDermott, Commissioners Bernosky, Kaplan, Mier and Milburn

NAYS: None

SUBMITTED BY:

eScribers, LLC



## COMMISSION ACTION REPORT

Discussion and Possible Action to Recommend Adoption of Resolution No. 12075, Authorizing a Lease Agreement with 3R Ranch, Inc. for the lease of North General Aviation Executive Box Hangar Space at the Scottsdale Airport

**Agenda Item No.:** 1

**Meeting Date:** 02/17/21

**Staff Contact:** Carmen Williams,  
Aviation Finance & Administration  
Manager

**Phone:** (480) 312-8475

### ACTION

Discussion and possible action to recommend adoption of Resolution No. 12075, authorizing a lease agreement with 3R Ranch, Inc. (2021-022-COS) and for the lease of the north general aviation executive box hangar space at the Scottsdale Airport.

### PURPOSE

To authorize a new lease agreement for north general aviation executive box hangar space at the Scottsdale Airport.

### KEY CONSIDERATIONS

- The General Aviation Box Hangars are located on the Kilo Ramp at the north end of the Airport property. Building A (Phase I) and Building B (Phase II) each consist of seven contiguous executive box hangars, each dimensioned at 62 feet wide by 47 feet deep.
- Construction of Phase I is completed. Construction of Phase II units are underway with an anticipated construction time of ten months.
- 3R Ranch, Inc. is executing a new lease agreement for unit A-6; this lease agreement will replace lease agreement 2020-170-COS that has recently been terminated per the request of the previous Lessee.
- The base rent for all fourteen (14) box hangar leases will generate approximately \$464,400 in annual revenues to the Airport Enterprise Fund.
- Aviation Enterprise Funds under CIP Project Center AF01 (Design/Construct North General Aviation Box Hangars) will cover the costs of the development.

Attachments: 1. Resolution No. 12075  
2. Summary of Box Hangar Leases  
3. Location Map of General Aviation Box Hangars  
4. Contract No. 2021-022-COS

Action taken:

RESOLUTION NO. 12075

A RESOLUTION OF THE COUNCIL OF THE CITY OF SCOTTSDALE, MARICOPA COUNTY, ARIZONA AUTHORIZING LEASE AGREEMENT 2021-022-COS WITH 3R RANCH, INC., FOR THE LEASE OF HANGAR SPACE AT THE SCOTTSDALE AIRPORT.

WHEREAS, the City is the owner of certain real property known as the Scottsdale Airport on which the City is constructing the North General Aviation Box Hangars; and

WHEREAS, the City desires to lease box hangar space to 3R Ranch Inc. pursuant to the terms set forth in the lease agreement;

NOW, THEREFORE, be it resolved by the Council of the City of Scottsdale as follows:

Section 1: The Mayor is authorized and directed to execute on behalf of the City of Scottsdale, lease agreement 2021-022-COS with 3R Ranch, Inc., a Wyoming corporation, for the lease of hangar space at the Scottsdale Airport.

PASSED AND ADOPTED by the Council of the City of Scottsdale this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

CITY OF SCOTTSDALE, an Arizona  
municipal Corporation

\_\_\_\_\_  
David D. Ortega, Mayor

ATTEST:

By: \_\_\_\_\_  
Carolyn Jagger, City Clerk

APPROVED AS TO FORM:

OFFICE OF THE CITY ATTORNEY

\_\_\_\_\_  
Sherry R Scott, City Attorney  
By: Eric C. Anderson, Senior Assistant City Attorney

**General Aviation - Box Hangar Leases**

Updated: 2/1/2021

	Name	Company	Contract No.	Hangar Size	Unit Assigned	Lease Term	Mo. Rate	Notes
1	James Van Horn	Van Horn Aviation, LLC	2019-043-COS	62' x 47'	A-1	60 month	\$ 2,600.00	mezzanine floor
2	Rick Wielebski	Ross Aviation - M06479	2019-042-COS	62' x 47'	A-2	24 month	\$ 2,600.00	mezzanine floor
3	Rick Wielebski	Ross Aviation	2019-042-COS	62' x 47'	A-3	24 month	\$ 2,600.00	mezzanine floor
4	Rick Wielebski	Ross Aviation	2019-042-COS	62' x 47'	A-4	24 month	\$ 2,600.00	mezzanine floor; adding unit for amendment/Res 12002
5	Brandon Maldonado	Pantheon Global Realty	2021-009-COS	62' x 47'	A-5	36 month	\$ 2,900.00	approved Feb 2021/Res 12038
6	Laurence Palestrant	3R Ranch, Inc.	Pending	62' x 47'	A-6	36 month	\$ 2,900.00	2021-022-COS; replacement for S. Armstrong lease
7	Bob Brown	Brown Community Management - MC	2019-053-COS	62' x 47'	A-7	24 month	\$ 2,200.00	no mezzanine floor; moved from A-5 to A-7/Res 12002

*Clayton Wolfe (former A-7), Stephen Darcy (former A-4) and Sean Armstrong (former A-6) agreements have been terminated*

	Name	Company	Contract No.	Hangar Size	Unit Assigned	Lease Term	Mo. Rate	Notes
8	David Best	Jet Aviation of America, Inc.	2020-169-COS	62' x 47'	B-1	72 months	\$ 2,900.00	no mezzanine floor
9	David Best	Jet Aviation of America, Inc.		62' x 47'	B-2	72 months	\$ 2,900.00	no mezzanine floor
10	David Best	Jet Aviation of America, Inc.		62' x 47'	B-3	72 months	\$ 2,900.00	no mezzanine floor
11	David Best	Jet Aviation of America, Inc.		62' x 47'	B-4	72 months	\$ 2,900.00	no mezzanine floor
12	David Best	Jet Aviation of America, Inc.		62' x 47'	B-5	72 months	\$ 2,900.00	no mezzanine floor
13	David Best	Jet Aviation of America, Inc.		62' x 47'	B-6	72 months	\$ 2,900.00	no mezzanine floor
14	David Best	Jet Aviation of America, Inc.		62' x 47'	B-7	72 months	\$ 2,900.00	no mezzanine floor

\$ 38,700.00 monthly revenues  
 \$ 464,400.00 annual revenues





**HANGAR LEASE AGREEMENT**

THIS LEASE AGREEMENT is made and entered into this 1<sup>st</sup> day of April 2021, by and between the CITY OF SCOTTSDALE, an Arizona municipal corporation ("Lessor"), and 3R Ranch, Inc., a Wyoming corporation ("Lessee").

**WITNESSETH**

A. Lessor is the owner of certain real property at the Scottsdale Airport (the "Airport") on which it is constructing the North General Aviation Box Hangars (the "Box Hangars"). The Box Hangars will be located airside on the Kilo Ramp north of the wash rack.

B. Lessor desires to lease to Lessee, and Lessee desires to lease Building A unit A-6, a large 62' x 47' box hangar (the "Leased Premises") generally depicted on Exhibit "A" consisting of approximately 2914 square feet of hangar space solely for the storage of the aircraft identified in the approved Scottsdale Airport Aircraft Storage Permit application (or any replacement aircraft as approved in writing by the Aviation Director).

NOW, THEREFORE, for and in consideration of the foregoing, the rent hereinafter to be paid by Lessee, and the covenants and agreements contained herein to be kept and performed by Lessee, and other good and valuable consideration, Lessor and Lessee agree as follows:

**I. RECITALS**

1. Recitals. The foregoing recitals are incorporated into this Agreement.

**II. PREMISES**

2. Premises and Fixtures.

2.1 Agreement to Lease. Lessor hereby agrees to lease the Leased Premises to Lessee and Lessee hereby agrees to lease the Leased Premises from Lessor, subject to the terms and conditions of this Agreement.

2.1.1 Lessor's Fixtures Included. The Leased Premises also includes all fixtures attached to the Leased Premises, which, once attached, are owned by the Lessor.

### III. TERM OF LEASE

3. Term of Lease. This Agreement is effective as soon as it is entered into and is subject to the following provisions and conditioned upon Lessee's full, timely, complete and faithful performance of all obligations and things to be performed or done hereunder by Lessee. Lessee shall accept the Leased Premises in writing at the time possession of the Leased Premises is delivered to Lessee, the date of which will mark the start of the term of the Leased Premises. The term of the Leased Premises shall be for a period of 36 months from the start of the time possession is delivered to the Lessee.

3.1 Holding Over. In any circumstance whereby Lessee would hold over and remain in possession of the Leased Premises after the expiration of this Agreement, such holding over shall not be considered to operate as a renewal or extension of this Agreement, but shall only create a tenancy from month to month which may be terminated at any time by Lessor upon 10 days' notice to Lessee. During any such hold over period, Base Rent shall increase to 150% of its previous rate and shall be prorated to the date Lessee vacates the Leased Premises.

3.2 Option to Renew Lease. Provided that Lessee is not, nor has been, in default of any of the terms and conditions of the lease agreement, Lessee shall have the option to extend the term of the Lease for two (2) additional periods of one (1) year at a mutually agreed upon rate. Lessee shall notify, in writing, no later than three (3) months prior to the expiration of the Lease term of its intent to exercise said Option. Failure to exercise said Option in strict compliance with the manner provided, shall automatically terminate Lessee's right to exercise said Option. Notwithstanding the provisions of the Lease, the Option granted shall inure only to the benefit of the named herein and shall not be exercisable by Lessee or during the tenancy of any assignee, sub-tenant, mortgagee, or other transferees of said Lessee. The Aviation Director shall have authority to consent to an extension as set forth above without necessity of further City Council approval.

### IV. LEASE PAYMENTS

4. Lease Payments. Lessee shall pay to Lessor all of the following payments together with all other payments required by this Agreement (all payments by Lessee to Lessor required by this Agreement for any reason are collectively the "Rent"):

4.1 Rent Payment Date. All Rent shall be payable in advance on the 25th day of the preceding calendar month. For example, the Rent for September shall be payable on or before August 25th. The first installment of Rent prorated for the portion of the month remaining in the month in which the term of the Leased Premises will begin is due at least five days prior to the estimated delivery of possession of the Leased Premises as determined by the Aviation Director.

4.2 Rent. The rental amount (the "Rent") Lessee shall pay to Lessor for each of the first 12 months from the commencement of the term of this Agreement is **\$2,900.00**. The Rent is due and payable each month during the term hereof.

4.3 Rent Adjustment. The Rent shall be automatically adjusted upward on the one-year anniversary of the commencement of the term of this Agreement and every year thereafter on the basis of changes in the United States Consumer Price Index for all Urban Consumers (CPI-U), U.S. City Average published by the United States Bureau of Labor Statistics as of the date two (2) months prior to the adjustment date (the "Cost of Living Index"). The amount of each adjusted monthly rent installment of Rent (represented by the letter "R" in the formula set forth below) shall be equal to the then current Cost of Living Index number (represented by the letter "C" in the formula set forth below) divided by the Cost of Living Index number for the month during which the term of this Lease commences (represented by the letter "M" in the formula set forth below), and multiplied by the original monthly Rent amount (represented by the "\$" symbol in the formula set forth below). This computation is expressed by the following formula:

$$R = \frac{C}{M} \times \$$$

provided, that in no event shall the Rent be adjusted downward from any previous period. If such Cost of Living Index shall no longer be published at the adjustment date, then another similar index published by any federal agency shall be substituted by Lessor in Lessor's reasonable discretion. In the event of a holdover without Lessor's consent, Rent shall be increased by an additional 150% over the amount of Rent otherwise payable.

4.4 Security Deposit. At the time of execution of this Agreement, Lessee shall provide to, and maintain with Lessor at all times during the term of this Agreement, a security deposit guaranteeing the faithful performance of this Agreement in the name of Lessor, in the sum of **\$2,900.00**. Any portion of said security deposit to which Lessee may then be entitled, net of any setoff or other obligation of Lessee to Lessor, shall be paid to Lessee by the Lessor within 60 days after termination of this Agreement.

4.5 Taxes, Liens and Assessments. In addition to all other Rent herein provided, Lessee shall pay, when due and as the same become due and payable, all taxes and general and special fees, charges, and assessments of every description which during the term of this Agreement may be levied upon or assessed against the Leased Premises, the operations conducted therein, any Rent paid or other performances under this Agreement by either party, and all possessory interest in the Leased Premises and improvements and other property thereon, whether belonging to the Lessor or Lessee. Lessee agrees to indemnify, defend, and hold harmless Lessor and the Leased Premises and such property and all interest therein and improvements thereon from any and all such taxes and assessments, including any interest, penalties and other expenses that may be imposed, and from any lien therefor or sale or other proceedings to enforce payment thereof. Lessee shall have the right to contest, but not the right to refuse to timely pay, any taxes and assessments. Lessor shall have the right from time to time to require that all of the foregoing payments be made by Lessee through Lessor. Lessee shall pay all sales, transaction privilege, and similar taxes.

4.6 Late Fees. Should any installment of Rent not be paid on or before the date due, a 10% late fee shall be added to the amount due or found to be due. Furthermore, any and all amounts payable by Lessee under this Agreement that are not timely paid shall accrue interest at

the rate of 1% per month from the date the amount first came due until paid. Lessee expressly agrees that the foregoing represents a reasonable estimate of Lessor's costs in the event of a delay in payment of Rent.

4.7 Rent Amounts Cumulative. All amounts payable by Lessee hereunder or under any tax, assessment or other existing or future ordinance or other law of the City of Scottsdale or the State of Arizona shall be cumulative and payable in addition to each other payment required hereunder, and such amounts shall not be credited toward, substituted for, or setoff against each other in any manner.

4.8 No Setoffs. All Rents shall be paid in full directly to Lessor without setoff or deduction of any description. Lessee expressly waives any right of setoff.

4.9 Utilities. Lessor will pay all charges, fees, deposits and other amounts for sewer and waste disposal services at the applicable rates as determined by Lessor. Lessee will pay all electricity and water service charges and fees for the Leased Premises.

4.10 Maintenance by Lessor. Lessor will maintain the structural integrity of the box hangar units comprising the Leased Premises (including without limitation doors, roof and exterior walls). Lessor is not responsible for maintenance of a routine or minor nature or of Lessee's furnishings, fixtures or improvements.

4.11 Maintenance by Lessee. Lessee is responsible for the following:

4.11.1 Janitorial and all other cleaning service in the Leased Premises.

4.11.2 Adequate and sanitary handling and disposal of all trash, garbage and other refuse related to Lessee's use of the Leased Premises.

4.11.3 All other repairs and maintenance of the Leased Premises not specifically required to be performed by Lessor, except that if it is determined by the Lessor that the failure of any of the systems described in this section is due to the negligence of the Lessee, the Lessee will be responsible for the costs of any such repairs.

#### V. USE RESTRICTIONS AND COMPLIANCE WITH ALL LAWS

5. Use Restrictions. Lessee's use and occupation of the Leased Premises shall in all respects conform to all and each of the following cumulative provisions:

5.1 Permitted Uses. Lessee will use the Leased Premises for aircraft storage only, with the exception that the Lessor may give written consent to allow other accessory and related to aviation storage uses from time to time. Such accessory and related to aviation storage uses may only be conducted following the Aviation Director giving to Lessee written notice of consent, which will not be unreasonably withheld, and Lessor through the Aviation Director may impose conditions and limitations on such consent and the Aviation Director may later revoke and retract any prior written consent at any time. Renter shall further be allowed to service its aircraft(s) in the Hangar in accordance with the Scottsdale Airport Rules and Regulations.

5.2 Compliance with Law. Lessee shall perform its obligations under this Agreement in accordance with all federal, state, county and local laws, ordinances, regulations or other rules or policies as are now in effect or as may hereafter be adopted or amended.

5.3 Airport Regulations. Lessor reserves the right to adopt, amend and enforce against Lessee rules and regulations governing the operation of the Airport, including the Leased Premises, Lessee's activities therein and thereon, and the public areas and facilities used by Lessee in connection therewith.

5.4 Aviation Regulations. Lessee shall comply with any and all rules, regulations, laws, ordinances, statutes or orders of the FAA and any other governmental authority, whether federal, state, county, or Lessor, lawfully exercising authority over the Airport.

5.5 Liability and Indemnity. Lessee shall be liable to Lessor, and shall pay, indemnify, defend and hold harmless Lessor against any and all claims, demands, damages, fines or penalties of any nature whatsoever which may be imposed upon Lessor, including attorneys' fees, arising from any violation of law caused directly or indirectly by act, omission, negligence, abuse or carelessness on the part of Lessee, its employees, agents, customers, visitors, suppliers, or invitees. Without limitation, the preceding sentence requires the payment by Lessee of any fines or penalties for any breach of security arising from the unauthorized entry of any of the aforementioned persons or their vehicles onto the passenger loading areas, taxiways, runways, aircraft movement areas and any other restricted portion of the Airport.

5.6 Grant Agreement Assurances. Lessee shall observe and comply with the following covenants and conditions:

5.6.1 No person shall be excluded from participation, denied the benefits of, or be otherwise subjected to discrimination in the use of the Leased Premises on the grounds of race, color, handicap, or national origin. No person shall be excluded on the grounds of race, color, handicap, or national origin from participation in, denied the benefits of, or otherwise be subject to discrimination in the construction of any improvements on, over, or under the Leased Premises and the furnishing of services thereon.

5.6.2 Lessee shall use the Leased Premises in compliance with all other requirements imposed by or pursuant to Title 49, Code of Federal Regulations, Department of Transportation, Subtitle A, office of the Secretary, Part 21, Nondiscrimination in Federally-Assisted Programs of the Department of Transportation-Effectuation of Title VI of the Civil Rights Act of 1964, and as said regulations may be amended.

5.6.3 Lessee shall furnish its accommodations and/or services on a fair, equal and not unjustly discriminatory basis to all users thereof, and it shall charge fair, reasonable and not unjustly discriminatory prices for each unit or service; provided, that Lessee may be allowed to make reasonable and nondiscriminatory discounts, rebates or other similar type of price reductions to volume purchases.

5.6.4 Lessee agrees that it shall insert this article and all of the other provisions of this article titled "Grant Agreement Assurances" in any agreement, lease, contract, etc. by which Lessee grants a right or privilege to any persons, firm or corporation to render accommodations and/or services to the public on the Leased Premises together with a provision that the "Grant Agreement Assurances" shall constitute a material breach thereof, and in the event of such non-compliance Lessor shall have the right to terminate the agreement and the estate thereby created without liability therefore. Either or both Lessor or the United States shall have the right to enforce the "Grant Agreement Assurances."

5.7 Federal Agreements. This Agreement shall be subordinate to the provisions and requirements of any existing or future agreement between the Lessor and the United States, relative to the development, operation or maintenance of the Airport.

5.8 War or National Emergency. This Agreement shall be subject to whatever right the United States Government now has or in the future may have or acquire, affecting the control, operation, regulation and taking over of the Airport or the exclusive or non-exclusive use of the Airport by the United States during the time of war or national emergency.

5.9 Control of Common Areas. All parking areas, driveways, entrances and exits thereto, landscaping areas, aircraft wash rack and all other Common Areas and facilities provided by Lessor for the common use of the users of the Airport, shall at all times be subject to the exclusive control and management of Lessor, and Lessor shall have the right from time to time to establish, modify and enforce reasonable rules and regulations with respect to the use of all the Common Areas and facilities. Lessor shall have the right to operate and maintain the same in such manner as Lessor, in its sole discretion, shall determine from time to time, including without limitation the right to employ all personnel and to make all rules and regulations pertaining to and necessary for the proper operation and maintenance of the Common Areas and facilities. Lessor shall have the exclusive right at any and all times to alter, construct, re-construct, enlarge, contract, modify or relocate any of the Common Areas, to close any portion of the Common Areas for the purpose of making repairs, changes or additions thereto and may change the size, area, layout or arrangement of the parking areas or the number of spaces or the lighting thereof, within or adjacent to the existing areas and may enter into agreements with adjacent owners for cross-easements for parking, ingress or egress. The Lessor shall also have the right to place vending or amusement devices in the Common Areas and any other use which, in the Lessor's judgment, tends to benefit the Airport.

5.10 Parking. Vehicle parking will be allowed inside the hangars only when the aircraft is not occupying the hangar. Vehicle parking shall be subject to current and future rules and regulations governing parking at aircraft hangars and the Airport.

5.11 Airport Operations. Lessee acknowledges that Lessee's use of the Leased Premises shall be subject and subordinate to Lessor's operation of the Airport, which will necessarily directly and indirectly affect Lessee, the Leased Premises, and Lessee's use of the Leased Premises. Lessee's use of the Leased Premises shall not be permitted by Lessee to in any way adversely affect Lessor's use or operation of the Airport. Without limitation:

5.11.1 Lessor reserves the right but shall not be obligated to Lessee to maintain and keep in repair the landing area of the Airport and all publicly-owned facilities of the Airport, together with the right to direct and control all activities of Lessee in this regard.

5.11.2 There is hereby reserved to Lessor, its successors and assignees, for the use and benefit of Lessor and the public, a right of flight for the passage of aircraft in the airspace above the surface of the Leased Premises. This public right of flight shall include the right to cause in said airspace any noise inherent in the operation of any aircraft used for navigation or flight through the said airspace or landing at, taking off from, or operation on the Airport.

5.11.3 Lessee by accepting this Agreement agrees for itself, its successors and assigns that it will not make use of the Leased Premises in any manner that might interfere with the taxiing, landing and taking off of aircraft from the Airport, the loading or unloading of passengers or cargo or other aircraft operations or otherwise constitute a hazard. Whether any particular conduct complies with the foregoing shall be determined by Lessor.

5.12 Communications Operations Restriction. Lessee shall not install, operate, or allow the use of equipment, methodology or technology that may or would interfere with the optimum effective use or operation of Lessor's fire, emergency or other communication equipment, methodology or technology (i.e., voice or other data carrying, receiving or transmitting equipment) that is presently in use or could be in use in the future. If such interference should occur, Lessee shall immediately discontinue using such equipment, methodology or technology that causes the interference until corrective measures are taken. Any such corrective measures shall be made at no cost to Lessor.

5.12.1 Lessor has not relinquished or limited any right of condemnation or eminent domain over the Leased Premises or any other property related to this Agreement.

5.12.2 This Agreement does not impair City of Scottsdale's power to enact, apply or enforce any laws or regulations, or exercise any governmental powers affecting in any way Lessee or the Leased Premises. Lessor's rights and remedies hereunder for Lessee's failure to comply with all applicable laws supplement and are in addition to and do not replace otherwise existing powers of the City of Scottsdale or any other governmental body.

## VI. BREACH

6 Breach by Lessee. Lessee shall comply with, perform and do each performance and thing required of Lessee herein and Lessee's failure to do so shall be a breach by Lessee of this Agreement.

6.1 Events of Default. This entire Agreement is made upon the condition that each and every one of the following events shall be deemed an "Event of Default" and a material breach by Lessee of Lessee's material obligations under this Agreement:

6.1.1 If Lessee shall be in arrears in the payment of Rent or the Security Deposit and shall not cure such arrearage within 10 days after Lessor has notified Lessee in writing of such arrearage.

6.1.2 If Lessee shall fail to maintain the Leased Premises as required in this Agreement.

6.1.3 If Lessee shall fail to timely pay any taxes or other amounts herein required to be paid by Lessee to Lessor or to any other person.

6.1.4 If Lessee shall fail to obtain or maintain any licenses, permits, or other governmental approvals from Lessor or any other governmental body or timely pay any taxes with respect to this Agreement, the Leased Premises or Lessee's use of the Leased Premises.

6.1.5 If Lessee shall engage in a pattern of repeated failure (or neglect) to timely do or perform or observe any provision contained herein. Three or more failures to comply with any provision of this Agreement during any 12 month period constitutes a repeated failure by Lessee to comply with such provision.

6.2 Lessor's Remedies. Upon the occurrence of any Event of Default or at any time thereafter, Lessor may, at its option and from time to time, without further demand or notice, exercise any or all or any combination of the following remedies in any order and repetitively at Lessor's option:

6.2.1 Terminate this Agreement.

6.2.2 Enter into and upon the Leased Premises or any part thereof, and expel Lessee and those claiming by, through or under it, and remove their effects, if any, forcibly if necessary, without being deemed guilty of trespass and without prejudice to any other remedy.

6.2.3 Abate at Lessee's expense any violation of this Agreement.

6.3 Notice of Breach. Lessee shall promptly give notice to Lessor of any event or circumstance that is (or with the passing of time or the giving of notice or both will become) an Event of Default under this Agreement.

6.4 Non-waiver. Lessee acknowledges Lessee's unconditional obligation to comply with this Agreement. No failure by Lessor to demand any performance required of Lessee under this Agreement, and no acceptance by Lessor of any imperfect or partial performance under this Agreement, shall excuse such performance or impair in any way Lessor's ability to insist, prospectively and retroactively, upon full compliance with this Agreement. No acceptance by Lessor of Rent or other performances hereunder shall be deemed a compromise or settlement of any claim Lessor may have for additional or further payments or performances. Any waiver by Lessor of any breach of condition or covenant herein contained to be kept and performed by Lessee shall not be deemed or considered as a continuing waiver and shall not operate to bar or otherwise prevent Lessor from declaring a default for any breach or succeeding breach either of the same condition or covenant or otherwise. No statement, bill or notice by Lessor concerning



payments or other performances due hereunder shall excuse Lessee from compliance with this Agreement nor estop Lessor (or otherwise impair Lessor's ability) to at any time correct such notice and/or insist prospectively and retroactively upon full compliance with this Agreement. No waiver of any description (including any waiver of this sentence or article) shall be effective against Lessor unless made in writing by a duly authorized representative of Lessor specifically identifying the particular provision being waived and specifically stating the scope of the waiver. LESSEE EXPRESSLY DISCLAIMS AND SHALL NOT HAVE THE RIGHT TO RELY ON ANY SUPPOSED WAIVER OR OTHER CHANGE OR MODIFICATION, WHETHER BY WORD OR CONDUCT OR OTHERWISE, NOT CONFORMING TO THIS ARTICLE.

6.5 Reimbursement of Lessor's Expenses. Lessee shall pay to Lessor upon demand any and all amounts expended or incurred by Lessor in performing Lessee's obligations.

6.6 Default by Lessor. Notwithstanding anything in this Agreement to the contrary, in the event Lessor at any time is required to render any performance, such performance is not due until 30 days after notice by Lessee to Lessor that that the performance is due. If a cure cannot be affected during that period, Lessor shall not be in default so long as Lessor commences cure during the period and diligently prosecutes the cure to completion provided such cure must be completed within 60 days after it is due.

## VII. TERMINATION

7 Rights at Termination. The following provisions shall apply upon expiration or other termination of this Agreement at any time for any reason:

7.1 Surviving Obligations. Lessee's obligations existing or arising prior to or simultaneous with, or attributable to, the termination or events leading to or occurring before termination shall survive and not terminate.

7.2 Delivery of Possession. Lessee shall, without demand, peaceably and quietly quit and deliver up the Leased Premises to Lessor thoroughly cleaned, in good repair, maintained and repaired and in as good order and condition, reasonable use and wear excepted, as the same or in such better condition as the Leased Premises may hereafter be delivered to Lessee's possession at the beginning of the lease term.

7.3 Mutual Termination. If it is in the best interests of the City, the Aviation Director may agree with Lessee to a mutual termination of this Lease upon commercially reasonable terms that account for the circumstances existing at the time of the termination.

## VIII. INDEMNITY AND INSURANCE

8 Indemnity and Insurance. Lessee shall insure the Premises and its property and activities at and about the Premises and shall provide insurance and indemnification as follows:

8.1 Insurance Required. Prior to entering, occupying or using the Premises in any way thereafter, and in any event not later than the date 30 days after the date of this Agreement, and at all times thereafter, Lessee shall obtain and cause to be in force and effect the following insurance:

8.1.1 Airport Premises Liability Insurance. Lessee shall provide aircraft liability insurance that covers damages for bodily injury or property damage arising out of the use of airport premises including ramps and taxiways for the parking and storage of aircraft. Such insurance shall have a minimum limit of One Million Dollars (\$1,000,000) for each occurrence and a One Million Dollar (\$1,000,000) aggregate if applicable.

8.1.2 Special Perils or All Risk Property Coverage. Lessee shall maintain Special Risk Causes or Loss Property coverage, as defined by Insurance Services Office, Inc. in an amount per occurrence equal to the full replacement cost of the tenant's betterments and improvements and naming the Lessor as loss payee for the damage or destruction of that property. Lessee's Property coverage shall include debris removal coverage in an amount sufficient to clear the premises of Lessee or Lessee's customers disabled or destroyed property.

8.1.3 Hangar Keeper's Liability. If Lessee will store other than owned aircraft, Lessee shall carry Hangar Keeper's Liability coverage covering the portions of the Premises used for aircraft storage in an amount equal to the full replacement cost of aircraft subject to loss or damage while in the care, custody, or control of Lessee for safekeeping, storage, service, or repair. The minimum insurance limits for this coverage shall be One Million Dollars (\$1,000,000).

8.1.4 Other Insurance. Any other insurance Lessor may reasonably require for the protection of Lessor and Lessor's employees, officials, representatives, officers, directors, and agents (collectively "Additional Insureds"), the Premises, surrounding property, Lessee, or the activities carried on or about the Premises. Likewise, Lessor may elect by notice to Lessee to increase the amount of any insurance to account for inflation, changes in risk, or any other factor that Lessor reasonably determines to affect the prudent amount of insurance to be provided. Any modification or variation from these insurance requirements must be made by the Lessor's Risk Manager, whose decision is final. Such action will not require a formal Contract amendment, but may be made by administrative action.

8.2 Form of All Insurance. All insurance policies shall meet the following requirements:

8.2.1 All policies except workers' compensation must name Lessor and the other Additional Insureds as additional insureds. Lessee shall cause coverage for Additional Insureds to be incorporated into each insurance policy by endorsement.

8.2.2 Lessee or Lessee's Insurer shall provide Lessor with at least 30 days prior notice of any cancellation, reduction or other material change in coverage.

8.2.3 All policies shall require that notices be given to Lessor in the manner specified for notices to Lessor under this Agreement.

8.2.4 "Occurrence" coverage is required. "Claims made" insurance is not permitted except for Environmental Impairment Liability and employment liability insurance.

8.2.5 Policies must also cover and insure Lessee's activities relating to the business operations and activities conducted from the Premises.

8.2.6 All insurance policies shall contain a waiver of any transfer rights of recovery (subrogation) against Lessor, and all other Additional Insureds.

8.2.7 No deductibles, retentions, or "self-insured" amounts shall exceed Seven Hundred Fifty Thousand Dollars (\$750,000) in the aggregate per year, per policy. If Lessee desires higher deductibles, retentions, or "self-insured" amounts, Lessee shall notify Lessor in writing not more often than once per year requesting a change in the amount. Lessor shall have the right to accept, modify, limit, or reject Lessee's request. Lessee shall be solely responsible for any self-insurance amount or deductible. Lessor may require Lessee from time to time to secure payment of such deductible or self-insured retention by a surety bond or irrevocable and unconditional letter of credit.

8.3 Insurance Certificates. Lessee shall evidence all insurance by furnishing to Lessor certificates of insurance annually and with each change in insurance. Certificates must evidence that the policy referenced by the certificate is in full force and effect and that the policy satisfies each requirement of this Agreement applicable to the policy. For example, certificates must indicate that Lessor and the other Additional Insureds are additional insureds and waiver of subrogation and other provisions apply. Certificates must be in a form acceptable to Lessor. All certificates are in addition to the actual policies and endorsements required. Lessee shall provide updated certificates at Lessor's request.

8.4 Acceptable Insurers. All insurance policies shall be issued by insurers acceptable to Lessor. At a minimum, all insurers shall be duly licensed (or qualified unlicensed non-admitted insurer) by the State of Arizona, Department of Insurance. At a minimum, all insurers shall have and maintain an A.M. Best, Inc. rating of B++6.

8.5 Primary Insurance. Lessee's insurance including excess liability policies shall be primary insurance. Any insurance or self-insurance maintained by Lessor shall not contribute to Lessee's insurance.

8.6 Indemnity. In addition to all other obligations hereunder, to the fullest extent permitted by law, throughout the term of this Agreement and until all obligations and performances under or related to this Agreement are satisfied and all matters described in this paragraph are completely resolved, Lessee shall pay, indemnify, defend and hold harmless Lessor and all other Additional Insureds for, from and against any and all claims or harm related to the Premises and/or this Agreement (the "Indemnity"). Without limitation, the Indemnity shall include and apply to any and all allegations, demands, judgments, assessments, taxes, impositions, expenses, proceedings, liabilities, obligations, suits, actions, claims (including without limitation claims of personal injury, bodily injury, sickness, disease, death, property damage, destruction, loss of use or other impairment), damages, losses, expenses, penalties, fines or other matters (together with all attorney fees, court costs, and the cost of appellate proceedings) which may arise in any manner out of any use of the Premises or Lessor's property related to this Agreement or any actions, acts, errors, mistakes or omissions relating to work or services in the performance of or related to this Agreement, including any injury or damages or cause of action claimed or caused by any employees, contractors, subcontractors, tenants, subtenants, agents or other persons upon or using the Premises or surrounding areas related to this Agreement, including without limitation

claims, liability, harm or damages caused in part by Lessor or any other Additional Insured or anyone for whose mistakes, errors, omissions or negligence Lessee or Lessor may be liable. The Indemnity shall also include and apply to any environmental, personal injury or other liability relating to Lessor's or Lessee's acquisition, ownership or use of real property developed, operated, owned, used, controlled or possessed by Lessor or Lessee under this Agreement.

Notwithstanding the foregoing, the Indemnity does not apply to:

8.6.1 Claims arising only from the sole gross negligence of Lessor.

8.6.2 Claims that the law prohibits from being imposed upon Lessee.

8.7 Risk of Loss. Lessor is not required to carry any insurance covering or affecting the Premises or use of Lessor's property related to this Agreement. Lessee assumes the risk of any and all loss, damage or claims to the Premises or related to Lessee's use of the Premises or other property of Lessor, Lessee or third parties throughout the term hereof. Lessor expressly disclaims any representation that required insurance is adequate to protect any person or property against any risks related to the Premises or any activities, uses or improvements related to the Premises. Lessee's obligations to indemnify do not diminish in any way Lessee's obligations to insure; and Lessee's obligations to insure do not diminish in any way Lessee's obligations to indemnify. Lessee's obligations to indemnify and provide insurance are in addition to, and do not limit, any and all other liabilities or obligations of Lessee under or connected with this Agreement. Lessee shall be responsible for any and all damages to its property and equipment related to this Agreement and shall hold harmless and indemnify Lessor regardless of the cause of such damages. In the event Lessee secures other insurance related to the Premises or any improvements, property or uses related thereto, Lessee shall effect an endorsement under such policy waiving any and all insurer's rights of subrogation against Lessor and the other Additional Insureds.

8.8 Insurance to be Provided by Lessees, Sublessees, and Others. Any Subleases, Contractors, or other persons occupying, working on or about, or using the Premises pursuant to this Agreement must also provide for the protection of Lessor and all other Additional Insureds all of the insurance and indemnification required by this Agreement. The preceding sentence does not require such persons to provide insurance that merely duplicates insurance Lessee provides. Lessee shall cause any persons basing aircraft at the Premises to name Lessee, Lessor, and the Additional Insureds as additional insureds under their aircraft liability policies. Such policies shall contain waivers of subrogation as to Lessee and Lessor and the other Additional Insureds. Lessee shall execute a written agreement with Subcontractors, Sublessees, or others occupying, working on or about, or using the Premises pursuant to this Agreement containing the same Indemnification Clause and Insurance Requirements set forth herein protecting Lessor and Lessee. Lessee shall be responsible for executing the agreement with any Sublessees, Subcontractors or others occupying the Premises and obtaining Certificates of Insurance verifying the insurance requirements.

IX. ASSIGNMENT/SUBLET

9 Terms and Conditions Applicable to Assignment and Subletting.

9.1 Regardless of Lessor's consent, no assignment or subletting shall:

9.1.1 Be effective without the express written assumption by such assignee or sublessee of the obligations of Lessee under this Lease;

9.1.2 Release Lessee of any obligations hereunder; or

9.1.3 Alter the primary liability of Lessee for the payment of Rent or for the performance of any other obligations to be performed by Lessee.

9.2 Lessor may accept Rent or performance of Lessee's obligations from any person other than Lessee pending written approval or disapproval of an assignment. Neither a delay in the approval or disapproval of such assignment nor the acceptance of Rent or performance shall constitute a waiver or estoppel of Lessor's right to exercise its remedies for Lessee's Default or Breach.

9.3 Lessor's consent to any assignment or subletting shall not constitute a consent to any subsequent assignment or subletting.

9.4 In the event of any Default or Breach by Lessee, Lessor may proceed directly against Lessee, any Guarantors or anyone else responsible for the performance of Lessee's obligations under this Lease, including any assignee or sublessee, without first exhausting Lessor's remedies against any other person or entity responsible therefore to Lessor, or any security held by Lessor.

9.5 Each request for consent to an assignment or subletting shall be in writing, accompanied by information relevant to Lessor's determination as to the financial and operational responsibility and appropriateness of the proposed assignee or sublessee, including but not limited to the intended use and/or required modification of the Premises, if any, \$500 as consideration for Lessor's considering and processing said request. Lessee agrees to provide Lessor with such other or additional information and/or documentation as may be reasonably requested.

9.6 Any assignee of, or sublessee under, this Lease shall, by reason of accepting such assignment or entering into such sublease, or entering into possession of the Premises or any portion thereof, be deemed to have assumed and agreed to conform and comply with each and every term, covenant, condition and obligation herein to be observed or performed by Lessee during the term of said assignment or sublease, other than such obligations as are contrary to or inconsistent with provisions of an assignment or sublease to which Lessor has specifically consented to in writing.

9.7 Lessor's consent to any assignment or subletting shall not transfer to the assignee or sublessee any Option granted to the original Lessee by this Lease unless such transfer is specifically consented to by Lessor in writing.

9.8 Additional Terms and Conditions Applicable to Subletting. The following terms and conditions shall apply to any subletting by Lessee of all or any part of the Premises and shall be deemed included in all subleases under this Lease whether or not expressly incorporated therein:

9.8.1 Lessee hereby assigns and transfers to Lessor all of Lessee's interest in all Rent payable on any sublease, and Lessor may collect such Rent and apply same toward Lessee's obligations under this Lease; provided, however, that until a Breach shall occur in the performance of Lessee's obligations, Lessee may collect said Rent. In the event that the amount collected by Lessor exceeds Lessee's then outstanding obligations any such excess shall be refunded to Lessee. Lessor shall not, by reason of the foregoing or any assignment of such sublease, nor by reason of the collection of Rent, be deemed liable to the sublessee for any failure of Lessee to perform and comply with any of Lessee's obligations to such sublessee. Lessee hereby irrevocably authorizes and directs any such sublessee, upon receipt of a written notice from Lessor stating that a Breach exists in the performance of Lessee's obligations under this Lease, to pay to Lessor all Rent due and to become due under the sublease. Sublessee shall rely upon any such notice from Lessor and shall pay all Rents to Lessor without any obligation or right to inquire as to whether such Breach exists, notwithstanding any claim from Lessee to the contrary.

9.8.2 In the event of a Breach by Lessee, Lessor may, at its option, require sublessee to attorn to Lessor, in which event Lessor shall undertake the obligations of the sublessor under such sublease from the time of the exercise of said option to the expiration of such sublease; provided, however, Lessor shall not be liable for any prepaid rents or security deposit paid by such sublessee to such sublessor or for any prior Defaults or Breaches of such sublessor.

9.8.3 Any matter requiring the consent of the sublessor under a sublease shall also require the consent of Lessor.

9.8.4 No sublessee shall further assign or sublet all or any part of the Premises without Lessor's prior written consent.

9.8.5 Lessor shall deliver a copy of any notice of Default or Breach by Lessee to the sublessee, who shall have the right to cure the Default of Lessee within the grace period, if any, specified in such notice. The sublessee shall have a right of reimbursement and offset from and against Lessee for any such Defaults cured by the sublessee.

## X. MISCELLANEOUS

10 Miscellaneous. The following additional provisions shall apply:

10.1 Amendments. This Agreement may not be amended except by a formal writing executed by the parties, including the approval of the City Council. Provided, however, the Aviation Director is authorized to approve minor administrative amendments to the provisions of this Lease which allow for relocation of Lessee to a comparable premises with additional or different features (such as a mezzanine) as long as the amount of the Rent as set forth in Section 4 is adjusted to account for the reasonable rental value of such new premises.



If to Lessee: Laurence Palestrant  
3R Ranch, Inc.  
P.O. Box 5532  
Scottsdale, AZ 85261

Notices to Lessee may also be hand delivered to Lessee's management office at the Aviation Business Center Building. Notices given or served by personal delivery shall be deemed to have been received upon tender to the respective party. Notices given or served by mail or commercial courier shall be deemed to have been given or served as of the date of delivery (whether accepted or refused).

10.8 Funding. This article shall control notwithstanding any provision of this Agreement or any exhibit or other agreement or document related hereto. If funds necessary to fulfill Lessor's obligations under this Agreement are not appropriated by the Scottsdale City Council, Lessor may terminate this Agreement by 30 days' notice to Lessee. Termination in accordance with this provision shall not constitute a breach of this Agreement by Lessor. No person will be entitled to any compensation, damages or other remedy from Lessor if this Agreement is terminated pursuant to the terms of this subsection.

10.9 Article Headings. The article headings contained herein are for convenience in reference and not intended to define or limit the scope of any provision of this Agreement.

10.10 Lessor's Right of Entry. Lessor reserves the right at all reasonable times during the term for Lessor or Lessor's agents to enter the Leased Premises for the purpose of inspecting and examining the same, and to show the same to actual or prospective tenants or lenders, and to make such repairs, alterations, improvements or additions as Lessor may deem necessary or desirable, and for any other purposes Lessor deems necessary. During the 90 days prior to the expiration of the term or any renewal term, Lessor may exhibit the Leased Premises to prospective tenants, and place upon the Leased Premises customary "For Lease" signs, as the case may be, which signs Lessee shall permit to remain thereon without molestation. If Lessee shall not be personally present to open and permit entry into said Leased Premises, at any time, when for any reason an entry therein shall be necessary or permissible, Lessor or Lessor's agents may forcibly enter the same, without rendering Lessor or such agents liable therefor, and without any manner affecting the obligations and covenants of this Lease. Nothing herein contained, however, shall be deemed or construed to impose upon Lessor any obligation, responsibility or liability whatsoever for the care, maintenance or repair of the building or any part thereof, except as otherwise herein specifically provided. No exercise by Lessor of any rights under this Article 16 shall entitle Lessee to any damages for inconvenience, disturbance, constructive eviction, loss of business or other damage to Lessee occasioned thereby, nor to any abatement of rent.

10.11 Attorneys' Fees. In the event any action or suit or proceeding is brought by either Lessor or Lessee to enforce compliance with this Agreement or for failure to observe any of the covenants of this Agreement or to vindicate or exercise any rights or remedies hereunder, the prevailing party shall be entitled to recover reasonable attorneys' fees, accounting fees, and other costs incurred in that action or proceeding, in addition to any other relief to which it may be entitled.



10.12 No Third Party Beneficiaries. Except as otherwise expressly provided, no person or entity shall be a third party beneficiary to this Agreement or shall have any right or cause of action hereunder.

10.13 Exhibits. All exhibits attached hereto are incorporated into this Agreement by this reference.

10.14 Further Assurances. Lessee agrees to do such further acts and things and to execute and deliver such additional agreements and instruments as Lessor may reasonably require to consummate, evidence, confirm or carry out the agreement contained herein.

10.15 Construction. Whenever the context of this Agreement requires, the singular shall include the plural, and the masculine shall include the feminine. This Agreement shall be construed according to its plain meaning and neither for nor against any party hereto. Lessee acknowledges that the Rent payable hereunder was negotiated in light of the plain meaning of this Agreement and this Agreement shall therefore be interpreted according to its plain meaning and without regard to rules of interpretation, if any, which might otherwise favor Lessee.

10.16 Survival of Liability. All obligations of Lessee and Lessor hereunder and all warranties and indemnities of Lessee and Lessor hereunder shall survive termination of this Agreement for any reason.

10.17 Choice of Law. This Agreement is governed by, and shall be construed and enforced in accordance with, the laws of the State of Arizona, without giving effect to any conflict of laws rules, and each party irrevocably submits to the exclusive jurisdiction of the federal and state courts located in Maricopa County, Arizona for the purposes of any action or proceeding arising out of or relating to this Agreement. Each party hereby consents to such jurisdiction and agrees that venue shall lie in the state or federal courts within Maricopa County, Arizona with respect to any claim or cause of action arising under or relating to this Agreement. Each party hereby waives any objection based on *forum non conveniens* and waives any objection to venue of any action instituted hereunder

10.18 Approvals and Inspections. All approvals, reviews and inspections by Lessor under this Agreement or otherwise are for Lessor's sole benefit and not for Lessee's benefit.

10.19 Statutory Cancellation Right. In addition to its other rights hereunder, Lessor shall have the cancellation rights specified in A.R.S. § 38-511.

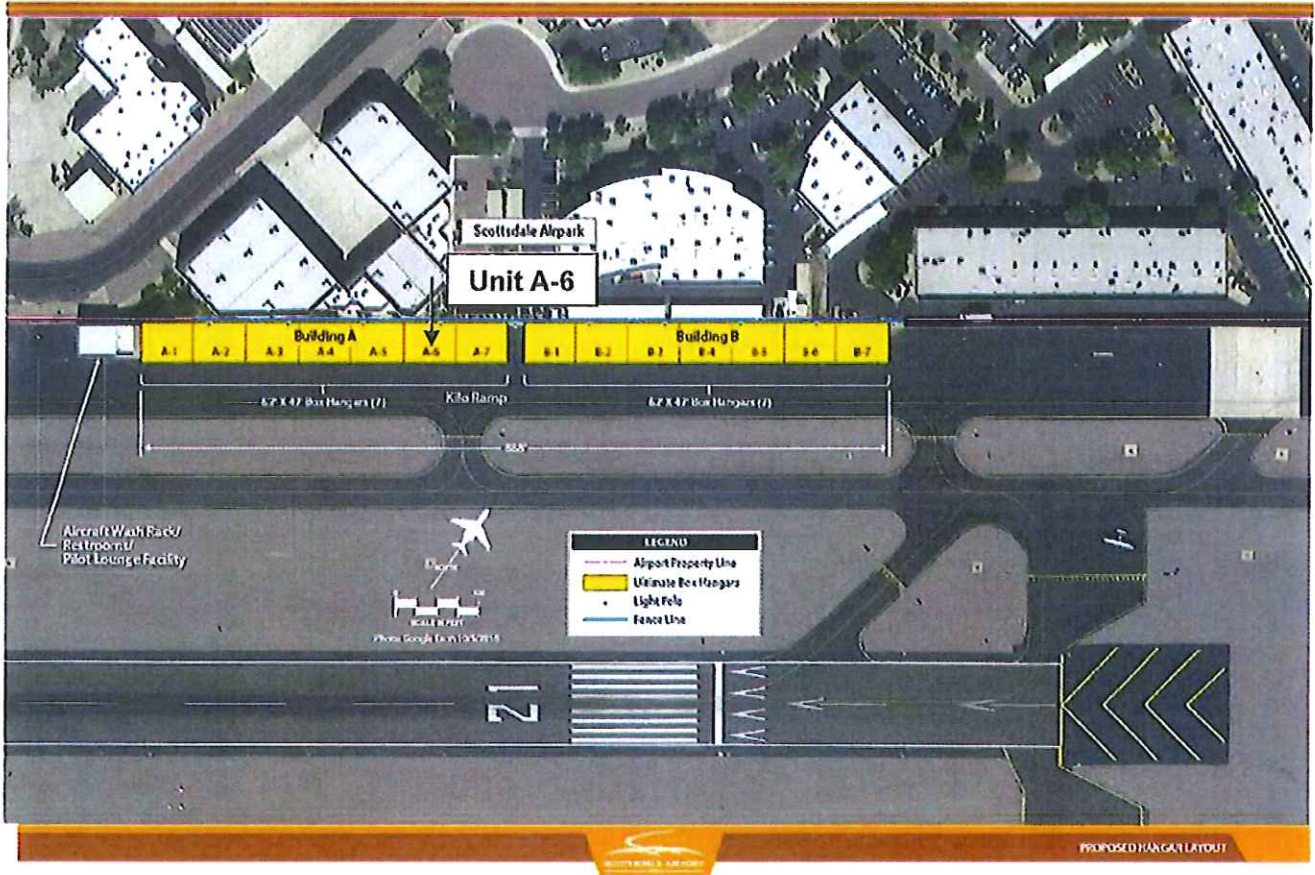
EXECUTED as of the date first given above.

*[Signatures on pages to follow]*





**Exhibit "A"**  
**General Layout of North General Aviation Box Hangars**





## COMMISSION ACTION REPORT

Discussion and possible action regarding 1-GP-2021: Draft Scottsdale  
General Plan 2035

**Agenda Item No.:** 2

**Meeting Date:** 02/17/21

**Staff Contact:** Taylor Reynolds,  
Project Coordination Liaison

**Phone:** (480) 312-7924

### ACTION

Discussion and possible action regarding 1-GP-2021: Draft Scottsdale General Plan 2035

### PURPOSE

To present the Airport Advisory Commission with the General Plan 2035 process, the Citizen Review Committee (CRC) recommend draft plan, and discussion related to draft plan content to the purview of the Commission.

Attachment(s): 1. Memo packet

Action taken:



Community & Economic Development Division  
Planning and Development Services

7447 East Indian School Road, Suite 105  
Scottsdale, Arizona 85251

**To:** Airport Advisory Commission  
**From:** Taylor Reynolds, Project Coordination Liaison  
**Through:** Gary P. Mascaro, Aviation Director  
Erin Perreault, AICP, Long Range Planning Director  
**Date:** February 17, 2021  
**Re:** 1-GP-2021: Draft Scottsdale General Plan 2035

This memorandum presents the Airport Advisory Commission with the General Plan 2035 update process, the Citizen Review Committee (CRC) recommended draft plan, and discussion related to draft plan content relevant to the purview of the Commission.

#### **BACKGROUND**

The current Scottsdale General Plan was adopted in 2001 and ratified by the voters in 2002. The Plan defines the community's goals for growth, development, character, mobility and a variety of other community aspects. Per State Statute, the city is required to update and ratify the General Plan every ten (10) years. In March 2012, after three (3) years of public input, the Scottsdale City Council sent an updated 2011 General Plan to the voters. However, the measure was defeated (52% to 48%), and thus, the 2001 General Plan remains in effect until the city adopts and ratifies a new General Plan.

In January 2013, City Council again directed that the Scottsdale General Plan be updated; including the public outreach, drafting and adoption/ratification timing to achieve it. City Council also directed staff to establish a citizen task force to oversee the drafting of a new plan. In November 2014, this 25-member Task Force – representing citizens from all areas of the city – delivered a draft 2035 General Plan for community, Planning Commission and City Council consideration, however it was never processed through the State required public hearings and thus remains as a draft Plan.

Subsequently, in a December 2016 public meeting, Council directed staff to update the 2001 General Plan to incorporate all state mandated content, and any necessary changes to the plan to update it to existing community conditions. This update to the 2001 General Plan was completed on November 12, 2019 and unanimously adopted by Council.

#### **THE GENERAL PLAN UPDATE CITIZEN REVIEW COMMITTEE 2020**

Under the direction of the City Manager and as approved by City Council on June 16, 2020 (Enclosure 1), a thirteen (13) member General Plan Update Citizen Review Committee (CRC) was formed to update the Scottsdale General Plan. This update effort utilized the 2014 Task Force recommended draft 2035 General Plan as the baseline plan.

Consisting predominantly of either the chair or vice chair from each of the city's boards and commissions that has related content within the General Plan (which is a similar citizen review process used with the 2001 and 2011 General Plan updates) CRC members (Enclosure 2) represented the following:

- McDowell Sonoran Preserve Commission
- Historic Preservation Commission
- Human Relations Commission
- Tourism Development Commission
- Airport Advisory Commission
- Scottsdale Environmental Advisory Commission
- Library Board
- Parks & Recreation Commission
- Development Review Board
- Transportation Commission
- Industrial Development Authority
- Neighborhood Advisory Commission
- Human Services Commission

Accordingly, Commissioner Larry Bernoski represented the Commission at the CRC meetings. The purpose of the CRC was to review the content of the draft plan during public meetings and make suggestions as to any proposed adjustments. The CRC workplan concluded on December 14, 2020 with a final recommended draft General Plan 2035 that will be used for review by the community, city boards and commissions, Scottsdale Planning Commission, and Scottsdale City Council so as to adopt an updated General Plan that can then be sent to the November 2021 ballot for voter ratification consideration, per state law requirements.

Due to the COVID-19 Pandemic, all (13) meetings of the CRC were conducted electronically. Although physical facilities were not open to the public, the CRC meetings were noticed as public meetings with regularly posted agendas and minutes consistent with the practices of City Council, other City boards and commissions and State Open Meeting Law. Additionally, these meetings were televised on Cox Cable Channel 11 and streamed online at ScottsdaleAZ.gov as to allow the public to listen/view the meeting in progress. Public comments (300+) were also solicited through both written and spoken format.

### **THE DRAFT GENERAL PLAN 2035**

Although many new or enhanced ideas are included in the Plan, many concepts from the 2001 General Plan remain, including:

- The foundation for the vision statement: Scottsdale's Shared Vision and CityShape 2020.
- The three-levels of planning—General Plan, Character Area Plan, and Neighborhood Plan established in CityShape 2020.
- A substantial focus on community character and design.
- The type and location of land uses city-wide.
- Scottsdale's leadership role in environmental stewardship and open space preservation.
- The existing Growth Areas (Old Town, Airpark, and McDowell), but with specific rather than generalized boundaries.
- The existing adopted Character Areas.

- Four major General Plan amendment criteria that focus on changes in land use, acreage, Character Area conformance, and Water/Sewer infrastructure.

The following are some aspects of the draft General Plan 2035 that are different from the 2001 General Plan, along with noted changes specifically created by the Citizen Review Committee:

- New vision statement, community values, and organization of the overall plan.
  - The Citizen Review Committee incorporated the theme of an educated citizenry within Vision Statement and Community Aspirations.
- Enhanced emphasis on tourism, fiscal sustainability, open space, community health, arts and culture, and safety.
- Enhanced focus on community character, such as transitions/buffers and contextual compatibility.
- Shift from a primary focus on new development to revitalization, redevelopment, and preservation.
- The Citizen Review Committee provided additional emphasis on the concepts of sustainability, inclusivity, and public safety.
- Three community-added elements: Arts, Culture & Creative Community, Healthy Community, and Tourism.
  - The Citizen Review Committee created the Tourism Element, comprised of both existing and new goals and policies.
- Revised Character Area Planning map showing existing/adopted plans and possible boundaries for future Character Areas.
- Removal of ambiguous designations from the Land Use Map (e.g. “resort stars,” golf course (G), and open space circle designations).
- Expanded and more specific General Plan Amendment Criteria addressing:
  - Land use changes of 10 or more acres now city-wide (previously it was 15 acres in the north and 10 acres in the south),
  - Designation/expansion of Infill Incentive Districts would now require a major General Plan amendment process ,
  - Proposed changes to the text of the amendment criteria would now require a major General Plan amendment process ,
  - Proposed growth area designation or expansion would now require a major General Plan amendment process , and
  - Clarification of major/minor amendment determinations for criteria exceptions maintained from the 2001 General Plan—such as properties with land use category overlays (e.g. Shea Corridor or Regional Use).
    - The Citizen Review Committee further clarified and reduced the number of criteria exceptions associated with amending the General Plan.
- A new section/list identifying implementation programs to carry out the plan and evaluate General Plan progress.



- The Citizen Review Committee updated the implementation programs so as to reflect all changes previously noted within the plan (e.g. new Tourism Element).

### **DRAFT 2035 GENERAL PLAN CONTENT SPECIFIC TO AIRPORT ADVISORY COMMISSION**

The recommended draft 2035 General Plan, as approved by the Citizen Review Committee (CRC), updates the 2014 Task Force Recommended Plan and is notated with tracked edits, as discussed and approved by the Committee. The following color-coded legend describes the tracked edits found in the plan:

- Black Font – Text that has been unaltered, and is from the General Plan Task Force process (2014)
  - **(NEW)** – This signifies General Plan Task Force content that is new to the General Plan, and not 2001 General Plan content
- **RED FONT** – Suggested edits from public comments collected between 2014-2020
- **GREEN FONT** – Suggested edits from a Technical Advisory Committee (TAC) comprised of city staff that reviewed and provided input on the plan in 2020
- **LIGHT BLUE FONT** – Edits from the CRC, made during their public meetings held in 2020

Please also note the following regarding legislative edits by the Citizen Review Committee:

- Maintaining the **RED** or **GREEN** text in the CRC draft plan indicates that the CRC accepted the respective suggested edits from the public/TAC for inclusion in the plan.
- If the **RED** or **GREEN** text has **LIGHT BLUE** strike through lines this indicates that the CRC has recommended striking the public/TAC edits.

Long Range Planning staff has provided draft 2035 General Plan content related to the purview of the Airport Advisory Commission. Consequently, enclosures to this memorandum include the draft Land Use (Enclosure 3), Safety (Enclosure 4), Circulation (Enclosure 5), and Economic Vitality (Enclosure 6) Elements, as well as, the draft Vision and Community Values (Enclosure 7) and Implementation Chapter (Enclosure 8) – as recommended and approved by the Citizen Review Committee. Additionally, the Airport Advisory Commission is encouraged to review the entire draft document on the city’s website, which includes the following options for review:

- [www.scottsdaleaz.gov/Assets/ScottsdaleAZ/General+Plan/CRC\\_FULLDOC.pdf](http://www.scottsdaleaz.gov/Assets/ScottsdaleAZ/General+Plan/CRC_FULLDOC.pdf) – the entire draft plan in tracked, legislative format, as described above;
- [www.scottsdaleaz.gov/Assets/ScottsdaleAZ/General+Plan/CRC\\_FULLDOC\\_Recommended.pdf](http://www.scottsdaleaz.gov/Assets/ScottsdaleAZ/General+Plan/CRC_FULLDOC_Recommended.pdf) – the entire draft plan in clean, non-legislative format; and,
- [www.scottsdaleaz.gov/general-plan/general-plan-updates](http://www.scottsdaleaz.gov/general-plan/general-plan-updates) - the General Plan update webpage where comments related to the various draft Elements and sections of the plan can be submitted.

The following is a summation of the more significant changes that resulted from CRC discussion, input, and subsequent recommendation as it relates to the purview of the commission and the enclosed sections of the draft General Plan 2035:

- The Land Use Element includes a new goal of protecting the viability of the Scottsdale Airport (LU 7). To this end there are new supporting policies with this goal that address:
  - Maintaining the Airport Part 150 Noise Compatibility Program (LU 7.1)
  - Maintaining Runway Protection Zones (LU 7.2)
  - Supporting aviation-related economic development opportunities (LU 7.3)
- The Land Use Element includes an Airport Influence Area Map (page 81) that provides detail to the Airport Influence Areas, Noise Contours, and Runway Protection Zones.
- The Safety Element includes a new goal of maintaining safe airspace (S 5). To this end there is a new supporting policy with this goal that addresses:
  - Maintaining clear zones around Scottsdale Airport (S 5.2)
- The Circulation Element includes a new policy to support the goal of improving transportation corridors that addresses:
  - Supporting the Scottsdale Airport as an integral transportation hub (C 1.9)
- The Economic Vitality Element includes a new policy to support the goal of managing land uses that enhance economic development that addresses:
  - Enhancing and protecting the Scottsdale Airport as a global connection for tourism and business development (EV 3.6)
- The Implementation Chapter includes general updates to implementation programs and initiatives – with no recommended timing changes specific to Airport-related initiatives.

## **NEXT STEPS**

The state mandated public hearing process for a General Plan update began in January 2021, when city staff sent out the draft updated General Plan to the state, county, and adjacent communities for review, as required by state law. Between January and June/July of 2021, the updated draft General Plan will be required to meet all of the public hearing state statute requirements, which will include but is not limited to, enhanced public outreach, presentations to boards and commissions that have General Plan related content, at least three Planning Commission study sessions, a remote Planning Commission hearing, a regular Planning Commission recommendation hearing, and at least three City Council meetings in addition to the City Council hearing in which Council must decide whether or not to adopt and send the updated Plan to the voters.

Should City Council decide to adopt and send the plan to the voters, it is anticipated that the first available regular election that the Council-adopted plan could be considered by the voters is the November 2021 election.

## **STAFF RECOMMENDATION, DISCUSSION, AND POSSIBLE ACTION**

Staff recommends that the Airport Advisory Commission discuss, consider, and provide comments on draft 2035 General Plan content relevant to the purview of the Commission.

If at any time Commission members have questions regarding the Scottsdale General Plan update, please feel free to contact:

- Adam Yaron, Project Coordination Liaison  
[ayaron@scottsdaleaz.gov](mailto:ayaron@scottsdaleaz.gov) (480) 312-2761
- Taylor Reynolds, Project Coordination Liaison  
[treynolds@scottsdaleaz.gov](mailto:treynolds@scottsdaleaz.gov) (480) 312-7924

Enclosure:

1. June 16, 2020 City Council Action Report Adopting Procedures for the Scottsdale General Plan Update Process, in compliance with State Statute requirements
2. Boards and Commissions Representatives to The Citizen Review Committee
3. Citizen Review Committee Recommended Draft General Plan 2035 – Land Use Element
4. Citizen Review Committee Recommended Draft General Plan 2035 – Safety Element
5. Citizen Review Committee Recommended Draft General Plan 2035 – Circulation Element
6. Citizen Review Committee Recommended Draft General Plan 2035 – Economic Vitality Element
7. Citizen Review Committee Recommended Draft General Plan 2035 – Vision and Community Values
8. Citizen Review Committee Recommended Draft General Plan 2035 – Implementation Chapter
9. Matrix of public comments collected regarding the draft Plan from 2014-2020, and during the 2020 Citizen Review Committee portion of the Scottsdale General Plan update process

# CITY COUNCIL REPORT



Meeting Date: June 16, 2020  
 General Plan Element: *Community Involvement*  
 General Plan Goal: *Seek early and ongoing involvement in policy-making discussions*

## ACTION

Adopt **Resolution No 11851** (Attachment 1) to establish written and adoption procedures for the Scottsdale General Plan update process, in compliance with State Statute requirements.

## BACKGROUND

### General Plan Update

Per State Statute requirements, the City of Scottsdale has begun a General Plan update process. As part of the General Plan public outreach and adoption process, the governing body of the municipality "shall adopt written procedures to provide effective, early and continuous public participation in the development and major amendment of general plans..." per ARS 9-461.06 (C)1.

## ANALYSIS & ASSESSMENT

### General Plan Update Process Written and Adoption Procedures

General Plan Process Written and Adoption Procedures direct the city Planning Agency and community as to how effective, early and continuous public participation, adoption and ratification processes associated with the development of the City of Scottsdale's General Plan will occur, as required by State Statute.

Incorporated as Exhibit 1 to Resolution # 11851 (Attachment 1), the Scottsdale General Plan Update Written and Adoption Procedures, detail the requirements and timing associated with Scottsdale's public outreach, adoption and ratification processes intended for the current General Plan update that is underway.

### General Plan Update Adoption/Ratification Calendar

It is anticipated that the process for adopting an updated Scottsdale General Plan will adhere to the following calendar:

#### January – December 2020

- Establish a General Plan Update Citizen Review Committee
- The Citizen Review Committee (CRC) will use the Draft Scottsdale General Plan 2035 as the baseline plan that will be reviewed as part of the update process
- CRC will review and make recommendations on the Draft 2035 Plan per public meeting format

APPROVED ON CONSENT

## City Council Report | General Plan Update Written and Adoption Procedures

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- The CRC will release a Final Draft 2035 Plan for community, Planning Commission and City Council consideration and the Final Draft Plan will be subject to State Statute public participation and public meetings requirements

### January – July 2021

Review of Final Draft Plan per the following:

- Review/discussions with city boards and commissions
- Community Open Houses
- Continued discussions with community members/groups city-wide
- Study Session with Planning Commission
- State Statute required Remote Planning Commission Hearing
- State Statute required City Council Work Study Session
- State Statute required Planning Commission Recommendation Hearing
- State Statute required City Council Adoption Hearing/Call for Election

### November 2021

- Election Day – Voters Consider General Plan Ratification per State Statute Requirements

## STAFF RECOMMENDATION

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### **Recommended Approach**

Staff recommends that the Council adopt **Resolution No 11851** (Attachment 1 and associated exhibits) to revise the Scottsdale General Plan Update Written and Adoption Procedures to remain in continued compliance with State Statute requirements associated with the development of General Plans in Arizona.

### **Proposed Next Steps**

- A series of bimonthly public meetings through December 2020, regarding the Citizen Review Committee's update of the Draft 2035 General Plan.

## RESPONSIBLE DEPARTMENT(S)

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Community & Economic Development Division  
Planning and Development Services Department – Long Range Planning

## STAFF CONTACTS (S)

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Erin Perreault, AICP  
Director  
Long Range Planning  
480.312.7093  
[Eperreault@scottsdaleaz.gov](mailto:Eperreault@scottsdaleaz.gov)

City Council Report | General Plan Update Written and Adoption Procedures

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APPROVED BY

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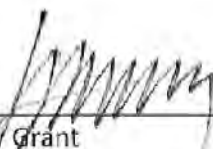


Erin Perreault, AICP, Long Range Planning Director

(480) 312-7093, [eperreault@scottsdaleaz.gov](mailto:eperreault@scottsdaleaz.gov)

06/01/2020

Date



Randy Grant

Planning + Development Services Executive Director

(480) 312-2664, [rgrant@scottsdaleaz.gov](mailto:rgrant@scottsdaleaz.gov)

6/1/20

Date

ATTACHMENTS

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1. Resolution No. 11851 establishing updated, written and adoption procedures for the Scottsdale General Plan Update  
Exhibit 1: Scottsdale General Plan Update Written and Adoption Procedures
2. Proposed General Plan Update State Statute Required Public Participation, Public Meetings, and Adoption/Ratification Calendar

RESOLUTION NO. 11851

A RESOLUTION OF THE CITY OF SCOTTSDALE, MARICOPA COUNTY, ARIZONA, REPEALING THE GENERAL PLAN PROCESS WRITTEN PROCEDURES ADOPTED BY RESOLUTION 10100 AND ADOPTING NEW SCOTTSDALE GENERAL PLAN UPDATE ADOPTION PROCEDURES.

WHEREAS, pursuant to Article 1, Section 3 of the City Charter, the City of Scottsdale has the power to adopt a comprehensive General Plan as provided by Arizona law, regarding the future physical development of the city to serve as a guide to all future council action concerning land use regulations and expenditures for capital improvements; and

WHEREAS, pursuant to Arizona Revised Statutes Sections 9-461.05 through 9-461.06, the City of Scottsdale is required to adopt and ratify a comprehensive, long-range general plan every ten years; and

WHEREAS, the first attempt to update the Scottsdale General Plan 2001 in March 2012 was not ratified by the voters; and

WHEREAS, the second attempt to update the Scottsdale General Plan 2001 resulted in a draft General Plan 2035 being released for city board, commission and public review, but that has yet to be vetted through the state statute public hearing and voter ratification process; and

WHEREAS, sound decisions concerning the City's General Plan can best be made with broad public participation in developing the General Plan; and

WHEREAS, the City Council approved Resolution No. 9393 to adopt written procedures to provide effective, early and continuous public participation in the development and adoption of the City's new General Plan; and

WHEREAS, the Process Notification and Public Participation procedures approved in Resolution No. 9393 to provide effective, early and continuous public participation in the development of the City's new General Plan have been completed, thus it is in the best interest of the city to repeal and adopt new General Plan update adoption procedures;

WHEREAS, the City Council repealed Resolution No. 9393 and approved Resolution No. 10100 on May 5, 2015, to adopt new General Plan update adoption procedures; and

WHEREAS, on March 18, 2020 Mayor W. J. "Jim" Lane issued a declaration of emergency in Scottsdale as part of the city's ongoing response to the COVID-19 pandemic to encourage social distancing in order to mitigate the effects and spread of the virus, thus in-person meetings for city boards, commissions and other public entities were canceled or replaced with a virtual online public meeting format; and

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Scottsdale, Maricopa County, Arizona, as follows:

Section 1. That the City Council hereby repeals the General Plan Process Written Procedures adopted by Resolution 10100 and adopts procedures entitled Scottsdale General Plan Adoption Procedures, as set forth in Exhibit 1, attached hereto and incorporated herein by reference.

Section 2. That the Planning Agency is directed to prepare a document entitled the General Plan Adoption Calendar as set forth in New Scottsdale General Plan Adoption Procedures.

Section 3. That copies of the New Scottsdale General Plan Adoption Procedures and the General Plan Adoption Calendar shall be kept up-to-date and on file in the Office of the Clerk, located at 3939 N Drinkwater Boulevard, Scottsdale, Arizona and posted on the city's website.

PASSED AND ADOPTED by the Council of the City of Scottsdale, Maricopa County, Arizona this \_\_\_\_\_ day of \_\_\_\_\_, 2020.

ATTEST:

CITY OF SCOTTSDALE,  
an Arizona municipal corporation

By: \_\_\_\_\_  
Carolyn Jagger, City Clerk

By: \_\_\_\_\_  
W. J. "Jim" Lane, Mayor

APPROVED AS TO FORM:  
OFFICE OF THE CITY ATTORNEY

Michael Hamblin  
Sherry R. Scott, City Attorney  
By: Michael Hamblin, Assistant City Attorney



## **SCOTTSDALE GENERAL PLAN UPDATE ADOPTION PROCEDURES**

In accordance with Arizona Revised Statutes (9-461.06), Scottsdale's General Plan shall be adopted in accordance with the Scottsdale City Council's established General Plan Process Written Procedures as provided below.

### **General Plan Public Outreach**

To provide effective, early and continuous public participation in the development of General Plans, the City's Planning Agency will provide the following:

- The broad dissemination of proposals and alternatives;
- The opportunity for written comments;
- Public hearings after effective notice;
- Open discussions, communications programs and information services both in-person and online; and
- Consideration of public comments.

A General Plan Citizen Review Committee 2020 will be formed and charged between April and December 2020 with reviewing and possibly modifying the draft 2035 General Plan for public, Planning Commission and City Council consideration. The first two meetings of the General Plan Citizen Review Committee 2020, scheduled in April, will be hosted as virtual, online public meetings, and will continue with the online format until such time that the declaration of emergency in response to the COVID-19 pandemic is lifted in Scottsdale and boards, commissions and other entities can return to meeting in public, in-person formats.

A written summary report of citizen and stakeholder public participation input will be prepared for General Plan Citizen Review Committee 2020, Planning Commission and City Council consideration and will highlight the following:

- Suggestions/Comments received;
- Comment source (when available);
- General Plan content reference; and
- Actions on suggestions/comments.

### **General Plan Adoption**

When a draft General Plan has been prepared for Planning Commission consideration, the document shall be broadly distributed, including:

- Legal notice in the newspaper of record;
- Copies made available at the City Clerk and Long Range Planning offices;
- Placing the General Plan document on the city's website, social media, and other electronic public information outlets.

### **EXHIBIT 1**

At least sixty (60) days before the General Plan is noticed, the City's Planning Agency shall transmit the proposal to the Planning Commission, and shall submit a copy for review and further comment to:

- The Planning Agency of the county in which the municipality is located;
- Each county or municipality that is contiguous to the corporate limits of the municipality or its area of extraterritorial jurisdiction;
- The regional. planning agency within which the municipality is located;
- The Arizona Commerce Authority or any other state agency that is subsequently designated as the general planning agency for the state;
- The Department of Water Resources for review and comment on the water resources element, if a water resources element is required: and
- Any person or entity that requests in writing to receive a review copy of the proposal.

Additionally, the City will consult with, advise and provide an opportunity for official comment by public officials and agencies, Maricopa County, school districts, associations of governments, public land management agencies, other appropriate government jurisdictions, public utility companies, civic, educational, professional and other organizations, property owners, citizens, and stakeholders generally to secure maximum coordination of plans and to indicate properly located sites for all public purposes on the General Plan.

The following represents the adoption requirements for a new General Plan:

- The General Plan Citizen Review Committee 2020 will create and deliver a recommended draft General Plan to the Scottsdale Planning Commission.
- The processing of a new General Plan for adoption by the City Council will consist of at least six months.
- The City of Scottsdale's Planning Commission shall hold at least two (2) or more public hearings at different locations within the municipality (known as Remote and Recommendation Hearings) to promote citizen participation. Notice of the time and place of a hearing and availability of studies and summaries related to the hearing shall be given at least fifteen (15) and not more than thirty (30) calendar days before the hearing by:
  - o Publication at least once in a newspaper of general circulation published or circulated in the municipality;
  - o Such other manner in addition to publication as the municipality may deem necessary or desirable; and
  - o Recommendation action by the Planning Commission on the new General Plan shall be transmitted to the City Council.

**EXHIBIT 1**

- City Council Required Hearings:
  - Before adopting a new General Plan, the City Council shall hold at least one public hearing (study session);
  - To adopt a new General Plan requires a resolution of the City Council, and the City Council shall hold a separate public hearing (Adoption Hearing) for that purpose; and
  - Both hearings will require that notice shall be given at least fifteen (15) and not more than thirty (30) calendar days before the hearing in the time and manner provided for the giving of notice of the hearing.
- The adoption of a new General Plan shall be approved by affirmative vote of at least two-thirds (2/3) of the Scottsdale City Council.
- The City's Planning Agency shall create a new General Plan Adoption Calendar for important public hearings and public outreach meetings associated with the new General Plan. The Planning Agency may make modifications to the General Plan Adoption Calendar, when necessary, provided the public is given reasonable notice of important public hearings and public outreach meetings as required, and the Planning Agency keeps the General Plan Adoption Calendar up to date, on file in the Office of the City Clerk and posted on the City's website.

### **General Plan Ratification**

The following represents the ratification requirements for a newly adopted General Plan:

- The City Council shall submit the new General Plan to the voters for ratification at the next regularly scheduled municipal election or at a special election scheduled at least one hundred twenty (120) days after the governing body adopts the new General Plan.
- During the one hundred twenty (120) day period between City Council adoption and the election at which the General Plan will be considered for ratification by the voters, the City will provide educational public outreach and materials to Scottsdale residents and stakeholders regarding the new General Plan content.
- Additionally, the City Council shall include a general description of the new General Plan and its elements in the municipal election pamphlet and shall provide public copies of the new General Plan in at least two locations that are easily accessible to the public and include posting of the new General Plan on the municipality's official internet website.
- If a majority of the qualified electors voting on the proposition approves the new General Plan, it shall become effective as provided by law. If a majority of the qualified electors voting on the proposition fails to approve the new General Plan, the current General Plan remains in effect until a new General Plan is approved by the voters.

### **EXHIBIT 1**

**Procedures Specific to the new General Plan Process**

The Planning Agency shall consider recommending that all of the General Plan amendments to the current City of Scottsdale General Plan 2001, that the City Council approves before the new Scottsdale General Plan is adopted, also be included in the new General Plan before it is adopted, when it is reasonably practical to do so. Any amendment the City Council approves to the Scottsdale General Plan 2001 which is not included in the new General Plan prior to its adoption or which is considered after its adoption will be automatically nullified should the voters ratify the new Scottsdale General Plan. Any previously approved General Plan amendment that is automatically nullified by the voters' ratification of the new General Plan may be reconsidered by the Planning Agency and the Scottsdale City Council for incorporation into the newly ratified General Plan.

**Proposed General Plan Update (November 2021 General Election)**

<b>DATE</b>	<b>PUBLIC BODY/TOPIC</b>	<b>Community Outreach</b>
January 2021	<b>60-Day Notice</b> to other jurisdictions – 60 days before 15-day notice of Planning Commission meeting (state requirement)/Plan transmitted to City Council/Planning Commission	Letter sent to interested parties
February 2021	<b>Community Open Houses/Public Participation</b> Planning Commission Study Sessions	Public Testimony
March 2021	Planning Commission Study Sessions	Public Testimony
March 2021	<b>Remote Planning Commission Hearing</b> – no commission action/public and commission comments collected	Public Testimony
April 2021	<b>City Council Work Study Session</b> – no action, discussion of plan content, limited public input	Limited Public Testimony
May 2021	<b>Planning Commission Hearing</b> – Planning Commission recommendation to City Council	Public Testimony
June 2021	<b>City Council Adoption Hearing/Possible Call for Election</b> – consider adoption of plan; if adopted, Council action to establish public election for ratification	Public Testimony
July 2021	Starts 120-day period before election (state requirement)	Educational information provided to public about election items*
CLERK	Final Ballot language due to County	
CLERK	Ballot Pamphlet Language & Argument Letters (pro/con) due	
November	<b>General Plan Election Day</b>	

\* Continual education/ public participation about the General Plan throughout (e.g. flyers, posters, web, tv, social media)

**ALL DATES ASSOCIATED WITH ELECTION TIMING TO BE CONFIRMED WITH CITY CLERK**

## Enclosure 2

# MEMBERS

<b>TITLE</b>	<b>NAME</b>	<b>BOARD OR COMMISSION</b>	<b>PRIMARY OR ALTERNATE</b>
Chair	Steve Dodd	McDowell Sonoran Preserve Commission	Primary
Vice Chair	Cynthia Wenstrom	McDowell Sonoran Preserve Commission	Alternate
Chair	Blair Schweiger	Historic Preservation Commission	Primary
Vice Chair	Regina Buzzello	Historic Preservation Commission	Term Ended 10/20/2020
Chair	Janice Shimokubo	Human Relations Commission	Primary
Vice Chair	Emily Hinchman	Human Relations Commission	Alternate
Commissioner	Richard Newman	Tourism Development Commission	Primary
Chair	Larry Bernosky	Airport Advisory Commission	Primary
Vice Chair	John Berry	Airport Advisory Commission	Alternate
Commissioner	Ryan Johnson	Scottsdale Environmental Advisory Commission	Primary
Vice Chair	Tammy Bosse	Scottsdale Environmental Advisory Commission	Alternate
Vice Chair	Sheila Reyman	Library Board	Primary
Vice Chair	Kurt Jones	Parks and Recreation Commission	Primary
Vice Chair	William Scarbrough	Development Review Board	Resigned 10/6/2020
Board Member	Shakir Gushgari	Development Review Board	Primary (effective 10/15/2020)
Vice Chair	Pamela Iacovo	Transportation Commission	Primary
President	Shannon Scutari	Industrial Development Authority	Resigned 8/27/2020
President	Eloy Yndigoyen	Industrial Development Authority	Primary (effective 9/1/2020)
Vice Chair	Ross Cromarty	Neighborhood Advisory Commission	Resigned 8/20/2020
Commissioner	Carol Miraldi	Neighborhood Advisory Commission	Primary (effective 8/20/2020)
Chair	Janice Eng	Human Services Commission	Primary
Vice Chair	Denny Brown	Human Services Commission	Alternate



## LAND USE ELEMENT<sup>‡</sup>

Scottsdale has evolved and grown since its founding in the late 1800's and incorporation in 1951. Starting as a small agricultural community sprinkled with farms and citrus groves, Scottsdale has experienced growth cycles, annexations of large unincorporated lands, preservation of natural environment, and expanded resort, tourism, and cultural facilities. Today, Scottsdale features a variety of land uses that contribute to a diverse economy, unique community character, and a national reputation for quality and livability.

### **Resorts and Tourism**

Scottsdale is the major resort center and tourist destination of the metropolitan area. Although not all major resorts are located in Scottsdale, it has core specialty shopping, art galleries, and recreational facilities. Many of the region's premier cultural and sporting events, which attract and sustain the local tourism industry, are also found in Scottsdale. Preserving the quality of the city's visual and physical environments is an important part of maintaining this industry.

### **Desert Preservation Efforts**

The tremendous growth cycle of the mid to late 1990's led to a strong community desire to preserve large expanses of Sonoran Desert, particularly in and around the McDowell Mountains. Since that time, Scottsdale has emerged as a leader in open space preservation with the commitment to preserving one-third of its landmass in a natural preserve

(Scottsdale McDowell Sonoran Preserve), linking the McDowell Mountains with McDowell Mountain Regional Park and Fountain Hills McDowell Mountain Preserve to the east, the Tonto National Forest to the north, and the City of Phoenix open space networks to the west. Scottsdale has also required desert-sensitive design throughout the community through Scottsdale's Adopted Sensitive Design Principles, and the Environmentally Sensitive Lands Ordinance (ESLO), which strive to protect the Sonoran Desert on private, developable lands.

*[Cross-reference Open Space Element]*

### **Regional Employment Center**

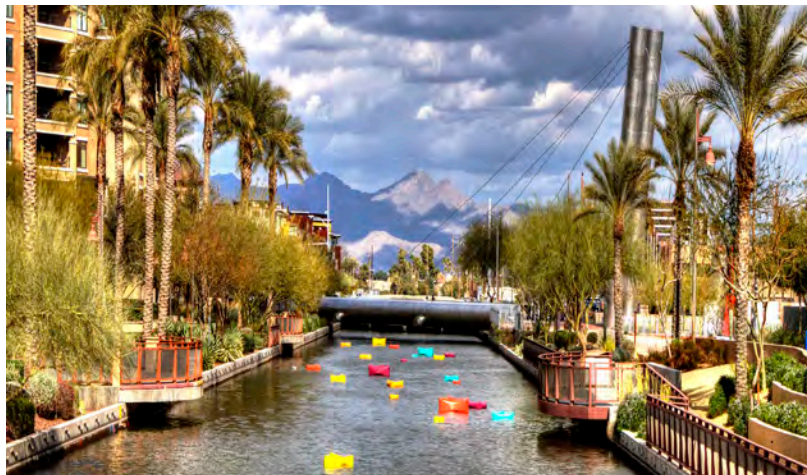
In 2002, construction of the Loop 101/Pima Freeway was finished, and stimulated a concentrated mixed-use land use pattern near the Scottsdale Airport. This area, more commonly known as the Greater Airport Character Area, has emerged as one of the most significant employment and business cores within the State of Arizona. These regional land uses, along with the city's land preservation efforts, have contributed to Scottsdale's economy, which in turn contributes to a high quality of life for residents.

### **Character-Based General Planning**

The General Plan Land Use Element, as with all the elements of the General Plan, incorporates broad goals, policies, and general direction for the entire city. More detailed planning is found at the Character Area and Neighborhood Planning levels. In some cases, specific land use considerations may be better addressed at the Character Area Plan and/or Neighborhood Plan levels rather than through the citywide General Plan. *[Cross-reference Character & Design Element]*

### **Land Use Patterns**

Land use patterns **should CAN** help conserve natural resources, **reduce the dependence on the automobile PROVIDE FOR ALTERNATIVE MOBILITY OPTIONS**, alleviate traffic congestion, contribute to the character of the community, support Scottsdale's tourism industry, and adequately serve the needs of citizens. Land use decisions **must INCLUDE** **considerATION OF** the relationships of adjacent land uses and sensitively integrate proposed land uses with existing natural and physical environments.





Scottsdale's Future Land Uses are distributed as follows:

GENERAL PLAN 2035 LAND USE MIXTURE					
Land Use Group	Land Use Category	General Density	Percentage*	Existing Acreage*	Total Percentage of Group
Residential	Rural Neighborhoods	1 dwelling unit per 1 or more acres	<del>26%</del> 27%	<del>28,586</del> <del>28,370</del> 30,981	51.48%
	Suburban Neighborhoods	More than 1 and less than 8 dwellings units per acre	23.20%	<del>20,029</del> <del>21,750</del> 27,181	
	Urban Neighborhoods	8 or more dwellings units per acre	2%	<del>2,168</del> 60 2,545	
Mixed Uses	Mixed-Use Neighborhoods		3%	<del>3,363</del> 61 4,081	5%
	Resorts/Tourism		21%	<del>1,680</del> 175 1,870	
Non-Residential	Cultural/Institutional or Public Use		<del>3.25%</del>	<del>2,737</del> 584 3,254	76.5%
	Commercial		1%	<del>1,079</del> 118 1,305	
	Employment		3%	<del>2,811</del> 959 3,205	
Open Space	Developed Open Space		7.5%	<del>8,097</del> <del>7,679</del> 8,431	37.40.52%
	Natural Open Space		<del>30.33%</del> 7.5%	<del>35,261</del> 8,061 35,244	
	McDowell Sonoran Preserve		27%	29,262	

\*ACREAGE IS APPROXIMATE

To maintain a healthy economic base and provide services for our SCOTTSDALE'S residents, businesses, and visitors, the community should provide a diversity of land uses<sup>‡</sup> and supporting infrastructure. The Land Use Element and its map indicate the general distribution, location, type, and relationship of all land uses<sup>‡</sup>. The Zoning Ordinance carries out the goals and policies of the Land Use Element by establishing the legal parameters for the development of a parcel of land.

Under Arizona Revised Statute 9-461.05 C1(g), the city must identify any sources of aggregates from state agency maps. No aggregates have been identified in Scottsdale, and therefore, no policies related to aggregates have been included in the Land Use Element.<sup>‡</sup>

The goals and policies of the Land Use Element reflect careful consideration for the locations of defined land uses and delineate the criteria that should be IS ARE carefully considered when contemplating a change in Land Use Category.

[‡ = State-required]

POLICY LU 1.3 MOVED TO THE NEW TOURISM ELEMENT AS REQUESTED BY THE CRC AT THEIR MAY 18, 2020 MEETING.

## Goals and Policies

### GOAL LU 1

Enhance Scottsdale's economic viability by encouraging ~~Encourage~~ land uses that ~~contribute to economic viability and community character to sustain and enhance Scottsdale's role as a major~~ reinforce the city's reputation as the premier southwestern tourist destination, cultural center, and economic hub, and are compatible with ~~Scottsdale's brand, character, and physical appearance. regional economic hub, tourist destination, and cultural center.~~

#### Policies

**LU 1.1** Encourage land uses that preserve a high quality of life and further define Scottsdale's sense of place within the region.

**LU 1.2** Celebrate Scottsdale's ~~identity as a~~ desert city **IMAGE** by preserving natural open space and natural ecosystems. *[Cross-reference Open Space; Environmental Planning; and Conservation Elements]*

~~LU 1.3~~ — Promote ~~destination resorts in conjunction with recreation, retail, housing, and cultural uses that support tourism and sustain a resort-like lifestyle.~~ *[Cross-reference Economic Vitality; Healthy Community; and Arts, Culture & Creative Community Elements]*

**LU 1.43** Promote development patterns that integrate with and ~~complement surrounding uses and~~ reinforce ~~an area's~~ **THE character OF AN AREA, OR ESTABLISH NEW CHARACTER WHERE DEVELOPMENT PATTERNS ARE FRAGMENTED OR EVOLVING.** *[Cross-reference Character & Design and Neighborhood Preservation & Revitalization Elements]*

**LU 1.54** Work with adjacent jurisdictions to understand the dynamics of, **AND COLLABORATE ON,** emerging and redeveloping areas ~~on the borders of Scottsdale~~ **ADJACENT TO THE CITY BOUNDARY.**



## GOAL LU 2

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**Sensitively transition and integrate land uses with the surrounding natural and built environments.**

### *Policies*

**LU 2.1** Ensure neighborhood “edges” transition to one another through compatible land uses and development patterns. *[Cross-reference Character & Design; Neighborhood Preservation & Revitalization; Circulation; and Bicycling Elements]*

**LU 2.2** Sensitively integrate low-density development patterns and land uses bordering the Scottsdale McDowell Sonoran Preserve and other sensitive natural features. *[Cross-reference Open Space Element]*

**LU 2.3** Locate employment and major non-residential uses along major transportation networks to limit impacts on residential areas and provide citywide and regional access. *[Cross-reference Economic Vitality and Circulation Elements]*



**GOAL LU 3**

**Maintain a balance of land uses to support a high quality of life.**

*Policies*

**LU 3.1** Allow for the diversity and innovative development patterns of residential uses and supporting services to provide for the needs of the community. *[Cross-reference Housing Element]*

**LU 3.2** Integrate housing, employment, and supporting infrastructure, primarily in mixed-use neighborhoods and Growth and Activity Areas, to support a jobs/housing balance. *[Cross-reference Housing; Economic Vitality; and Growth Areas Elements]*

**LU 3.3 †** Maintain a citywide balance of land uses, and consider modifications to the land use mix to accommodate changes in community vision, demographic needs, and economic sustainability. *[Cross-reference COMMUNITY INVOLVEMENT, Housing, and Economic Vitality Elements; and, Vision and Values]*

**LU 3.4** Provide an interconnected, accessible open space system, which includes pedestrian and equestrian links, recreation areas, canals, and drainage ways. *[Cross-reference Open Space and Recreation Elements]*

**LU 3.5** ~~(NEW) ENGAGE THE COMMUNITY IN RESIDENTIAL AND NEIGHBORHOOD LAND USE DISCUSSIONS.~~



**GOAL LU 4**

Develop **context appropriate** land use patterns that **are compatible with and** support a variety of **compatible** mobility choices and services.

*Policies*

- LU 4.1** Integrate land uses and **mobility TRANSPORTATION** systems to allow for **shorter and fewer automobile trips and greater choices for A VARIETY OF mobility CHOICES**. *[Cross-reference Circulation Element]*
- LU 4.2 †** ~~Support mixed-use and compact development patterns, to p~~Provide opportunities for pedestrian-oriented development, reduced parking demand, and **support context appropriate** alternative mobility choices. *[Cross-reference Circulation Element]*
- ~~LU 4.3 — Promote transportation choices that respond to the community’s land use patterns and local neighborhood lifestyles. [Cross-reference Circulation Element]~~
- LU 4.43 †** Locate regional land uses and intensities in Growth and Activity Areas to provide the greatest access to alternative modes of transportation. *[Cross-reference Growth Areas and Circulation Elements]*

**GOAL LU 5 †**

Promote land use patterns that conserve resources, including land, clean air, water, and energy.

*Policies*

- LU 5.1 †** Encourage a variety of compatible mixed-use land uses within or next to Growth and Activity Areas, along major streets, and within particular Character Areas to **reduce automobile use and** improve air quality. *[Cross-reference Environmental Planning and Circulation Elements]*
- LU 5.2 †** Concentrate greater development intensities in Growth and Activity Areas, thereby reducing development pressures in low-density areas and conserving energy. *[Cross-reference Growth Areas and Energy Elements]*
- LU 5.3** Minimize environmental hazards and protect the natural character of the desert **by encouraging THROUGH** sensitive development on Environmentally Sensitive Lands. *[Cross-reference Character & Design; Open Space; and Conservation Elements]*

**GOAL LU 6 (NEW)**

Attract and retain diverse, ~~high-quality~~ employment, business, and retail land uses to improve the economic well-being of Scottsdale's residents.

*Policies*

- LU 6.1** (NEW) Promote opportunities for the expansion and revitalization of employment and commercial uses within the city. *[Cross-reference Conservation, Rehabilitation, & Redevelopment Element]*
- LU 6.2** (NEW) Support well-planned, clustered ~~employment centers of healthcare and research and development related or similar uses such as Healthcare and Research and Development~~ land uses. *[Cross-reference Healthy Community and Economic Vitality Elements]*
- LU 6.3** (NEW) Encourage commercial land uses ~~of similar scale and character~~ of similar scale and character ~~next in proximity~~ to or within medium- to high-density residential areas to promote walkable connections. *[Cross-reference Neighborhood Preservation & Revitalization Element]*

**GOAL LU 7 (NEW)**

Protect the viability of the Scottsdale Airport by encouraging compatible land uses and development types in the surrounding area.

*Policies*

- ~~LU 7.1~~ (NEW) ~~Promote safe operation of the Scottsdale Airport by discouraging uses that will impede safe flight operations or negatively impact airport funding sources.~~ *[Cross-reference Safety; Economic Vitality; and Circulation Elements]*
- LU 7.21** (NEW) Maintain and ~~implement~~ FOLLOW the Airport Part 150 Noise Compatibility Program, and encourage disclosure of the Scottsdale Airport's location according to the Airport Influence Area and Noise Contour Maps. *[Cross-reference Character & Design; Safety; Economic Vitality; and Circulation Elements]*
- LU 7.32** (NEW) Maintain Runway Protection Zones next to the Scottsdale Airport. *[Cross-reference Safety; Economic Vitality; and Circulation Elements]*
- LU 7.43** (NEW) Support aviation-related economic development opportunities and land uses near the Scottsdale Airport. *[Cross-reference Economic Vitality Element; and Greater Airpark Character Area Plan]*

## FUTURE LAND USE MAP DESIGNATIONS ‡

The Land Use Element encourages orderly and efficient distribution of land uses in the city. A full range and mix of land uses, including rural, suburban, and urban neighborhoods, employment, commercial, recreational, cultural, and preservation areas are provided in the Element.

“Neighborhood” Land Use Categories are mostly residential classifications. These land uses accommodate a mix of dwelling types and densities for a variety of neighborhood and environmental conditions. Other uses are included that support the residential land uses, such as shopping and small business. Sometimes, the city does not control the location of special uses, such as schools, places of worship, or major transmission lines. State and Federal governments can also preempt local land use authority. However, the city can work with other jurisdictions and agencies on land use decisions. All development, regardless of jurisdiction, ~~should CAN take special care to ARE is~~ **ENCOURAGED TO** provide adequate transitions and avoid impacts between uses with different functions and intensities.

Whenever non-residential uses are next to established or planned residential areas, special care ~~must be~~ **IS** taken to ensure privacy and to protect personal property. Residential buffering and transition techniques include increased setbacks, landscaping, restricted land uses, diversion of traffic, controlled noise or light, height limitations, and transitional land uses (e.g. offices, medium-high density residential uses). Mixed-use areas can still provide a combination of housing and non-residential uses while respecting privacy and personal property rights.



[‡ = State-required]

### Residential Land Use Categories:

- **RURAL NEIGHBORHOODS:** This category includes large-lot, single-family neighborhoods. Densities in Rural Neighborhoods are usually one **house dwelling unit** per one or more acres. **Within the Environmentally Sensitive Lands area, Nn**ative desert vegetation is abundant ~~in many Rural Neighborhoods~~, and special care is needed to preserve open desert character and environmental features. **The goal in Rural Neighborhoods is to retain the large-lot character while preserving** ~~Clustering is encouraged to preserve-~~ desert vegetation, washes, and natural features. **Clustering may be used to achieve this goal.** Rural Neighborhoods may also include equestrian or golf course **uses**character.
- **SUBURBAN NEIGHBORHOODS:** This category includes medium- to small-lot single-family neighborhoods or subdivisions. Densities in Suburban Neighborhoods are usually more than one **house dwelling unit** per acre, but less than eight **houses dwelling units** per acre. This category also includes some townhouses and small-lot single-family homes, such as patio homes. Suburban Neighborhoods may be used as transitions among less intense areas, Urban Neighborhoods, and non-residential uses. ~~The terrain should be relatively flat or gently sloping, to accommodate this density.~~
- **URBAN NEIGHBORHOODS:** This category generally includes multi-family housing. Densities in Urban Neighborhoods are usually eight or more dwellings **units** per acre. Urban Neighborhoods are generally located near retail centers, offices, or other compatible non-residential uses, serving as transitional land uses between lower-density residential areas and higher intensity non-residential areas. The design of developments in this category **must** take **S** care to provide adequate circulation to accommodate traffic volumes, **and** provide access to transportation choices (e.g. pedestrian, bicycle, transit) **AND BUFFER LESS DENSE RESIDENTIAL FROM HIGHLY TRAVELED ROADWAYS.** ~~The terrain should be relatively flat or gently sloping to accommodate this density.~~

### Mixed-Use Land Use Categories:

- **MIXED-USE NEIGHBORHOODS:** Mixed-Use Neighborhoods focus on human-scale development and are located in areas with strong access to multiple modes of transportation and major regional services. These areas accommodate higher-density housing combined with complementary office or retail uses. Mixed-Use Neighborhoods are most suitable near and within Growth and Activity Areas. Mixed-Use Neighborhoods may be non-residential in the Greater Airpark Character Area. *[Cross-reference Greater Airpark Character Area Plan]*
- **RESORTS/TOURISM:** This category includes hotels and a variety of resorts, which can be freestanding, part of a resort community, or part of a master-planned development. Resort locations capitalize on the natural amenities and the leisure or shopping opportunities of the area. Hotels or resorts often include ancillary retail, cultural, and recreational uses. In some areas of the community, lower-density may be needed to preserve desert character, environmental features, land slopes, or other physical constraints. Medium-density housing with a resort character and lifestyle may integrate into resort uses and may share resort amenities. Resort uses are often placed next to open space, as a key resort feature.



### Non-Residential Land Use Categories:

- **COMMERCIAL:** Commercial uses provide a variety of goods and services to the people who live, work, or visit Scottsdale and have been designated throughout the community at various locations. Neighborhood-serving commercial uses ~~should be~~ **ARE TYPICALLY encouraged to be** located at frequent intervals in relationship to the density of nearby housing to reduce travel time and distance. ~~The size and scale of neighborhood business centers should be compatible with surrounding areas.~~ **The size and scale of neighborhood business centers are encouraged to be compatible with surrounding areas.** Neighborhood-serving commercial uses are best located on collector or arterial streets. Community- and regional-serving commercial uses ~~should be~~ **ARE TYPICALLY encouraged to be** located on arterial streets for high visibility and traffic volumes. Community and regional commercial uses work best when they are integrated with a mix of uses. In Suburban and Urban Character Types, mid-range and limited-service hotel accommodations may be appropriate.
- **EMPLOYMENT:** The employment category permits a range of employment uses from light industrial to office uses. There are two types of Employment Land Uses:

  - **LIGHT INDUSTRIAL/OFFICE:** The Employment-Light Industrial/Office areas provide a variety of employment opportunities, business enterprises, aviation uses [see Greater Airpark Character Area Plan], light manufacturing, warehousing, and other light industrial and heavy commercial type activities. Light Industrial/Office uses ~~should be~~ **ARE** located and designed to limit impacts on and access to residential neighborhoods. These areas ~~should~~ **also** have excellent access to labor pools and transportation networks. Major streets serving Light Industrial/Office areas ~~should~~ **TYPICALLY** accommodate truck traffic. Transit facilities and other multimodal options are also needed at significant employment centers to accommodate commuters and minimize traffic congestion.
  - **OFFICE:** The Employment-Office Land Use Category includes a variety of office uses. Office uses that embody residential scale and character, and generate low to moderate traffic volumes, may be located along collector or arterial streets or next to residential areas. When next to residential land uses, **OFFICE** design ~~of offices should~~ incorporate elements that minimize negative impacts on the neighborhood. Offices needing strong transportation access to accommodate citywide and regional labor pools ~~should be~~ **ARE** generally located in and around Growth and Activity Areas and in areas with arterial street or freeway access.
- **CULTURAL/INSTITUTIONAL OR PUBLIC USE:** This category includes a variety of public and private facilities including, but not limited to, government buildings, schools, **museums**, private and public utilities, the Scottsdale Airport, and Taliesin West.

### Open Space Land Use Categories:

- **DEVELOPED OPEN SPACE:** The Developed Open Space Category includes public or private recreation areas including, but not limited to, golf courses and city parks. Some Developed Open Space may also be used as drainage facilities for flood control. Developed Open Space amenities serve both residents and visitors. They may also provide links between neighborhoods and provide recreation in employment centers. Their design ~~should~~ **IS TYPICALLY encouraged to be** integrated with adjacent neighborhoods. Resort/Tourism and Cultural/Institutional and Public Uses are encouraged to locate next to Developed Open Space.
  
- **NATURAL OPEN SPACE:** The Natural Open Space Category applies to locations where significant environmental amenities or hazards may exist, including, mountainous terrain, steep slopes, boulders, flood hazard areas, and washes. Lands in **THE** Natural Open Space Category ~~should~~ **CAN ARE ENCOURAGED TO** remain as permanent **OR REVEGETATED** open space. This classification is often the result of rezoning actions where developers have agreed to leave part of a property in a natural condition in return for placing an agreed-upon intensity in a less environmentally sensitive area. The community ~~should~~ **continue** efforts to preserve mountainous areas, washes, and areas with native desert vegetation. Low-impact recreational activities may be suitable for some of these sensitive areas and may include hiking, equestrian, or mountain biking trails.
  - **MCDOWELL SONORAN PRESERVE:** ~~The Natural Open Space Category includes the McDowell Sonoran Preserve. The Preserve~~ ~~The McDowell Sonoran Preserve Category~~ consists of mountain and desert land included in the Scottsdale McDowell Sonoran Preserve. Preserve land may be publicly or privately owned. Privately owned land ~~should~~ **THAT** ~~about~~ **S** city-owned Preserve land ~~and~~ may be protected through a variety of means, including zoning action, dedicated easements, or other entitlements that limit use for conservation/preservation purposes. This land generally possesses outstanding scenic value; valuable wildlife habitat and migration routes; lush desert vegetation; significant environmental conditions, such as sensitive washes, riparian areas; mountain peaks and valleys; archaeological and historic sites; and opportunities for passive recreation in designated areas. Preserve land will remain as permanent open space with limited permanent construction. *[Cross-reference Open Space Element, which illustrates the Recommended Study Boundary (RSB) of the Scottsdale McDowell Sonoran Preserve]*

### **Other Land Use Categories and Map Designations:**

- **CIRCLE:** A Circle on the General Plan Future Land Use Map indicates an area planned for the land use(s) ~~indicated~~ INCLUDED in the Circle, and is subject to existing entitlements granted as a part of particular General Plan and/or Master Planned Community Case, specifically 4-GP-2002 (State Land) and 54-ZN-1989 et al (DC Ranch). The Land Use categories in the Circle may occur anywhere in the subject area (subject area is denoted on the Future Land Use Map with a dashed outline), per the approved case.
- **REGIONAL USE OVERLAY:** The Regional Use Overlay Category provides flexibility for land uses when it can be shown that new land uses are viable in serving a regional market. Regional uses include, but are not limited to, corporate office, region-serving retail, major medical, educational campus, community service facilities, tourism, and destination attractions. In determining whether proposed land uses are regional in nature, the city will consider whether the use has a regional draw, fulfills current economic development policies, enhances the employment core and the city's attractiveness to regional markets, benefits from good freeway access, and complements the established character for the area.
- **SHEA CORRIDOR OVERLAY:** The Shea Corridor Overlay applies to neighborhoods along Shea Boulevard. Within this area, specific goals, policies, and guidelines are in effect per the East Shea Area Plan/Shea Area Plan (1987/1993). Policies include:
  - Enhance and protect the existing residential areas while allowing flexibility in residential parcels having Shea frontage.
  - Allow employers offering uses such as medically related services, corporate headquarters, or hotel accommodations.
  - Neighborhood-level retail centers, which provide everyday goods and services, such as groceries, drug stores, and dry cleaning ~~should~~ occur within ~~the~~ neighborhoods, on arterial streets, and outside of the Shea Corridor Overlay so that convenient vehicular and pedestrian access can occur and local traffic will not need to use Shea Boulevard.
- **MAYO SUPPORT DISTRICT OVERLAY:** The Mayo Support District Overlay applies to the area surrounding the Mayo Clinic. Within this district, a flexible approach to locating support uses ~~should be~~ IS considered. Policies for this area are included in the East Shea Area Plan/Shea Area Plan (1987/1993).
- **INFILL INCENTIVE DISTRICT OVERLAY:** The Infill Incentive District Overlay Category applies to areas, which the City Council has determined meet the State Statute Requirements (ARS 9.499.10) for establishing formal Infill Incentive Districts and Plans, to encourage redevelopment of the area, and promote infill development<sup>‡</sup> through fee waivers, APPROPRIATE relief from development standards, and expedited zoning and plan processing. Formal Infill Incentive Districts require Infill Incentive District Plans to carry out the intent of the districts. (NEW)

[‡ = State-required]

## GENERAL PLAN AMENDMENT CRITERIA ‡

The community recognizes that a proposed change of land uses within any given part of the city may have a great impact on the citywide balance of land uses. The General Plan Land Use Element was written with full consideration of the appropriate character and balance of land uses throughout the city. This leads to a land use pattern that reflects the community’s vision, aspirations, values, and goals.

Arizona Revised Statutes require each city to establish criteria to determine if a proposed change to the General Plan Land Use Element qualifies as a ‘Major’ Amendment. Per State Statute, a ‘Major’ Amendment is defined as a “substantial alteration of the municipality’s land use mixture or balance, as established in the municipality’s existing General Plan Land Use Element.” For Scottsdale, an amendment to the General Plan shall be defined as a Major Amendment if it meets any one of the following criteria‡: (NEW)

### 1) Change in General Plan Land Use Category Criteria

A change in General Plan Land Use Category on the General Plan Future Land Use Map from one Category to another, as delineated in the following table:

CHANGE IN LAND USE CATEGORY					
From Category:		To Category:			
		A	B	C	D
<b>A</b>	Rural Neighborhoods	Minor	<b>Major</b>	<b>Major</b>	<b>Major</b>
	Natural Open Space				
	McDowell Sonoran Preserve* (NEW)				
<b>B</b>	Suburban Neighborhoods	Minor	Minor	<b>Major</b>	<b>Major</b>
	Developed Open Space				
	Cultural/Institutional or Public Use				
<b>C</b>	Urban Neighborhoods	<b>Major</b>	Minor	Minor	<b>Major</b>
	Resorts/Tourism				
<b>D</b>	Commercial	<b>Major</b>	<b>Major</b>	Minor	Minor
	Employment				
	Mixed-Use Neighborhoods				

\*Article 8 of the Scottsdale City Charter allows the City Council to remove the McDowell Sonoran Preserve designation from any parcel of land less than one (1) acre in area. Such removal is limited to a maximum of six (6) parcels within any one (1) calendar year. Such removal requires a two-thirds vote of all members of the City Council. Removal of the McDowell Sonoran Preserve designation from any larger parcel of land requires a two-thirds majority vote of all members of the City Council and ratification by the Scottsdale voters.

[‡ = State-required]

2) **Area of Change Criteria**

A change in General Plan Land Use Category of ten (10) or more gross acres.

3) **Character Area Criteria**

A change in General Plan Land Use Category that does not clearly meet the goals, **and** policies, **and strategies** embodied within an **approved adopted** Character Area Plan **or that changes an existing character area or boundary**.

Character Areas recognize and maintain the unique physical, visual, and functional conditions that occur in distinct areas across the community. The city recognizes that Character Areas form a context that is important to the lifestyle, economic well-being, and long-term viability of the community. The Character & Design Element identifies adopted Character Area Plans.

4) **Water/Wastewater Infrastructure Criteria**

A change in General Plan Land Use Category that results in the premature increase in the size of a master planned water transmission or sewer collection facility and that also requires public/community: a) investment for such facilities or b) physical provision of such facilities.

5) **Change to **the** Amendment Criteria and/or Land Use Category Definitions Criteria (NEW)**

A modification to the General Plan Amendment Criteria Section of the General Plan Land Use Element (**pages 76-79**) and/or a text change to the use, density, or intensity of the General Plan Land Use Category definitions (**pages 72-75**).

6) **Growth Area Criteria (NEW)**

A change in General Plan Land Use Category accompanied by a new or expanded Growth Area.

7) **General Plan Land Use Overlay Criteria (NEW)**

The modification **of, or** expansion of, **and/or creation of a new an existing** General Plan Land Use Overlay Category; (specifically regarding the Regional Use Overlay, Infill Incentive District Overlay, Shea Corridor Overlay, and Mayo Support District Overlay) **or the creation of a new General Plan Land Use Overlay Category**.

8) **Exceptions to the General Plan Amendment Criteria (CLARIFIED FROM 2001)**

Certain exceptions to the General Plan Amendment Criteria are considered in the best interest of the general public and in keeping with the vision, values, and goals of the community. The following exceptions to the General Plan Amendment Criteria ~~shall~~ **WILL** apply:

- ~~To facilitate completion of the Scottsdale McDowell Sonoran Preserve, any city-initiated change from any General Plan Land Use Category to the McDowell Sonoran Preserve Natural Open Space Land Use Category on city-OWNED OR-acquired parcels for INCLUSION IN the Scottsdale McDowell Sonoran Preserve will be exempt from a General Plan Amendment process. (NEW)~~

- An area designated by the Circle Land Use Category on the General Plan Future Land Use Map is determined to already be planned for the land use categories within the Circle Designation, per cases 4-GP-2002 (State Land) and 54-ZN-1989 et al (DC Ranch), and ~~therefore, an amendment to the General Plan is not required~~ **WILL BE PROCESSED AS A MINOR AMENDMENT. (CLARIFIED FROM 2001)**
- Regional uses [see Regional Use Overlay Category description] within the Regional Use Overlay area on the General Plan Future Land Use Map are determined as already planned land uses for that area and ~~not require amendment to the General Plan~~ **WILL BE PROCESSED AS A MINOR AMENDMENT. (CLARIFIED FROM 2001)**
- Proposed **LAND** uses **CHANGES** within the Shea Corridor Overlay and/or Mayo Support District Overlay areas ~~on the General Plan Future Land Use Map~~ that ~~clearly~~ **FULLY** meet the goals, policies, and guidelines of the East Shea Area Plan/Shea Area Plan (1987/1993) ~~are determined as already suitable uses within those areas and~~ will be ~~determined~~ **PROCESSED** as minor amendments. **(CLARIFIED FROM 2001)**
- If a project applicant wishes to appeal the designation of General Plan major amendment, the Planning and Development Director, or the position equivalent, will evaluate the appeal and make a major amendment **PROCESS** determination.
- **THE FOLLOWING ARE** exceptions to Criteria #2 - Area of Change - only:
  - A change greater than ten (10) gross acres from one General Plan Residential Land Use Category to another General Plan Residential Land Use Category of lesser density ~~shall~~ **WILL** be processed as a minor amendment.
  - ~~A change greater than ten (10) gross acres in General Plan Land Use Category will be processed as a minor amendment if it does not increase the planned number of dwellings units and includes at least 30% more Natural Area Open Space (NAOS) than required by the Environmentally Sensitive Lands Ordinance (ESLO), and if the open space:~~
    - ~~Is at least seven (7) or more contiguous gross acres, and designated as Natural Open Space on the General Plan Future Land Use Map, and~~
    - ~~Protects sensitive natural features and is visually and/or physically accessible to the general public, excluding land within Scenic Corridors or Vista Corridors.~~
  - A change greater than ten (10) gross acres in General Plan Land Use Category to Cultural/Institutional or Public Use with a municipal, ~~public school,~~ or non-profit cultural facility ~~will be processed as a minor amendment if the proposed facility is~~ **THAT IS** not adjacent to a Rural or Suburban Neighborhoods General Plan Land Use Category, or it does not share direct access to any street having single-family residential driveway access within one-half (1/2) mile of the proposal **WILL BE PROCESSED AS A MINOR AMENDMENT.**

### **General Plan Amendment Process † (CLARIFIED FROM 2001)**

All General Plan amendment proposals will be determined as major or minor according to Criteria 1-8. Both major and minor General Plan amendment processes include Planning Agency review, analysis, findings, and recommendations to the Planning Commission and City Council. If the proposal is located within the boundaries of an adopted Character Area Plan and/or Neighborhood Plan, the Planning Agency will conduct an analysis to see if the proposal supports the goals and policies within those plans. Both major and minor amendments require the City of Scottsdale to provide opportunity for public comment by public officials and agencies; Maricopa County; school districts; Maricopa Association of Governments (MAG); public land management agencies; public utility companies; civic, educational and professional organizations; citizens; and property owners.

### **MAJOR AMENDMENT† (CLARIFIED FROM 2001)**

State Statute allows the city to process major amendments to the General Plan only one time per year. Thus, all Major Amendment proposals must be submitted by a date each year, as decided by the Planning Agency. Major Amendments require that, 60-days before notice of the Planning Commission Remote Hearing, the Planning Agency shall transmit all Major Amendment proposals to the Planning Commission, City Council, Maricopa County, all contiguous municipalities to Scottsdale, Maricopa Association of Governments (MAG), Arizona Commerce Authority or designated State agency, State water resources department, and any person who requests copies of the proposals.

Major amendments to the General Plan require at least two (2) public hearings of the Planning Commission, a remote hearing and a regular hearing. The Planning Commission receives public comment, but does not take action at the remote hearing. Any Planning Commission recommendations made at the regular hearing will be transmitted to the City Council for consideration. (CLARIFIED FROM 2001)

Major Amendments may only be considered by the City Council at a single-hearing during the same calendar YEAR as submitted. Approval of a Major Amendment requires an affirmative vote of at least two-thirds of the members of the City Council. (CLARIFIED FROM 2001)

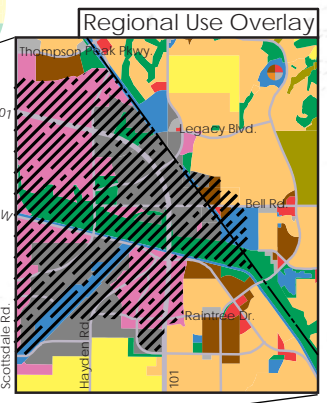
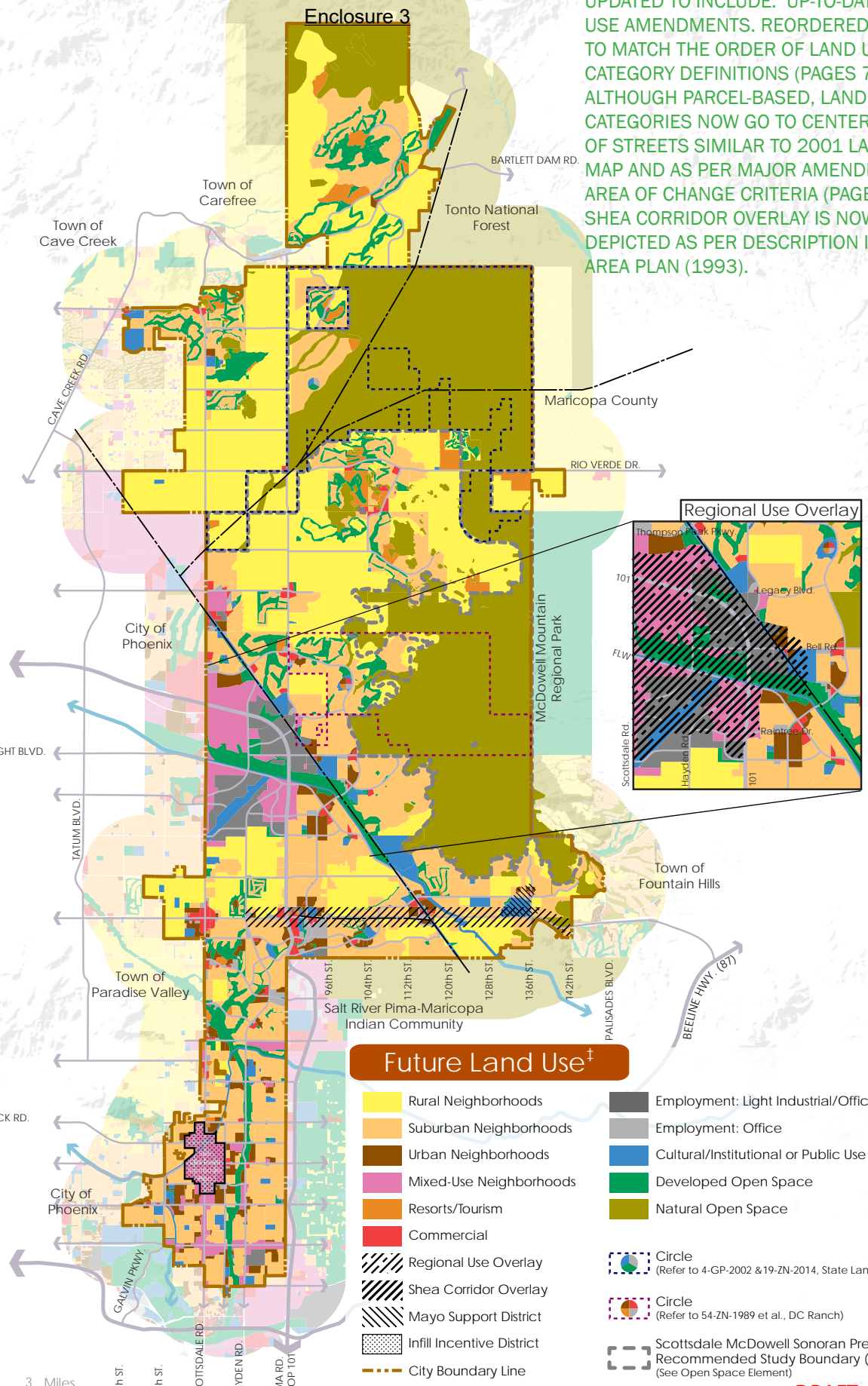
### **MINOR AMENDMENT**

Minor amendments applications are processed as they are received and can be heard any time during the year. Minor amendments do not require hearings in the same year as submitted. Minor amendments require at least one (1) public hearing of the Planning Commission and one (1) public hearing of the City Council. Approval of a minor amendment requires a simple majority vote of the City Council. (CLARIFIED FROM 2001)

[† = State-required]

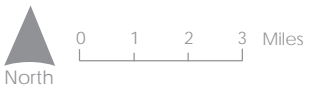
UPDATED TO INCLUDE: UP-TO-DATE LAND USE AMENDMENTS. REORDERED LEGEND TO MATCH THE ORDER OF LAND USE CATEGORY DEFINITIONS (PAGES 72 - 75). ALTHOUGH PARCEL-BASED, LAND USE CATEGORIES NOW GO TO CENTERLINE OF STREETS SIMILAR TO 2001 LAND USE MAP AND AS PER MAJOR AMENDMENT, AREA OF CHANGE CRITERIA (PAGE 77). SHEA CORRIDOR OVERLAY IS NOW DEPICTED AS PER DESCRIPTION IN SHEA AREA PLAN (1993).

JENNY LIN RD.  
 CIRCLE MOUNTAIN RD.  
 HONDA BOW RD.  
 ROCKAWAY HILLS RD.  
 DESERT HILLS DR.  
 JOY RANCH RD.  
 STAGECOACH PASS  
 CAREFREE HWY.  
 DOVE VALLEY RD.  
 LONE MOUNTAIN RD.  
 DIXILETA DR.  
 DYNAMITE BLVD.  
 JOMAX RD.  
 HAPPY VALLEY RD.  
 PINNACLE PEAK RD.  
 DEER VALLEY RD.  
 LOOP 101  
 UNION HILLS DR.  
 BELL RD./FRANK LLOYD WRIGHT BLVD.  
 GREENWAY PKWY.  
 THUNDERBIRD RD.  
 CACTUS RD.  
 SHEA BLVD.  
 DOUBLETREE RANCH RD.  
 McCORMICK PKWY.  
 INDIAN BEND RD.  
 LINCOLN DR.  
 McDONALD DR.  
 CHAPARRAL RD./CAMELBACK RD.  
 CAMELBACK RD.  
 INDIAN SCHOOL RD.  
 THOMAS RD.  
 McDOWELL RD.  
 LOOP 202  
 McKELLIPS RD.



**Future Land Use<sup>†</sup>**

- Rural Neighborhoods
- Suburban Neighborhoods
- Urban Neighborhoods
- Mixed-Use Neighborhoods
- Resorts/Tourism
- Commercial
- Employment: Light Industrial/Office
- Employment: Office
- Cultural/Institutional or Public Use
- Developed Open Space
- Natural Open Space
- Regional Use Overlay
- Shea Corridor Overlay
- Mayo Support District
- Infill Incentive District
- City Boundary Line
- Circle (Refer to 4-GP-2002 & 19-ZN-2014, State Land)
- Circle (Refer to 54-ZN-1989 et al., DC Ranch)
- Scottsdale McDowell Sonoran Preserve Recommended Study Boundary (RSB) (See Open Space Element)



**DRAFT**

The Future Land Use map is intended to illustrate the general location and distribution of the various categories of land uses anticipated by the City of Scottsdale General Plan.

Land Uses shown outside Scottsdale city boundaries are advisory and use the closest comparable Scottsdale land use categories. (Data: Maricopa Association of Governments)



JENNY LIN RD.

CIRCLE MOUNTAIN RD.

HONDA BOW RD.

ROCKAWAY HILLS RD.

DESERT HILLS DR.

JOY RANCH RD.

STAGECOACH PASS

CAREFREE HWY.

DOVE VALLEY RD.

LONE MOUNTAIN RD.

DIXILETA DR.

DYNAMITE BLVD.

JOMAX RD.

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SHEA BLVD.

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McCORMICK PKWY.

INDIAN BEND RD.

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McDONALD DR.

CHAPARRAL RD./CAMELBACK RD.

CAMELBACK RD.

INDIAN SCHOOL RD.

THOMAS RD.

McDOWELL RD.

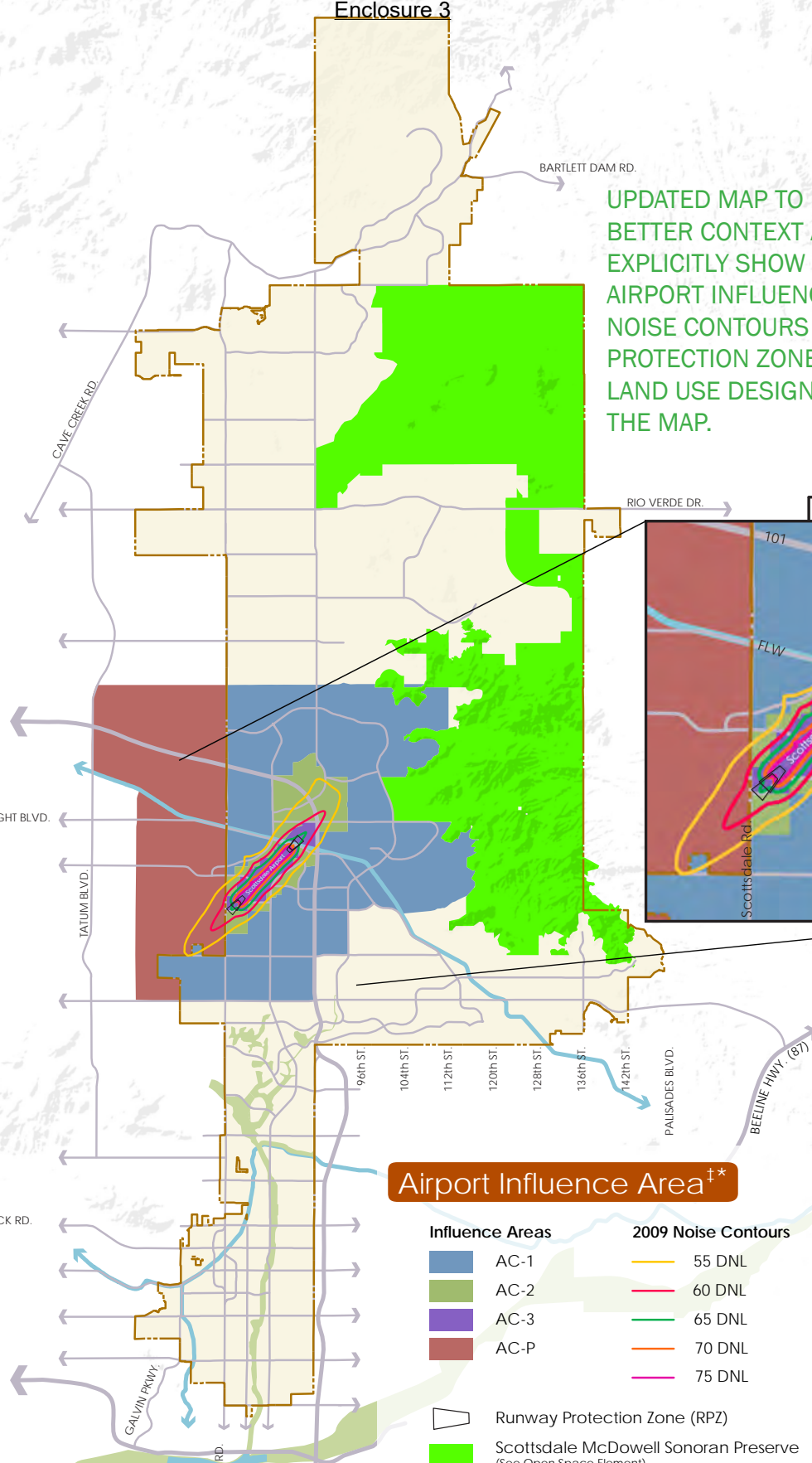
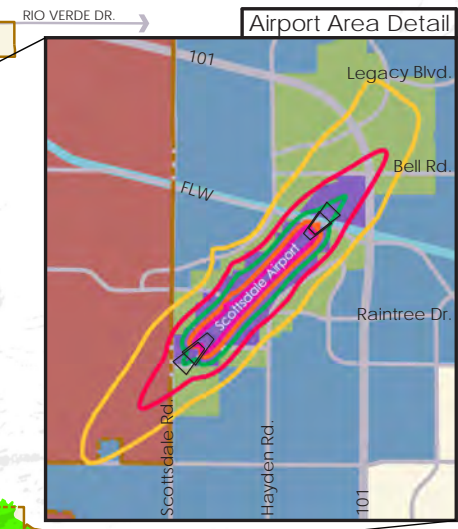
LOOP 202

McKELLIPS RD.

### Enclosure 3

BARTLETT DAM RD.

UPDATED MAP TO PROVIDE BETTER CONTEXT AND MORE EXPLICITLY SHOW BOTH THE AIRPORT INFLUENCE AREAS AND NOISE CONTOURS AND RUNWAY PROTECTION ZONE. REMOVED LAND USE DESIGNATIONS FROM THE MAP.



### Airport Influence Area<sup>†\*</sup>

- | Influence Areas  | 2009 Noise Contours |
|--|---------------------|
| AC-1   | 55 DNL              |
| AC-2   | 60 DNL              |
| AC-3   | 65 DNL              |
| AC-P   | 70 DNL              |
|  | 75 DNL              |
| Runway Protection Zone (RPZ)                                     |                     |
| Scottsdale McDowell Sonoran Preserve<br>(See Open Space Element) |                     |

\*More detail can be found in the Scottsdale Airport Part 150 Noise Compatibility Study and the Scottsdale Airport Master Plan.



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## SAFETY ELEMENT<sup>‡</sup>

The safety of a community is critical to quality of life, community health, and economic vitality. Scottsdale is consistently rated as one of the safest communities in the nation, with low crime rates and few natural disasters. As the community continues to evolve, providing superior public safety resources is crucial to Scottsdale's [image](#) as a livable community and premier tourist destination. (NEW)

Although Scottsdale is nationally-recognized for safety, it is not immune from disaster. Hazards such as flash flooding, brush fires, power outages, and other major threats, [SUCH AS PANDEMICS](#), can occur at any time with little or no warning. Management of these emergencies involves four phases: prevention and mitigation, preparedness, response, and recovery. (NEW)

Scottsdale is committed to protecting citizens and visitors from conditions, circumstances, and influences that would threaten, disrupt, or diminish the quality of life. The goals and policies of the Safety Element will ensure that the community is prepared and resilient when faced with disaster, and that our community and habitat are safe and enjoyable for all.

## Goals and Policies

### GOAL S 1 †

Work to prevent hazards and reduce physical risks throughout Scottsdale.

#### Policies

**S 1.1** Educate the public about accident and injury prevention.

**S 1.2 †** (NEW) Research and maintain information about emerging hazards, such as terrorism threats, PANDEMICS, geologic hazards, and electrical failures.



**S 1.3** (NEW) Identify and replace vulnerable infrastructure and critical service lifelines. *[Cross-reference Public Services & Facilities Element]*

**S 1.4** (NEW) Maintain public and private property to minimize fire hazards and risks. *[Cross-reference Neighborhood Preservation & Revitalization Element]*

**S 1.5** (NEW) Continue to build a support system for Scottsdale's senior population by providing outreach, education and notification. *[Cross-reference Healthy Community Element]*

**S 1.6** (NEW) EDUCATE THE PUBLIC ABOUT SWIMMING POOL SAFETY.

### GOAL S 2 (NEW)

Prepare for disasters and emergencies.

#### Policies

**S 2.1** (NEW) Provide education, emergency preparedness, and response training at the neighborhood level, and regularly conduct community-wide and regional preparedness drills to build a widespread culture of emergency preparedness.

**S 2.2** (NEW) Enhance communication capabilities in preparation for all phases of a disaster. *[Cross-reference Community Involvement and Public Services & Facilities Elements]*


**S 2.3** (NEW) Continue fire-safety planning and fire preparedness and prevention programs. *[Cross-reference Housing and Public Services & Facilities Elements]*

**S 2.4 †** (NEW) ~~Continue to prepare for water-related emergencies and~~ Ensure a safe and reliable water supply during an emergency situationS. *[Cross-reference Water Resources Element]*

**GOAL S 3 (NEW)**

Deliver effective and efficient response for emergencies and disasters.

*Policies*

- S 3.1** Provide EFFECTIVELY LOCATE public safety facilities TO MINIMIZE RESPONSE TIMES. PROVIDE; information; and training programs to minimize response times and maximize effectiveness in responding to hazards. *[Cross-reference Public Services & Facilities Element]*
- 
- S 3.2** (NEW) Train multilingual personnel to aid evacuation during emergency response.
- S 3.3** (NEW) Coordinate with hotels, resorts, and special event managers to effectively evaluate safety risks and the ability to respond to emergencies involving or affecting visitors. *[Cross-reference Economic Vitality Element]*
- S 3.4 †** (NEW) Identify emergency evacuation routes and effectively communicate the information to the public. Identify any vulnerable populations, such as the elderly or people with special needs, for first-priority evacuation assistance. *[Cross-reference Community Involvement; Healthy Community; and Circulation Elements]*
- S 3.5** (NEW) Encourage an effective “all-hazard” approach to emergency response by assessing the risks of all possible threats during an emergency.

**GOAL S 4 (NEW)**

Prepare for short- and long-term community recovery after disaster events.

*Policies*

- S 4.1** (NEW) Develop a framework for disaster recovery that identifies priorities for reinstatement of critical infrastructure and other physical assets, services, and functions throughout the community. *[Cross-reference Public Services & Facilities Element]*
- S 4.2** (NEW) Prepare city staff for official roles in emergency response and recovery.
- S 4.3** (NEW) Work with regional entities, hospitals, faith-based organizations, schools, community groups, and large employers to ensure continuity of operations after a disaster occurs.
- S 4.4** (NEW) Engage the community in the reconstruction planning process. *[Cross-reference Community Involvement Element]*
- S 4.5** (NEW) When appropriate, expedite neighborhood repair, reconstruction, and rebuilding to aid resident return to their neighborhoods. *[Cross-reference Conservation, Rehabilitation, & Redevelopment and Housing Element]*

**Goal S 5 (NEW)****Maintain safe airspace and transportation systems.***Policies*

- S 5.1** Manage the Scottsdale Airport through safety and security programs that protect airport users, surrounding neighborhoods, and nearby properties. *[Cross-reference Land Use and Circulation Elements]*
- S 5.2 †** **(NEW)** Maintain clear zones within the key safety areas around the Scottsdale Airport. *[Cross-reference Land Use Element]*
- S 5.3 †** Design, develop, and manage Scottsdale’s transportation **FACILITIES** and traffic control systems with a focus on public safety. *[Cross-reference Circulation and Bicycling Elements]*

**Goal S 6****Protect the health, safety, and welfare of the public from the impacts of flooding.***Policies*

- S 6.1** Identify drainage system needs, and make improvements where flood control problems exist. *[Cross-reference Conservation Element]*
- S 6.2** Mitigate watercourse obstructions resulting from natural causes, and restrict watercourse obstructions resulting from man-made causes. *[Cross-reference Conservation Element]*
- S 6.3** Design and construct watercourse and drainage system street crossings, where possible, to promote traffic safety. *[Cross-reference Conservation and Circulation Elements]*
- S 6.4** **(NEW)** Locate critical facilities outside of the 100-year floodplain. *[Cross-reference Conservation Element]*



**Goal S 7 (NEW)****Maintain Scottsdale as a safe community through crime prevention.***Policies*

- S 7.1** Encourage crime prevention strategies in the design and redevelopment of all areas of the city. *[Cross-reference Character & Design; Neighborhood Preservation & Revitalization; and Conservation, Rehabilitation, & Redevelopment Elements]*
- S 7.2** (NEW) Use enhanced crime tracking, trending, and predictive modeling to reduce crime and increase offender apprehension **AND PUBLIC SAFETY**.
- S 7.3** (NEW) ~~Continue to~~ Embrace **new MULTIPLE** communication platforms, such as social media, to deliver timely crime prevention strategies and increased security awareness. *[Cross-reference Community Involvement Element]*
- S 7.4** (NEW) ~~Focus on increased~~ **EMPHASIZE** crime prevention associated with visitor safety and security via event planning and management programs. *[Cross-reference Economic Vitality Element]*
- S 7.5** (NEW) ~~Create~~ **EMPHASIZE** community participation and partnerships to reduce crime and develop safer neighborhoods. *[Cross-reference Neighborhood Preservation & Revitalization Element]*
- S 7.6** (NEW) ~~Continue to~~ Locate and design **police PUBLIC SAFETY** facilities to enhance **police PUBLIC SAFETY** functions. *[Cross-reference Public Buildings Element]*
- S 7.7** (NEW) Enhance collaboration between public safety providers and the Planning Agency to incorporate best development practices in reducing crime. Considerations include improved lighting, building orientation, neighborhood ingresses and egresses, and Crime Prevention through Environmental Design (CPTED). *[Cross-reference Character & Design Element]*
- S 7.8** (NEW) ~~Continue efforts to~~ Ensure the safety of Scottsdale's schools and school-aged children. *[Cross-reference Circulation and Public Buildings Elements]*
- S 7.9** (NEW) **CONTINUOUSLY EVALUATE POLICE ROLES, FUNCTIONS, AND RESPONSIBILITIES AND THEIR EFFECTIVENESS FOR PROVIDING COMMUNITY SAFETY AND COLLABORATION.**
- S 7.10** (NEW) **COLLABORATE WITH SCHOOLS, NEIGHBORHOOD ASSOCIATIONS, COMMUNITY CENTERS, and RELIGIOUS AND MULTICULTURAL ORGANIZATIONS TO FOSTER CONTINUOUS DIALOGUE BETWEEN LAW ENFORCEMENT AND RESIDENTS about SAFETY NEEDS AND CONCERNS.**



**Goal S 8 (NEW)**

---

Promote the safe handling, storage, and disposal of hazardous materials.

*Policies*

- S 8.1** Properly manage hazardous materials and contaminants to minimize their potential harm. *[Cross-reference Conservation Element]*
- S 8.2** Minimize the use of toxic and hazardous materials and encourage the use of alternative materials and practices. *[Cross-reference Environmental Planning Element]*
- S 8.3** Continue working with appropriate agencies to clean up hazardous waste.
- S 8.4** (NEW) Provide training opportunities and educational materials on hazardous waste management. *[Cross-reference Community Involvement Element]*
- S 8.5** (NEW) Develop a plan for the transport of hazardous waste.
- S.8.6** ~~Continue the~~ Expand programs for wastes that require special management to protect public health, safety, and the environment, such as household hazardous waste, and electronics, ~~collection programs to ensure safe disposal practices and pharmaceuticals.~~ *[Cross-reference Environmental Planning and Public Services & Facilities Elements]*



Enclosure 4

JENNY LIN RD.

CIRCLE MOUNTAIN RD.

HONDA BOW RD.

ROCKAWAY HILLS RD.

DESERT HILLS DR.

JOY RANCH RD.

STAGECOACH PASS

CAREFREE HWY.

DOVE VALLEY RD.

LONE MOUNTAIN RD.

DIXILETA DR.

DYNAMITE BLVD.

JOMAX RD.

HAPPY VALLEY RD.

PINNACLE PEAK RD.

DEER VALLEY RD.

LOOP 101

UNION HILLS DR.

BELL RD./FRANK LLOYD WRIGHT BLVD.

GREENWAY PKWY.

THUNDERBIRD RD.

CACTUS RD.

SHEA BLVD.

DOUBLETREE RANCH RD.

McCORMICK PKWY.

INDIAN BEND RD.

LINCOLN DR.

McDONALD DR.

CHAPARRAL RD./CAMELBACK RD.

CAMELBACK RD.

INDIAN SCHOOL RD.

THOMAS RD.

McDOWELL RD.

LOOP 202

McKELLIPS RD.

BARTLETT DAM RD.

RIO VERDE DR.

CAVE CREEK RD.

TATUM BLVD.

96th ST

104th ST

112th ST

120th ST

128th ST

136th ST

142th ST

PALISADES BLVD.

BEELINE HWY. (87)

56th St.

64th St.

SCOTTSDALE RD.

HAYDEN RD.

PIMA RD.

LOOP 101

GALVIN PKWY.

Public Safety

- Existing Police Station
- Existing Fire Station
- Proposed Fire Station
- Scottsdale City Court
- Scottsdale McDowell Sonoran Preserve (See Open Space Element)



UPDATED TO INCLUDE:  
UP-TO-DATE PUBLIC  
SAFETY LOCATIONS

DRAFT

## 5. CONNECTIVITY



### INTRODUCTION

The way residents, visitors, and employees travel throughout the city **effects AFFECTS** overall quality of life and community image. Scottsdale is the premier center in the Valley for arts, culture and tourism; a regional shopping hub; and an importer of employees. Therefore, it is critical to provide a variety of mobility choices and examine transportation **issues CHALLENGES AND THEIR COST EFFECTIVENESS** at a local, as well as, regional level.

The automobile will remain an important means of travel in Scottsdale. To maintain mobility, land use and transportation policies **should** emphasize a mix of uses and activities served by **more SMART, SAFE**, efficient, **AFFORDABLE** and accessible transportation options. To reduce traffic congestion and **ADVERSE** impacts on the built and natural environments, Scottsdale **must CAN** make land use decisions that strive to reduce the length and number of automobile trips. Efficient, accessible, and comfortable mobility choices, as well as employer participation in trip reduction programs, can **EFFECTIVELY** transform reliance on the automobile and reduce congestion on streets. Offering a variety of accessible mobility choices will also support people in the community with limited or impaired mobility who may not be able to drive.



Through the Connectivity Chapter, Scottsdale will safely, conveniently and efficiently move people and goods.

**ELEMENTS IN THIS CHAPTER:**

- Circulation
- Bicycling

*“We will have... a wide range of innovative, efficient, and effective mobility options that connect to citywide and regional networks.”*

*- Visioning Scottsdale Town Hall Report (2013)*

## Connectivity Chapter

### CIRCULATION ELEMENT<sup>‡</sup>

- Goal C 1 DESIGN** safe/efficient transportation corridors<sup>‡</sup>
- Goal C 2 REDUCE** automobile trip **S reduction**
- Goal C 3 DEVELOP A** connected multi-modal system<sup>‡</sup>
- Goal C 4** Plan for future expansion
- Goal C 5** Protect neighborhoods
- Goal C 6 PARTICIPATE IN** regional coordination
- Goal C 7 COORDINATE WITH** schools & neighborhoods
- Goal C 8 PROVIDE A** comfortable & accessible system

### BICYCLING ELEMENT<sup>‡</sup>

- Goal B 1 DEVELOP** accessible & interconnected networks<sup>‡</sup>
- Goal B 2** Encourage increased bicycle use
- Goal B 3 PROMOTE** bicycle education & safety

[<sup>‡</sup> = State-required]



## CIRCULATION ELEMENT<sup>‡</sup>

Scottsdale's transportation system is the backbone of the city, supporting the economy and serving and influencing land use patterns. The automobile historically has been, ~~and will continue to be,~~ the predominant mode of transportation in Scottsdale. ~~While the automobile will remain an important means of travel in Scottsdale, the community must~~ **WILL CONTINUE TO** make land use decisions that strive to reduce the length and number of automobile trips. ~~However, to match the character, needs, and lifestyle of different areas, the city will need to diversify its transportation choices.~~ A variety of mobility choices will provide greater accessibility and connectivity; alleviate pollution and congestion; and foster community well-being and quality of life. (NEW)

~~This shift will depend on~~ Scottsdale's ability to continuously investigate, plan, and incorporate new and traditional **TRANSPORTATION AND MOBILITY** technologies ~~including high capacity transit modes~~ into our public transit system ~~to~~ **WILL** assure adequate access to and within our community for residents, employees, visitors, and businesses. To correspond to the character, needs, and lifestyle of different areas within Scottsdale, the city will need to diversify its transportation choices **THAT MAY REQUIRE SPECIFIC AREA SOLUTIONS**. Land use and transportation policies ~~should~~ emphasize a mix of uses and activities served by more efficient and accessible transportation options, **SUCH AS WALKING AND BICYCLING**. ~~These policies~~ **INCORPORATION OF THESE OPTIONS** will reduce traffic congestion and impacts on the built and natural environments. Together land use planning, application of sustainable technology, and employer participation in trip reduction programs; can transform reliance on the automobile and reduce congestion on streets. ~~A variety of mobility choices will provide greater accessibility and connectivity and foster community well-being and quality of life.~~ (NEW)

The Circulation Element recognizes the primary role of the automobile, but also fully integrates other modes, such as public transit, air travel, bicycling, and walking. It also recognizes the interrelationships among transportation, land use, neighborhoods, and Growth and Activity Areas. Different areas within the city may have unique mobility needs requiring specific-area solutions. However transportation systems and their impacts do not stop at the city boundary. Therefore, this element stresses the efficient use of Scottsdale's existing **AND NEW** transportation systems and strong inter-jurisdictional coordination.

## Goals and Policies

### Goal C 1 †

**Design and improve transportation corridors to safely and efficiently move people and goods.**

#### *Policies*

**C 1.1** (NEW) Support the Scottsdale public transit system to assure adequate **AND AFFORDABLE** access to and within our community for citizens, employees, visitors, and businesses through the use of existing and future technologies ~~that incorporate traditional and high capacity transit modes, excluding all forms of rail~~ **RELATED TO HIGH-CAPACITY TRANSIT.**

~~C 1.1~~ **C 1.2** † Coordinate transportation and land use planning to **enhance an integrated, sustainable provide a continuous and integrated mobility TRANSPORTATION** system ~~that promotes livable neighborhoods, economic vitality, safety, efficiency, and mode choice.~~ *[Cross-reference Land Use Element]*

**C 1.3** ~~C 1.2~~ Reduce conflict points between various ~~means of travel~~ **TRANSPORTATION CHOICES** modes of travel, for example, where the paths of vehicles and bicycles, pedestrians, or equestrians, cross, diverge, or merge.

**C 1.4** ~~C 1.3~~ Protect regional corridor traffic flow, function, and safety by using grade separations for non-motorized travel. *[Cross-reference Safety Element]*

**C 1.5** ~~C 1.4~~ ~~Use and manage technologies~~ Incorporate strategies **AND TECHNOLOGIES** that efficiently move people, ~~increase the carrying~~ improve transportation system capacity ~~of roads~~, and enhance mobility choices.

**C 1.6** (NEW) Assess alternatives to high-capacity **REGIONAL** transit ~~alternatives, excluding all forms of rail.~~

**C 1.7** ~~C 1.5~~ Provide connections that ensure functional and uninterrupted movement between transportation modes **THROUGH INTELLIGENT TRANSPORTATION SYSTEMS AND INDUSTRY STANDARD, BEST PRACTICES.**

- C 1.8** ~~C 1.6~~ Retrofit aging neighborhood infrastructure and streets and create non-motorized neighborhood connections to enhance livability, ~~and~~ safety, **ACCESSIBILITY, AND COMFORT**. *[Cross-reference Neighborhood Preservation & Revitalization Element]*
- C 1.9** ~~C 1.7~~ <sup>‡</sup> (NEW) Support the Scottsdale Airport as an integral transportation hub, connecting the community to national and international markets. *[Cross-reference Land Use and Economic Vitality Elements]*
- C 1.10** ~~C 1.8~~ <sup>‡</sup> (NEW) Promote consistent **accessibility and** wayfinding elements, including, signage, street naming, and numbering, to increase the efficiency of transportation systems. *[Cross-reference Bicycling Element]*
- C 1.11** ~~C 1.9~~ Control access to and from **ARTERIAL STREETS AND** regional transportation corridors to protect their abilities to move **MULTIMODAL** traffic efficiently ~~and~~ **decrease congestion**.

## Goal C 2

**Reduce the number, length, and frequency of automobile trips to improve air quality, reduce traffic congestion, and enhance quality of life and the environment.**

### Policies

- C 2.1** Encourage a mix of land uses that will reduce the distance and frequency of automobile trips and support mobility choices. *[Cross-reference Environmental Planning; Growth Areas; and Land Use Elements]*
- C 2.2** Integrate, ~~AS APPROPRIATE,~~ a variety of mobility choices along **LOCAL AND** regional transportation **networks CORRIDORS**. *[Cross-reference Growth Areas Element]*
- C 2.3** Reduce demands on transportation networks by using trip reduction strategies and travel demand management techniques, including **TECHNOLOGY AND APPLICATIONS**, telecommuting, alternative work schedules, carpooling, and transit/bicycling incentives **IN ORDER TO PROVIDE TRAVELERS WITH EFFECTIVE CHOICES TO IMPROVE TRAVEL RELIABILITY**. *[Cross-reference Environmental Planning and Bicycling Elements]*
- C 2.4** Work with employers to provide incentives and encouragement for trip reduction strategies. *[Cross-reference Environmental Planning and Economic Vitality Elements]*
- C 2.5** Promote non-motorized travel for short neighborhood trips. *[Cross-reference Environmental Planning and Land Use Elements]*

## Goal C 3 †

Continue to develop an effective, safe, and connected multimodal transportation system (e.g. streets, trails, bikeways, shared-use paths, transit).

### Policies

**C 3.1** Integrate park-and-ride **lots FACILITIES** and transit centers along regional corridors and within Growth and Activity Areas. *[Cross-reference Growth Areas Element]*

**C 3.2** Create a diversity of mobility choices in Growth and Activity Areas, which have the greatest intensity of development. *[Cross-reference Growth Areas Element]*

**C 3.3** Actively work with neighboring jurisdictions to maintain mobility choices and network continuity. *[Cross-reference Open Space Element]*

**C 3.4** Integrate regional employment and commercial centers into the regional multimodal system. *[Cross-reference Land Use Element]*

**C 3.5** Increase accessibility to transit **OPTIONS** through non-motorized and other transit connections, such as, neighborhood circulators ~~or~~; **ON-DEMAND TRANSPORTATION NETWORK COMPANIES Dial-A-Ride type services; AND IMPROVED/ENHANCED SIDEWALKS, MULTIMODAL PATHS, OR BIKE BICYCLE LANES; AND, NON-MOTORIZED OPTIONS FOR THE FINAL MILE TO REACH PUBLIC TRANSIT SERVICES.**

**C 3.6** Create, preserve, and enhance multimodal connections between residential areas and neighborhood-supporting land uses. *[Cross-reference Land Use and Healthy Community Elements]*

**C 3.7** Support mobility choices that reflect the character and dominant lifestyle within a neighborhood. For example, in equestrian areas of the community, create links to the citywide and regional trail system. *[Cross-reference Character & Design; Land Use; and Open Space Elements]*

**C 3.8** **(NEW) INCORPORATE DEDICATED PEDESTRIAN ZONES, OR AREAS OF PEDESTRIAN-ONLY TRAFFIC, INTO AREAS OF THE CITY THAT HAVE THE GREATEST INTENSITY OF DEVELOPMENT.**





## Goal C 4

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### Plan for the expansion and modification of the transportation system.

#### *Policies*

- C 4.1** **PROVIDE AND** maintain expansion and modification options for existing and future transportation networks to efficiently serve future mobility needs.
- C 4.2** † Preserve and/or acquire public rights-of-way to ensure mobility networks can be sufficiently expanded. *[Cross-reference Open Space Element]*
- C 4.3** Provide alternative routes and mobility options if expansion of existing routes is not possible.
- C 4.4** Coordinate local and regional construction projects to reduce mobility delays and hindrances **ON TRAVELERS, INCLUDING DISCOURAGEMENT OF CONCURRENT CONSTRUCTION ON PARALLEL TRANSPORTATION CORRIDORS. DISCOURAGE CONCURRENT CONSTRUCTION ON PARALLEL TRANSPORTATION CORRIDORS.**
- C 4.5** ~~Explore~~ **ADVANCE** partnerships ~~or~~ **AND** privatization to provide ~~more~~ **ADDITIONAL mobility** choices ~~and~~ **TO** address**ING** gaps in the ~~CITY'S~~ mobility system.
- C 4.6** Educate the community about the direct impacts of land use decisions on service levels and mobility choices. *[Cross-reference Land Use and Community Involvement Elements]*

## Goal C 5

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**Protect neighborhoods from negative impacts of regional and citywide transportation networks.**

### *Policies*

- C 5.1** Preserve reasonable emergency access to and within neighborhoods, and balance potential neighborhood street restriction with emergency accessibility needs. *[Cross-reference Safety Element]*
- C 5.2** Design neighborhood street ~~layouts that~~ **TO** reduce speeding and noise, and provide greater, **MORE DIRECT**, and safer opportunities for non-motorized transportation. *[Cross-reference Character & Design; Safety; and Neighborhood Preservation & Revitalization Elements]*
- C 5.3** Design citywide transportation networks **AND FACILITIES** to protect neighborhoods from regional or citywide traffic.
- C 5.4** Incorporate open space and buffers into street design to protect neighborhoods. *[Cross-reference Character & Design; Land Use; and Open Space Elements]*
- C 5.5** Provide transitions from regional systems to neighborhood systems by **gearing MATCHING** transportation design standards to the intensity of use and traffic volumes. *[Cross-reference Character & Design; Land Use; and Growth Areas Elements]*

## Goal C 6

---

**Actively work with regional jurisdictions and other agencies to coordinate and implement regional mobility systems and connections.**

### *Policies*

- C 6.1** Coordinate regional transportation planning and implementation strategies in partnership with neighboring jurisdictions and **quasi-REGIONAL, STATE, AND FEDERAL** governmental agencies.
- C 6.2** Support other agencies in the development of regional pedestrian, bicycle, shared-use path, and trail systems, **PARTICULARLY THOSE THAT CONNECT TO SCOTTSDALE'S SYSTEMS**. *[Cross-reference Open Space; Recreation; and Bicycling Elements]*

## Goal C 7

Balance the sensitive relationships and respective mobility needs of schools and neighborhoods.

### Policies

- C 7.1** Promote school locations and designs that encourage non-motorized travel. Accommodate direct links **SUCH AS SIDEWALKS, TRAILS, AND PATHS** between schools and neighborhoods to minimize exposure to vehicles. *[Cross-Reference Character & Design and Public Buildings Elements]*
- C 7.2 †** **(NEW)** ~~Where applicable, WORK COLLABORATE WITH SCHOOLS TO~~ minimize the impacts of ~~student drop-offs AND PICK-UP ACTIVITY SCHOOL TRANSPORTATION~~ on neighborhoods by ~~providing enough ADEQUATE SCHOOL parking, for students and employees on school grounds, AND locating student drop-off, areas for personal vehicles PICKUP, AND QUEUING SPACE on school property, FACILITATING RIDE-SHARING,~~ providing convenient access to public transit, and encouraging non-motorized transportation to school.
- C 7.3** **(NEW)** Work with schools to create safe and accessible transportation routes to campus facilities. *[Cross-reference Safety Element]*

## Goal C 8

Provide a comfortable and accessible transportation system **TO INCREASE ABILITY TO REACH GOODS, SERVICES, AND ACTIVITIES. MANY FACTORS AFFECT ACCESSIBILITY, INCLUDING MOBILITY (PHYSICAL MOVEMENT), THE QUALITY AND AFFORDABILITY OF TRANSPORTATION OPTIONS, TRANSPORTATION SYSTEM CONNECTIVITY, AND LAND USE PATTERNS.**

### Policies

- C 8.1** Provide pedestrian safety, comfort, and amenities that reflect streetscape design and character of an area. *Cross-reference Character & Design Element*
- C 8.2** **(NEW)** Consider the needs of all community members and visitors, as well as, the special needs of children, seniors, and people with impaired mobility in the planning and design of the transportation system. *[Cross-reference Healthy Community Element]*



JENNY LIN RD.  
 CIRCLE MOUNTAIN RD.  
 HONDA BOW RD.  
 ROCKAWAY HILLS RD.  
 DESERT HILLS DR.  
 JOY RANCH RD.  
 STAGECOACH PASS  
 CAREFREE HWY.  
 DOVE VALLEY RD.  
 LONE MOUNTAIN RD.  
 DIXILETA DR.  
 DYNAMITE BLVD.  
 JOMAX RD.  
 HAPPY VALLEY RD.  
 PINNACLE PEAK RD.  
 DEER VALLEY RD.  
 LOOP 101  
 UNION HILLS DR.  
 BELL RD./FRANK LLOYD WRIGHT BLVD.  
 GREENWAY PKWY.  
 THUNDERBIRD RD.  
 CACTUS RD.  
 SHEA BLVD.  
 DOUBLETREE RANCH RD.  
 McCORMICK PKWY.  
 INDIAN BEND RD.  
 LINCOLN DR.  
 McDONALD DR.  
 CHAPARRAL RD./CAMELBACK RD.  
 CAMELBACK RD.  
 INDIAN SCHOOL RD.  
 THOMAS RD.  
 McDOWELL RD.  
 LOOP 202  
 McKELLIPS RD.

**\*Cactus removed as a street from 124th to 128th - legacy depiction that changed with the acquisition of the Preserve**

Enclosure 5

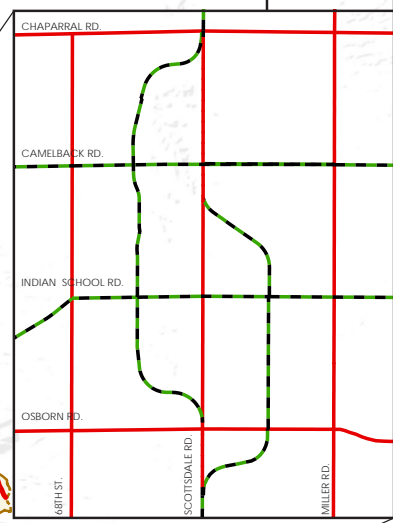
BARTLETT DAM RD.

UPDATED TO INCLUDE: OLD TOWN SCOTTSDALE CALL-OUT; FREEWAY SYSTEM; UP-TO-DATE ARTERIALS/COLLECTORS; TRANSPORTATION MASTER PLAN NOTATION; UPDATED TO INCLUDE 2020 AMENDMENT TO TRANSPORTATION MASTER PLAN.

CAVE CREEK RD.

RIO VERDE DR.

Old Town



TATUM BLVD.

96th ST  
104th ST  
112th ST  
120th ST  
128th ST  
136th ST  
142th ST

PANSADES BLVD.

BEELINE HWY (87)

GALVIN PKWY.  
SCOTTSDALE RD.  
HAYDEN RD.  
PIMA RD.  
LOOP 101

**Street Classification<sup>†\*</sup>**

- Arterial
- Collector
- Regional Connection
- Freeway
- Scottsdale McDowell Sonoran Preserve (See Open Space Element)

\*More detail can be found in the Transportation Master Plan. Neighborhood Street Systems not mapped.



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JENNY LIN RD.

CIRCLE MOUNTAIN RD.

HONDA BOW RD.

ROCKAWAY HILLS RD.

DESERT HILLS DR.

JOY RANCH RD.

STAGECOACH PASS

CAREFREE HWY.

DOVE VALLEY RD.

LONE MOUNTAIN RD.

DIXILETA DR.

DYNAMITE BLVD.

JOMAX RD.

HAPPY VALLEY RD.

PINNACLE PEAK RD.

DEER VALLEY RD.

LOOP 101

UNION HILLS DR.

BELL RD./FRANK LLOYD WRIGHT BLVD.

GREENWAY PKWY.

THUNDERBIRD RD.

CACTUS RD.

SHEA BLVD.

DOUBLETREE RANCH RD.

McCORMICK PKWY.

INDIAN BEND RD.

LINCOLN DR.

CHAPARRAL RD./CAMELBACK RD.

CAMELBACK RD.

INDIAN SCHOOL RD.

THOMAS RD.

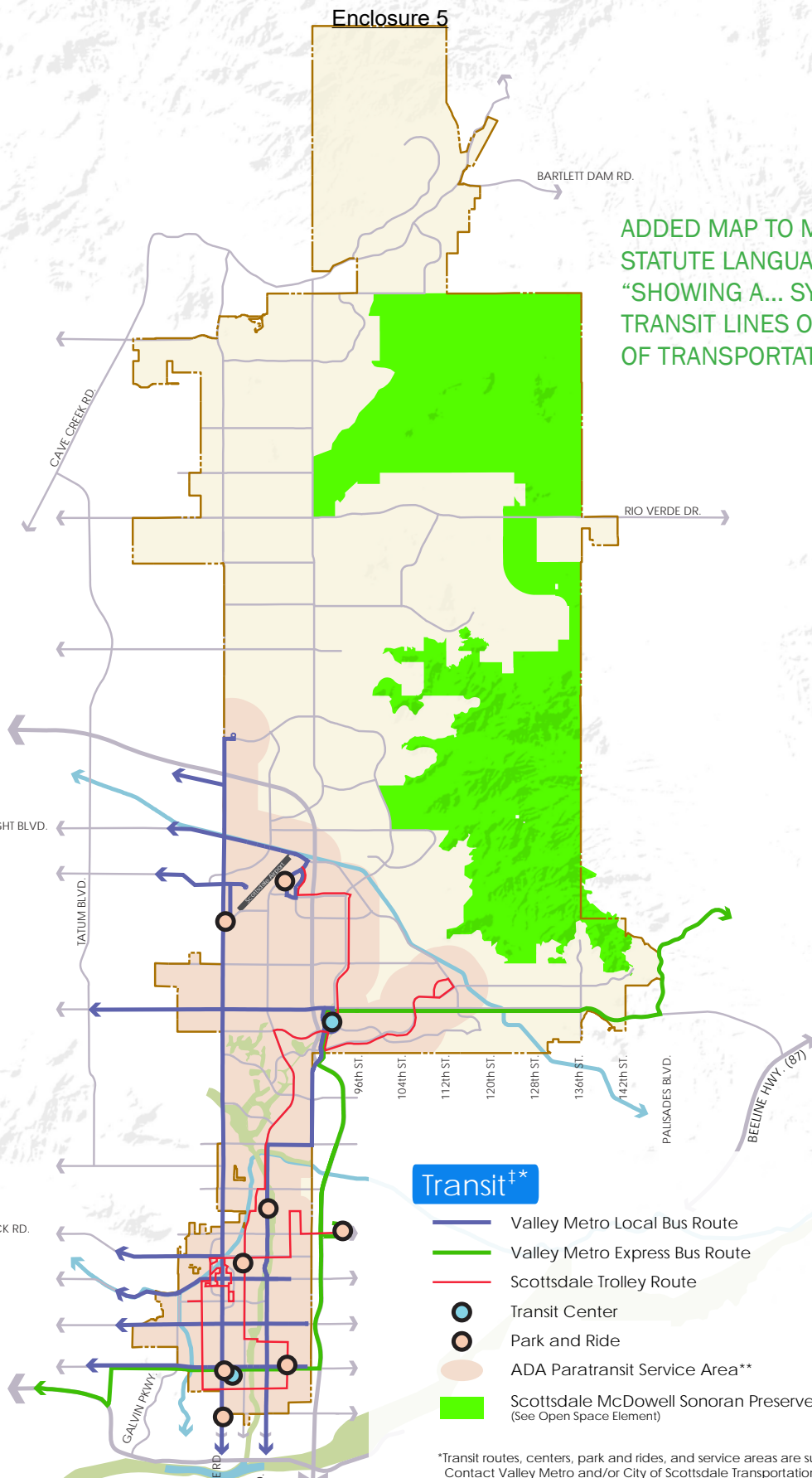
McDOWELL RD.

LOOP 202

McKELLIPS RD.

### Enclosure 5

ADDED MAP TO MEET STATE STATUTE LANGUAGE REGARDING "SHOWING A... SYSTEM OF... TRANSIT LINES OR OTHER MODE OF TRANSPORTATION".

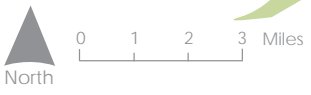


### Transit+\*

- Valley Metro Local Bus Route
- Valley Metro Express Bus Route
- Scottsdale Trolley Route
- Transit Center
- Park and Ride
- ADA Paratransit Service Area\*\*
- Scottsdale McDowell Sonoran Preserve (See Open Space Element)

\*Transit routes, centers, park and rides, and service areas are subject to change. Contact Valley Metro and/or City of Scottsdale Transportation staff to confirm service coverage.

\*\*ADA Paratransit service is generally available within 3/4 mile of a fixed bus route. Contact Valley Metro and/or City of Scottsdale Transportation staff to confirm service coverage.



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## 7. INNOVATION & PROSPERITY



### INTRODUCTION

Scottsdale's future as an exceptional place depends on a dynamic, diversified, and growing economic base that complements the character **AND LIVABILITY** of the community. Scottsdale is regionally competitive and attracts businesses that employ our residents, provide **essential VALUE-ADDED** services, **SUPPORT BROAD-BASED EDUCATIONAL OPPORTUNITIES**, respect the desert environment, reduce dependence on fossil fuels, complement and support the tourism industry, and bolster the tax base. Scottsdale celebrates and embraces existing economic strengths, seeks to diversify and develop new strengths, and supports fiscally sustainable ways of doing business.

Tourism ~~and high-paying as well as~~ industries; such as healthcare and bio/life sciences; are integral to Scottsdale's identity and serve as the community's core economic engines. In some respects, the tourism and healthcare markets in Scottsdale overlap, with both influenced by the climate, natural and built environments, and **THE DIVERSITY**, quality, and character of the community. ~~Whereas While~~ tourism is more of an ~~elastic~~ **SEASONAL, cyclical** industry, ~~healthcare and other significant industries in Scottsdale balance the it~~ ~~contributes extensively to the~~ economy, provides a ~~stable~~ diverse range of employment ~~base opportunities~~, and contributes to community well-being ~~through diverse jobs and community services~~. Preservation and enhancement of Scottsdale's core economic sectors is critical to continuous economic health.



To maintain high public service standards and physical quality, the city **must CAN** support and expand its economic base. This can be done by targeting new economic **AND EDUCATIONAL** opportunities, which provide support for the future fiscal health of the city. The Innovation & Prosperity Chapter recognizes that Scottsdale operates within broader regional and global economic settings, and, as such, the city **must CAN** maintain and strengthen its competitive position by creating an environment where Scottsdale can **EFFECTIVELY** respond, **INNOVATE**, and adjust rapidly.

#### **ELEMENTS IN THIS CHAPTER:**

- Economic Vitality
- **(NEW) TOURISM**

*"Different types and sizes of businesses should be in Scottsdale... A mixture of businesses is quintessential to keeping Scottsdale's small town feel."*

*- Scottsdale Youth Town Hall Report (2014)*



## Innovation & Prosperity Chapter

### ECONOMIC VITALITY ELEMENT

Goal EV 1 Strengthen tourism

Goal EV 2 1 FOSTER economic resiliency

Goal EV 3 2 Enhance socioeconomic prosperity

Goal EV 4 3 MANAGE LAND USES TO ENHANCE economic development

Goal EV 5 4 ENSURE fiscal sustainability

### TOURISM ELEMENT

GOAL T 1 STRENGTHEN TOURISM

GOAL T 2 ENHANCE MOBILITY AND WAYFINDING

GOAL T 3 SUPPORT SPECIAL EVENTS AND VENUES



## ECONOMIC VITALITY ELEMENT

A healthy, vibrant economy is fundamental to Scottsdale's lifestyle and identity. Scottsdale relies on its strong economic vitality to generate financial resources; provide services and a high quality of life; offer employment and prosperity for its citizens; and preserve special places in the community. Without these economic strengths, citizens would experience decreased levels of services and amenities, and/or increased taxes and costs of services.

~~Tourism is an integral part of Scottsdale's identity and economy, and it serves as one of the community's leading economic engines. To maintain and enhance the tourism experience, it is essential to provide and preserve our authentic natural, built, social, and cultural environments. The tourism market is dependent on the quality, character, and service level of local hotels and resorts; the availability of natural and built attractions; presence of entertainment activities; and the unique desert experience.~~

While tourism remains a significant aspect of Scottsdale's economy, the city's economic base is quite diverse. Longstanding economic themes of health, research, bio/life sciences, arts and culture, and advanced business services work together with other emerging sectors to foster a vibrant economy and prosperity for our residents. Extensive retail and diverse employment opportunities within the Greater Scottsdale Airpark and **Downtown OLD TOWN** are examples of how Scottsdale has focused on its strengths, broadened its tax base, and provided high-quality employment opportunities.

The Economic Vitality Element provides goals and policies to sustain the economy while protecting Scottsdale's unique southwestern character and quality of life.

## Goals and Policies

### Goal EV 1

~~Strengthen Scottsdale's position as a premier regional, national, and international tourism and resort destination.~~

#### ~~Policies~~

- ~~EV 1.1 — Support the development and revitalization of hotels and resorts that reflect Scottsdale's Sonoran Desert character and high-quality image. [Cross-reference Character & Design; Land Use; and Conservation, Rehabilitation, & Redevelopment Element]~~
- ~~EV 1.2 — Accommodate diverse, high-quality lodging and tourism market segments according to regional and national visitor trends and community character.~~
- ~~EV 1.3 — Preserve Scottsdale's natural, social, and cultural environments so that Scottsdale's tourism experiences remain uniquely competitive and viable. [Cross-reference Open Space and Arts, Culture & Creative Community Elements]~~
- ~~EV 1.4 — Enhance Scottsdale's tourism support services including, fine dining, specialty retail, art galleries, spas and wellness centers, transportation, recreation and leisure opportunities, and entertainment. [Cross-reference Healthy Community; Arts, Culture & Creative Community; and Recreation Elements]~~
- ~~EV 1.5 — Provide destination attractions and events that celebrate Scottsdale's heritage, including, the key theme areas of the arts, southwestern culture, Native American culture, cowboy/western lore, and the Sonoran Desert environment. [Cross-reference Character & Design and Arts, Culture & Creative Community Elements]~~
- ~~EV 1.6 — Build on Scottsdale's strength for attracting sporting events/entertainment opportunities, and signature special events. [Cross-reference Arts, Culture & Creative Community and Safety Elements]~~
- ~~EV 1.7 — Provide multimodal transportation choices and regional links to ease tourist mobility. [Cross-reference Circulation Element]~~

GOAL EV 1 AND ASSOCIATED  
POLICIES MOVED TO THE  
NEW TOURISM ELEMENT AS  
REQUESTED BY THE CRC AT  
THEIR MAY 18, 2020 MEETING.

- ~~EV 1.8 — Support tourism by providing public scenic, outdoor, educational, and recreational facilities for visitors. [Cross-reference Character & Design; Open Space; Recreation; Arts, Culture & Creative Community; and Public Buildings Elements]~~
- ~~EV 1.9 — Preserve and enhance historical and archaeological sites that support tourism/tourist destination attractions. [Cross-reference Arts, Culture & Creative Community Element]~~
- ~~EV 1.10 — Encourage and maintain high-quality retail, event experiences, and entertainment activities that enhance tourism and attract visitors from nearby communities. [Cross-reference Land Use; Recreation; and Arts, Culture & Creative Community Elements]~~
- ~~EV 1.11 — Continue to support a concentration of visitor services and experiences in the Downtown Old Town area. [Cross-reference Growth Areas and Arts, Culture & Creative Community Elements]~~
- ~~EV 1.12 — Advocate the development of new retail opportunities, especially those that capture the unique flavor of Scottsdale and complement the quality, resort, and desert character of the community. [Cross-reference Character & Design and Land Use Elements]~~

GOAL EV 1 AND ASSOCIATED  
POLICIES MOVED TO THE  
NEW TOURISM ELEMENT AS  
REQUESTED BY THE CRC AT  
THEIR MAY 18, 2020 MEETING.

## Goal EV 2 1

Foster Scottsdale's resiliency to economic change through support of our core industries (e.g. tourism, healthcare, bio/life sciences, advanced business services), assets, regional competitiveness, and economic diversity.

### Policies

- EV 2 1.1** (NEW) Direct economic growth and change through the implementation and regular update of economic and tourism development strategic plans, that:
- Identify major and emerging growth sectors in Scottsdale, the metropolitan area, and Southwest;
  - Support Scottsdale's competitive strengths and are compatible with Scottsdale's lifestyle;
  - Guide the coordination of the local economy with regional economic initiatives; and
  - Encourage and support local businesses, well-paying jobs, fiscal sustainability, clean industries, and the broader community vision and goals.  
*[Cross-reference Vision and Values]*
- EV 2 1.2** Support retention and expansion of established businesses, and provide resources for businesses to adapt to changing market conditions.
- EV 2 1.3** Diversify Scottsdale's businesses, focusing on industries that add value to the existing economic environment.
- EV 2 1.4** Recruit and retain **environmentally-sensitive** companies **at the forefront of economic and technological change, with core values that include both innovation and environmental stewardship** and encourage partnerships between these companies and research institutions. *[Cross-reference Environmental Planning and Energy Elements]*
- EV 2 1.5** (NEW) Foster collaboration and the creation of support networks among Scottsdale businesses.

## Goal EV 3 2

Provide diverse economic activities, employment opportunities, and educational pursuits to enhance the socioeconomic prosperity of all community members.

### Policies

- EV 3 2.1** Target specific economic sectors for expansion or relocation in Scottsdale that will enhance the quality of life of the community, provide the greatest positive impact, and deliver the fewest negative impacts.

*[Cross-reference Land Use; Cost of Development; and Public Services & Facilities Elements]*



- EV 3 2.2** Strengthen community partnerships with **educational** institutions to grow and support high-value startup businesses, research, and other entrepreneurial opportunities. *[Cross-reference Healthy Community Element]*

- EV 3 2.3** **(NEW) Support ENHANCE** accessible and effective education and job training- **from pre-school through higher education OPPORTUNITIES AT ALL LEVELS.** *[Cross-reference Healthy Community Element]*

- EV 3 2.4** **(NEW)** Attract and retain a mix of businesses and industries that can provide jobs for residents of all skill and education levels. *[Cross-reference Land Use and Healthy Community Elements]*

### Goal EV 4 3

Sensitively manage land uses to provide and enhance economic development, fiscal health and job growth, while simultaneously protecting ~~both the environment and~~ the integrity and lifestyle of neighborhoods.

#### Policies

- EV 4 3.1** Support ~~Plan for~~ and enhance, where necessary, major street, freeway, and telecommunications access to key employment and regional retail centers. *[Cross-reference Land Use; Growth Areas; Circulation; and Public Services & Facilities Elements]*
- EV 4 3.2** Maintain and create distinctive business, shopping, and cultural/entertainment clusters. *[Cross-reference Land Use and Arts, Culture & Creative Community Elements]*
- EV 4 3.3** Maintain and develop neighborhood-scale shopping areas near residential concentrations. *[Cross-reference Land Use and Neighborhood Preservation & Revitalization Elements]*
- EV 4 3.4** Focus major employment and commercial uses in Growth Areas. *[Cross-Reference Growth Areas Element]*
- EV 4 3.5** Ensure neighborhoods ~~and sensitive environmental habitats~~ are adequately protected from major development through design sensitivity, buffering, and traffic management. *[Cross-reference Land Use and Character & Design Elements]*
- EV 4 3.6** **(NEW)** Enhance and protect the Scottsdale Airport as a global connection for tourism and business development. *[Cross-reference Land Use; Circulation; and Safety Elements]*
- EV 4. 3.7** **(NEW)** Identify and promote opportunities for infill development, and ensure that infill development projects sensitively integrate into the ~~environmental and~~ neighborhood setting. *[Cross-reference Land Use; Character & Design; Conservation, Rehabilitation, & Redevelopment; and Neighborhood Preservation & Revitalization Elements]*
- EV 4. 3.8** **(NEW)** Promote orderly, planned growth to reduce service costs, maximize use of existing and proposed public facilities, ~~ensure adequate infrastructure resources,~~ and enhance available revenues. *[Cross-reference Land Use; Growth Areas; Cost of Development; and Public Services & Facilities Elements]*
- EV 4. 3.9** **(NEW)** Maintain and expand, ~~when appropriate for the city's fiscal health,~~ **WHEN APPROPRIATE FOR THE CITY'S FISCAL HEALTH,** resorts/tourism, employment, and commercial land uses to provide revenue, jobs, and contribute to the socioeconomic prosperity of our residents. *[Cross-reference Land Use and Growth Areas Elements]*



## Goal EV 5 4 (NEW)

Ensure that Scottsdale retains fiscal resources needed to effectively govern, provide services at a level consistent with community expectations, and fulfill the community's vision.

### Policies

- EV 5 4.1** Ensure the highest level of services and public amenities are provided at the lowest costs in terms of property taxes and travel distances. *[Cross-reference Land Use; Public Services & Facilities; and Recreation Elements]*
- EV 5 4.2** (NEW) Diversify Scottsdale's economic base to financially insulate the city in a down economic cycle, and to encourage and enhance prosperity during times of economic growth.
- EV 5 4.3** (NEW) Seek and support economic development projects that will contribute positively to the city's finances and advance the community's vision and goals. *[Cross-reference Vision & Values]*
- EV 5 4.4** (NEW) Operate the city in a fiscally-responsible manner through long-term planning and maintaining a positive balance between available revenue and expenditures. *[Cross-reference Public Services & Facilities Element]*
- EV 5 4.5** (NEW) Evaluate projected changes in city revenue and service costs as a part of General Plan review. *[Cross-reference Implementation Chapter]*
- EV 5 4.6** (NEW) ~~Explore new options~~ Promote efficiencies gained from comprehensive energy management to supplement the city's existing resources devoted to the operation and maintenance of infrastructure and service delivery. *[Cross-reference Cost of Development and Public Services & Facilities Elements]*
- EV 5 4.7** (NEW) Carefully consider the fiscal implications of land use decisions ~~that result in service expansions to avoid significant negative fiscal impacts, unless necessary to achieve other critical community objectives.~~ *[Cross-reference Land Use; Cost of Development; and Public Services & Facilities Elements]*
- EV 5 4.8** (NEW) Invest in the city's high-performance ~~organization and~~ workFORCE culture to continue to provide a high level of services for community members and visitors.





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Scottsdale is a community of choices, welcoming to, and inclusive of diverse cultures and lifestyles. Many characteristics combine to make Scottsdale what it is now: the physical setting of the Sonoran Desert; a vibrant, walkable downtown; thriving businesses and industries; authentic arts and culture; and varied neighborhoods, each unique in character. Some of Scottsdale's best features include our Scottsdale McDowell Sonoran Preserve, the Indian Bend Wash greenbelt, WestWorld, golf courses, great weather, open skies, beautiful views, worldwide prestige, civic engagement, healthcare resources, safe neighborhoods, cleanliness, and an abundance of entertainment and leisure attractions. [\(NEW\)](#)

The passage of time inevitably brings changes. The Scottsdale of today differs from the city it was twenty years ago, and the city it will be twenty years from now. As we welcome the future, retaining the community's unique features will strengthen the evolution of our sophisticated city with small town charm.

Our future begins today. Building upon decades of planning and thousands of hours of community involvement, General Plan 2035 guides the physical development of Scottsdale and acts as a blueprint to enhance our Community Aspirations—Exceptional Experience, Outstanding Livability, and Community Prosperity—over the next twenty years.

# VISION

~~Scottsdale will be an exceptional Sonoran Desert experience and the premier southwestern tourist destination. Our SCOTTSDALE'S diverse neighborhoods will foster outstanding livability through connected, healthy and sustainable communities. Scottsdale WE will thrive by AS A RESULT OF OUR EDUCATED CITIZENRY AND BY attracting and retaining business centers of excellence that encourage innovation and prosperity. SCOTTSDALE WILL BE AN EXCEPTIONAL SONORAN DESERT EXPERIENCE AND THE PREMIER SOUTHWESTERN TOURIST DESTINATION. (NEW)~~







# EXCEPTIONAL EXPERIENCE





Scottsdale is a special place in the Sonoran southwest. Our community will continue to [draw](#) **ATTRACT** visitors, businesses, and new residents from around the world because of our natural desert beauty; vast open spaces and environmental assets; high standards for design; world-class events and resorts; vibrant downtown; and distinctive heritage and culture. [\(NEW\)](#)





# OUTSTANDING LIVABILITY





Enclosure 7

Scottsdale will continue to offer a variety of multi-generational lifestyle choices that are responsibly planned, connected, attractive, and supported with appropriate infrastructure and services for urban, suburban, and rural living. Our neighborhood experiences will advance well-being and safety through promotion of physical and social connection. (NEW)







# COMMUNITY PROSPERITY





Enclosure 7



Scottsdale will be a thriving, prosperous city that attracts and grows world-class businesses, leverages technology, encourages innovation and creativity, and cultivates **ACADEMIC OPPORTUNITIES FOR** a well-educated **CITIZENRY AND** workforce. ~~Our citizens will have opportunities to prosper.~~  
**(NEW)**



# OUR COMMUNITY VALUES



These values will be at the forefront of our decision-making in implementing our vision, community aspirations, and goals found in the General Plan and shall be the basis upon which inconsistencies in the General Plan are resolved (values listed are of equal importance):

## ■ **Respect Character and Culture**

Enhance and protect Scottsdale's unique features, neighborhood identity, character, livability, southwestern heritage, and tourism through appropriate land uses and high standards for design. Create vibrant and attractive places that accommodate a variety of ages and incomes and support the arts and multicultural traditions.

## ■ **Conserve and Preserve the Environment**

Lead the region in the stewardship and **effective sustainable** management of the Sonoran Desert environment and conservation of natural resources and open spaces for the visual, physical, and personal enrichment of everyone.

## ■ **Collaborate and Engage**

Promote strong, visionary leadership that is transparent, responsive, and efficient; collaborates regionally; respects and honors our community values; recognizes the benefit of interactive community involvement and volunteerism; and embraces citizens as active partners in decisions that affect their neighborhoods and city.



■ **Foster Well-Being**

Promote a culture of lifelong physical and mental health, safety, and well-being for residents, visitors, employers, and employees. **FOSTER SOCIAL CONNECTIVITY ACROSS CULTURAL AND GENERATIONAL BOUNDARIES BY CULTIVATING A WELCOMING ENVIRONMENT; RESPECTING HUMAN DIGNITY; AND, RECOGNIZING AND EMBRACING CITYWIDE AND REGIONAL DIVERSITY.**

■ **Connect the Community**

Connect all community members ~~across geographic, cultural and generational boundaries by cultivating a welcoming environment; respecting human dignity; recognizing and embracing citywide and regional diversity; and~~ **THE REGION AND WITHIN THE CITY and to the region by** striving for cost-effective, adaptable, **and** innovative, **safe, and efficient** mobility options ~~THAT ARE SAFE AND EFFICIENT. CONNECTIVITY AND MOBILITY ARE INVOLVE MORE THAN GETTING LINEAR PHYSICAL SPACES THAT PERMIT PEOPLE TO GET FROM HERE TO THERE;~~ **CONNECTIVITY AND MOBILITY INFLUENCE THE FORM AND COMFORT OF URBAN COMMUNITIES.**

■ **Revitalize Responsibly**

**Vigorously PROACTIVELY** evaluate the short- and long-term impacts of **DEVELOPMENT** decisions to ensure that ~~development and redevelopment~~ **PUBLIC AND PRIVATE INVESTMENT WORK COLLABORATIVELY TO** support and maintain the unique features and **local** identity that make Scottsdale special, and contribute positively to the community's physical, fiscal and economic needs and high quality of life.

■ **Advance Innovation and Prosperity**

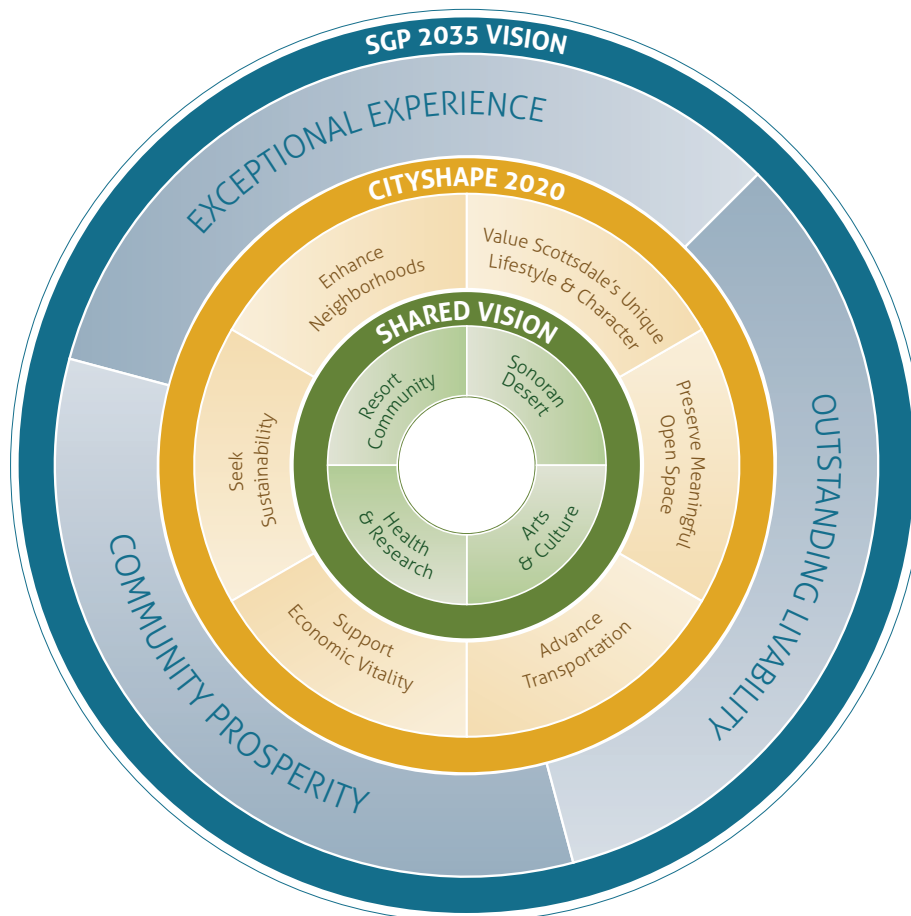
Embrace a diverse, and innovative economy to sustain our high quality of life through a variety of businesses, health and research institutions, and educational, technological, tourism and cultural elements.

## THE FOUNDATION FOR THE COMMUNITY VISION

Each of us has a vision of what Scottsdale should be like in the future. Although our visions are different, they share common qualities. Unanimous agreement about the future is not the goal of the General Plan. Rather, it strives to create balance and blending of opinions to form a community that collectively manages change. Only then can we retain the community’s unique characteristics and still welcome the future.

The Scottsdale General Plan 2035 strives to reflect the coherent vision, aspirations, and community values of a diverse population

The community vision is built on a foundation of citizen involvement. Building on the Scottsdale Town Enrichment Program (S.T.E.P.) forums of the 1960s, 1970s, and early 1980s, two community visioning processes, Scottsdale Visioning (1991-92) and CityShape 2020 (1994-96), identified Dominant Themes and created Guiding Principles for the community. These ideas were carried forward into the 2001 General Plan and validated through voter ratification. During the General Plan 2035 process, the community has retained these themes and principles by summing them up into primary Community Aspirations for Scottsdale’s future—Exceptional Experience, Outstanding Livability, and Community Prosperity. Differences of opinion about what the future should bring will always exist, but it is evident that the foundation laid by our legacy of community visioning holds true.



## Scottsdale Visioning and the Shared Vision

In 1991, a “community visioning” process began to identify the most important and significant beliefs and desires about the long-term future of the community. In December 1992, the City Council accepted a report outlining Scottsdale’s Shared Vision. The Shared Vision identified four mutually supportive Dominant Themes, which reflect Scottsdale’s special qualities and are the foundation for Scottsdale’s long-term vitality. The Four Themes represent who we are and present an inspiring vision of our emerging special place in the broader regional, national, and global economy.



The Four Dominant Themes **are WERE:**

- **Sonoran Desert:** Our growth and development should proceed with clear awareness of the impact on our rare and beautiful environment.
- **Resort Community:** Tourism and the constant influx of people from all over the world strongly affect our way of life as well as our economy.
- **Arts & Culture:** Scottsdale’s cultural assets are an integral part of the community and a basis for further development.
- **Health & Research:** Scottsdale has a culture of wellness and an optimistic spirit of innovation. Health, biotechnology, computer, and other high-tech businesses are a natural fit for the growing diversity of our community.

*“Building on its southwestern heritage, stylish reputation, and innovative methods for delivering municipal services, Scottsdale has evolved into an internationally recognized resort center, art community, and health care provider. The desert community of Scottsdale has always been its own special place. It has never tried to be all things to all people.”*

- Scottsdale Shared Vision 1992

## CityShape 2020

After the Visioning process, a comprehensive review of the General Plan called CityShape 2020, occurred in late 1994. CityShape 2020 was an extensive educational and community outreach process responsible for reaffirming and improving the General Plan as an expression of the Shared Vision. Completed in 1996, the recommendations from the CityShape 2020 process are the basis for planning in Scottsdale today. The recommendations include:

- Six Guiding Principles (the principles are equal, with no priority in the listing):
  - **Preserve Meaningful Open Space:** The City of Scottsdale is committed to promoting the acquisition, dedication, and setting aside of open space as a community amenity and in support of the tourism industry in Scottsdale.
  - **Enhance Neighborhoods:** Scottsdale’s residential and commercial neighborhoods are a major defining element of this community. The quality of our experience as a Scottsdale citizen is expressed first and foremost in the individual neighborhoods where we live, work, and play. Scottsdale is committed to maintaining and enhancing our existing and future neighborhoods. Development, revitalization, and redevelopment decisions, including rezoning and infrastructure planning, must meet the needs of our neighborhoods in the context of broader community goals.
  - **Seek Sustainability:** Scottsdale is committed to the effective management of its finite and renewable environmental, economic, social, and technological resources to ensure that they serve future needs.
  - **Support Economic Vitality:** Scottsdale is committed to the goal of supporting its existing economic strengths by targeting new opportunities which can diversify our economic base; providing for the fiscal health of the city; and forming partnerships with the community, which strengthen our ability to meet this goal.
  - **Advance Transportation:** The transportation system must be the backbone of Scottsdale, supporting its economy and serving and influencing its land use patterns in a positive way.
  - **Value Scottsdale’s Unique Lifestyle and Character:** Scottsdale offers a superior and desirable Sonoran Desert lifestyle for its citizens and visitors. The preservation of this unique lifestyle and character will be achieved through a respect for our natural and man-made environment, while providing for the needs of our citizens.
- An enhanced focus on “character and quality” in development; and
- A three-level approach to planning: Citywide, Character Area, and Neighborhood Planning

Both the Shared Vision and CityShape 2020 recommendations are reflected in General Plan 2035’s vision, aspirations, values, and goals and policies.

## **Future in Focus/2001 General Plan**

Future in Focus was a community effort to re-evaluate Scottsdale's General Plan, bring it up to date with the Growing Smarter and Growing Smarter Plus ACTS, and make sure the overall direction for our city's development (through the General Plan) was still in line with the community's vision and goals. Future in Focus examined the vision created through Scottsdale Visioning and CityShape 2020 in the light of changes that had occurred between 1990 and 2000. The process resulted in the 2001 General Plan, which was subsequently ratified by the Scottsdale voters in 2002, per State mandate.

Based on input received from Scottsdale community members, the 2001 General Plan focused on:

- Growth management policies;
- Strengthening and preserving community character and neighborhoods;
- Involving the community in the decision-making process;
- Expanding transportation choices; and,
- Housing affordability and the need for moderately priced senior living facilities.



Using community input throughout Scottsdale's 2001 General Plan process ensured that the plan incorporated the diversity of perspectives and beliefs held by Scottsdale community members.

*"The importance of community participation in the process to update the General Plan cannot be underestimated. For a General Plan to meet the needs, expectations and ultimate desires of its community, it must thoroughly consider all sectors of the community, consider various positions and alternatives, and ultimately analyze and present the results in a fair and non-biased manner."*

*- Future in Focus Community Involvement Outreach Summary, November 2000*



## Scottsdale General Plan 2035 (NEW)

General Plan 2035 started with a conversation about how Scottsdale should evolve over the next 20 years. That conversation began in early 2013 with a group of 80 youth in the Future Leaders Town Hall, followed by a consortium of 100 community members in the Visioning Scottsdale Town Hall. The conversation lasted for more than two years.

More than 1000 people were directly involved in the conversation—through one-on-one discussions; workshops; photo contests; online questionnaires; written submissions; Task Force, Planning Commission and City Council meetings; and community conversations. Many more were indirectly part of the conversation—by talking with each other, reading articles, blogging, and viewing General Plan exhibits and posters. Over the course of two years, ideas flowed. People listened, shared, and discussed hard topics.



IN 2014, THE EFFORT CULMINATED IN THE RELEASE OF A DRAFT 2035 TASK FORCE RECOMMENDED PLAN. HOWEVER, DUE TO OTHER COMMUNITY INITIATIVES, THE DRAFT PLAN NEVER PROGRESSED THROUGH THE STATE STATUTE REQUIRED PUBLIC HEARING PROCESS, NOR WAS THE PLAN CONSIDERED FOR ADOPTION OR RATIFICATION. IN 2020, THE 2014 TASK FORCE DRAFT PLAN WAS CHOSEN AS THE BASELINE PLAN TO BEGIN A NEW GENERAL PLAN UPDATE PROCESS. A CITIZEN REVIEW COMMITTEE MADE UP OF REPRESENTATIVES FROM CITY BOARDS AND COMMISSIONS WAS FORMED TO REVIEW THE TASK FORCE DRAFT 2035 PLAN AND PROVIDE SUGGESTED UPDATES TO IT. AS PART OF BOTH THE 2012 - 2014 AND 2020 PROCESSES, The following are some of the major concepts the community discussed INCLUDED:

- Scottsdale is a special city in the heart of the Sonoran Desert.
- Scottsdale's appearance, climate, specialty businesses, culture, leisure opportunities, **EDUCATION**, and healthcare are what bring visitors and businesses to the community.
- Scottsdale is a mosaic of diverse neighborhoods, each one contributing in its own way to the "picture" that is Scottsdale.
- Recognizing that growth will continue in the city, the community insists on contextually-sensitive, responsible, and managed growth. The goal is not to be the fastest growing city in Arizona, but rather, continue to make Scottsdale the best place for growth that is likely to occur.
- Scottsdale is **and should continue to be** an economic destination city, **this THAT** includes:
  - National and international tourists;
  - Regional visitors/shoppers;
  - Specialty, high-end retail;

- **A WORLD CLASS EDUCATIONAL SYSTEM;**
- A well-educated workforce and high-paying jobs;
- A diversity of large and small businesses in a variety of sectors; and
- A place for a variety of socioeconomic levels **AND DEMOGRAPHIC GROUPS.**
- Scottsdale is a leader in preserving the natural environment and open spaces, as well as promoting environmentally-sensitive development.
- Scottsdale strives to be healthy and interconnected through a variety of transportation choices, continues to excel in environmental initiatives, and provides open space and recreation opportunities for community members.
- Scottsdale continues to be a multi-generational community, with people living here during all life stages.
- Some recognized challenges in the future include:
  - Accommodating growth in a sensitive and economically-responsible way;
  - Recognizing Scottsdale's population is aging and has one of the highest elderly populations in the country, **WE MUST CONTINUE TO EXPAND SUPPORT AND SERVICES TO THIS GROWING DEMOGRAPHIC;**
  - Realizing the community is reaching build-out, and therefore, future development will be primarily infill or redevelopment; it will be important to ensure this development integrates into existing neighborhoods;
  - **PROVIDING A RANGE AND DIVERSITY OF HOUSING OPTIONS FOR THE COMMUNITY;**
  - Although the community will continue to use the automobile as the primary means of travel, encouraging use of **alternative transportation modes A VARIETY OF MOBILITY CHOICES** will enhance community health, reduce traffic congestion, and connect residents and visitors to destinations without using a car;
  - As established areas of Scottsdale revitalize and regenerate, providing access to significant, useable public open spaces and recreation opportunities will be important; and
  - Ensuring prudent implementation of the General Plan for consistency with the community's vision and values.

These citizen participation processes, encompassing thousands of work hours, have helped to define the future vision for the community and provided new tools with which to achieve that vision. The General Plan is one of those tools.



# PEOPLE WANT A CITY...

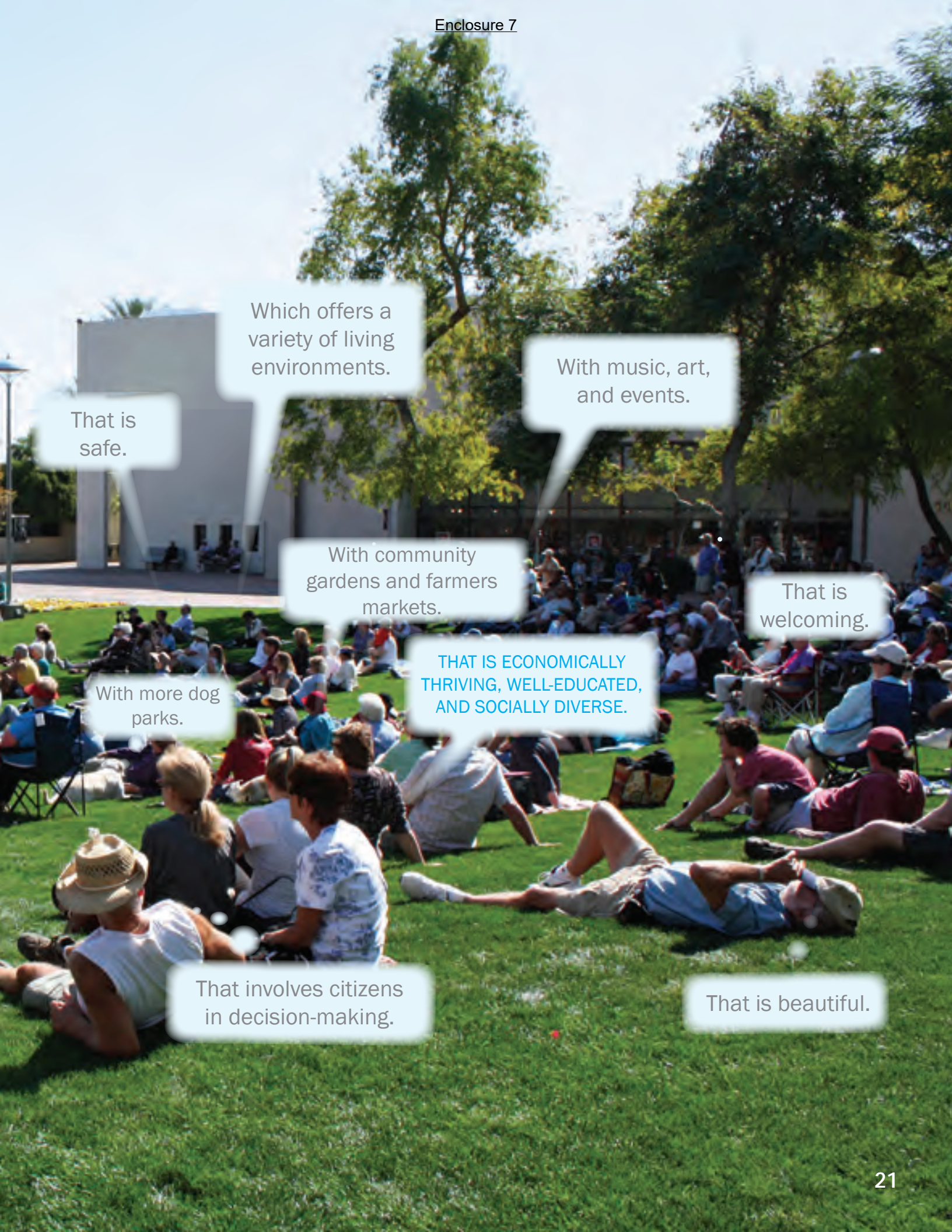
That continues to attract tourists from all over the world.

Where it is easy to get around.

With diverse businesses.

Where they can age-in-place.

That protects wildlife, native plants, and open spaces.



Which offers a variety of living environments.

With music, art, and events.

That is safe.

With community gardens and farmers markets.

That is welcoming.

With more dog parks.

THAT IS ECONOMICALLY THRIVING, WELL-EDUCATED, AND SOCIALLY DIVERSE.

That involves citizens in decision-making.

That is beautiful.

## 8. IMPLEMENTATION (NEW)



Implementation is the **ultimate-goal MANIFESTATION** of the General Plan. Scottsdale's General Plan sets forth a vision that will come alive as city government, residents, businesses, organizations, and others work together to fulfill the plan's goals and policies. This can be accomplished if the plan is consistently administered, maintained and evaluated. Because the General Plan will be implemented over the long-term, the Implementation Chapter provides a process to complete programs and projects in a systematic and coordinated manner.

Arizona State law provides for the municipal Planning Agency to take the following actions to implement the General Plan:

- Recommend measures to the City Council that will put into effect the provisions of the General Plan;
- Promote public interest and understanding of the General Plan and its regulations;
- Communicate with other public officials, agencies and organizations with regard to General Plan implementation;
- Develop specific plans as may be necessary to implement the General Plan;
- Contract for, receive, and utilize grants or other financial assistance made available by government agencies;
- Render an annual report to City Council on the status of the General Plan and progress of its application; and
- Create and maintain a Capital Improvement Program (CIP).



This Chapter is organized into the following sections:

- 1) **Implementation Tools** describe the primary ways the General Plan is carried out; with recommendations for certain action steps.
- 2) **Funding Sources** provide a list of primary funding sources for General Plan implementation.
- 3) **Oversight and Coordination** describes generally who implements the General Plan.
- 4) **Process and Programs** describes general next steps in implementing the General Plan and a list of major programs that should be created and/or updated to bring the General Plan to life.
- 5) **Measuring Progress** indicates how the General Plan will be evaluated and monitored over the next 20 years; with recommended reporting procedures.

## 1) Implementation Tools

The General Plan provides a broad framework for the physical development of the city and the delivery of public services. Much of the plan's implementation occurs on a daily basis, through both private and public actions. The following are some primary implementation tools of the General Plan, as well as some general action steps that should be taken to realize the community's vision and goals.

### City Code

Many General Plan policies are implemented through regulations adopted by the City of Scottsdale, based upon the city's "police power" to protect the public's health, safety, and welfare. The City Code is comprised of detailed regulations on a variety of areas, including, but not limited to, zoning, subdivision development, fire, police, stormwater management, aviation, native plants, property maintenance, parking, and historic preservation.

### **Action Steps:**

- Complete a comprehensive review and update of the City Code to conform to the General Plan.
- Strategically review and update the Zoning Ordinance in the following ways:
  - On a neighborhood basis, primarily in response to neighborhoods plans or emerging challenges or opportunities within individual neighborhoods;
  - On a Character Area basis, primarily to support implementation of Character Area Plans;
  - On a topical basis, primarily to achieve the policies of the General Plan and any legal mandates or community priorities that arise; and
  - On a citywide basis, to achieve the community's vision, aspirations, and goals in the General Plan.
- Evaluate rezonings and Zoning Ordinance text amendments for consistency with the General Plan, ~~particularly the Land Use and Character & Design Elements, and advancement of the goals and policies of other elements of the plan.~~
- Complete a comprehensive review of the city's public outreach and involvement processes.

## **Character Area Plans**

Character Area Plans work to define, maintain, or enhance a desired character for an area. They link the broad policy direction of the General Plan with more detailed policies and implementation projects for specific geographic areas of the city. The Character & Design Element of the General Plan includes a map showing adopted and future Character Areas.

### **Action Steps:**

- Prioritize Character Area Plans over a 10-20 year timeframe. Create approaches and timeframes for updating adopted plans and creation of future plans. New and updated Character Area Plans should be adopted **as minor General Plan amendments IN CONFORMANCE WITH STATE STATUTES**. Character Area Plans may recommend changes to the General Plan that may also constitute major or minor amendments, per the General Plan Amendment Criteria (e.g. change in Land Use category).

## **Neighborhood Plans**

The third-level of Scottsdale planning, Neighborhood Planning, complements the General and Character Area Plans. Neighborhood Plans are action-oriented and serve as tools to maintain and enhance the vitality of neighborhoods. Adopted Neighborhood Plans are illustrated in the Neighborhood Preservation & Revitalization Element.

### **Action Steps:**

- Develop a Neighborhood Planning Program.

## **Master/Strategic Plans**

Master and strategic plans are detailed plans for specific functions of the city. These plans typically include specific project recommendations, cost analyses, and other provisions and are updated on a 2-5 year basis. While the General Plan provides broad goals and community direction, master plans provide the methods and means for realizing them. Examples of master plans include, but are not limited to, the Transportation Master Plan, Fire/EMS Strategic and Standards of Coverage Plans, Integrated Water/**Wastewater RESOURCES** Master Plan, Community Services Master Plan, Police Department Strategic Plan, Tourism and Marketing Strategic Plan, Public Art Master Plan, Economic Development Strategic Plan, Airport Master Plan, **Sustainability Plan, Heat Island Mitigation Plan**, and Stormwater Master Plan. These plans are distinguished from master development plans for specific development projects, which are governed by the Zoning Ordinance.

### **Action Steps:**

- Examine, and update if necessary, all adopted master/strategic plans, and create new master/strategic plans when appropriate, upon ratification of the General Plan.



## **Design Guidelines and Standards**

Design guidelines and standards provide the framework for evaluating proposals on the basis of design, architecture, context, compatibility, landscaping, and other factors. Scottsdale's design guidelines include, but are not limited to, the Design Standards and Policies Manual (DS&PM), Scenic Corridor Design Guidelines, Lighting Design Guidelines, Sensitive Design Principles, Green Building Program, Downtown Urban Design and Architectural Guidelines, and Commercial Retail Design Guidelines.

### **Action Steps:**

- Evaluate existing design guidelines and standards for consistency with the General Plan, and update and/or create new guidelines as appropriate to meet the vision, values and goals of the community.

## **Capital Improvement Plan**

The Capital Improvement Plan (CIP) manages the timing and location of needed public improvements, such as flood control, water and sewer services, streetscape and traffic improvements, police and fire stations, and community facilities. The CIP sets priorities and funding for capital improvement projects annually.

### **Action Steps:**

- Annually evaluate and update the CIP to include any needed public improvements as a result of the General Plan and subsequent Character Area, Neighborhood and/or master/strategic plans.

## **Aligning the General Plan and the City Budget**

The General Plan includes an ambitious list of implementation programs over a 20 year period. Given the limited resources of the city, it is not possible to simultaneously fund implementation of every goal and policy. Effective implementation will require prioritization of programs and projects prior to determining funding.

### **Action Steps:**

- ~~State law requires the creation of a coordinated program submitted to the Planning Agency for review and report as to the conformity with the adopted General Plan. Therefore, when adopting the City Budget and Capital Improvement Plan, the City Council shall include a finding of consistency with the General Plan.~~ CITY COUNCIL PRIORITIZES IMPLEMENTATION FUNDING THROUGH THE ANNUAL ADOPTION OF THE CAPITAL IMPROVEMENT PLAN AND THE CITY BUDGET.

## 2) Funding Sources

Successful implementation of the General Plan may be realized through a variety of funding sources. The programming of city capital projects and their funding over time is outlined in the city's Capital Improvement Plan, which is updated annually. The following are examples of revenue sources used by or available, to support development, maintenance and/or operation of city services and facilities:

- **City Budget:** Scottsdale is required by law to adopt a budget each year and cannot spend more than the total amount budgeted. The Mayor and City Council identify and fund a number of priorities annually; ~~however, when adopting the City Budget, the City Council is required to find consistency with the General Plan.~~
- **Tax Revenue:** Scottsdale imposes three types of taxes—Transaction Privilege (Sales) Taxes & Use Taxes, Property Taxes, and Transient Occupancy (Bed) Taxes. Each of the allocations are distributed to various city services and projects, such as, transportation, purchasing land for the Scottsdale McDowell Sonoran Preserve, public safety, and tourism development.
- **Municipal Bonds:** Bonds are essentially loans made to the city by people or organizations that purchase bonds in a public offering. Bond funding is used for city improvements; of which, there are many types of bonds. For example, some fund street enhancements and others fund water and sewer improvements. Bonds must be repaid with a committed funding stream, such as property taxes, sales taxes, user fees, or other consistent revenue source that can be dedicated to repaying the debt.
- **Exactions:** Exactions ~~are a condition~~ **MAY BE** attached to a discretionary permit. For example, before granting a permit, a conservation easement or public access easement might be requested.
- **User Rates and Fees:** Users pay fees for a variety of city services including, refuse collection, water and wastewater service, recreation services, and library services.
- **Special Districts:** Special Districts are designated areas within Scottsdale in which a service is provided that is not typically provided by the city, such as a particular infrastructure improvement. Special Districts may have the power to tax; issue municipal bonds; set fees; or obtain funds from local, State, or Federal appropriations.
- **Impact/Development Fees:** There are generally three types of development ~~and impact~~ fees: 1) planning fees, which cover the administrative review costs of required planning documents; 2) building permit, plan check, and inspection fees, which cover review costs of building permits and other permit applications; and 3) capital facilities fees, which cover the up-front costs of providing public capital infrastructure.
- **County, State, and Federal Funding:** A variety of funding sources exist to assist municipalities in implementing the General Plan, such as allocated/shared tax funds, grants, tax credits, and loans.

### 3) Oversight and Coordination

Scottsdale will take an active leadership role in promoting use and implementation of the General Plan. However, implementation cannot rest on the city alone. The private sector, non-profits, and community members are pivotal to successful implementation. It will take the concerted efforts of residents, businesses, and city boards and commissions, to name a few, to bring the General Plan from vision to reality.

#### Intergovernmental Coordination

Scottsdale must coordinate with numerous local, regional, State, and Federal agencies to implement the General Plan. These agencies provide services, facilities, **and** funding, and administer regulations that directly or indirectly affect many goals addressed in the General Plan. The following are agencies that play a role in implementing the General Plan, **including but not limited to:** adjacent municipalities, school districts, **MILITARY FACILITIES, SALT RIVER PIMA-MARICOPA INDIAN COMMUNITY (SRPMIC)**, Maricopa Association of Governments (MAG), Arizona Department of Transportation (ADOT), Arizona Department of Environmental Quality (ADEQ), Arizona Commerce Authority (ACA), **Arizona Department of Water Resources (ADWR)**, **FLOOD CONTROL DISTRICT OF MARICOPA COUNTY (FCDMC)**, Federal Emergency Management Agency (FEMA), Federal Aviation Administration (FAA), Arizona State Land Department (ASLD), and the Greater Phoenix Economic Council (GPEC).

#### Joint Partnerships with the Private and Non-Profit Sectors

Scottsdale can combine its efforts with private and non-profit sectors to improve public services, manage public sector assets, or leverage private sector investments. By expanding the role of the private sector, the city can use its technical, management, and financial resources in creative ways to achieve the goals and policies of the General Plan.

#### City Boards and Commissions

City of Scottsdale Boards, Commissions and Task Forces are critical in implementing the General Plan. Thus, all members of a public body must be knowledgeable about the General Plan. The city will educate public bodies on the General Plan through a variety of methods, such as in orientation materials, presentations and discussions, and seeking input on future updates to the General Plan.

#### Individual Residents, Businesses, Project Sponsors, and Community Groups

The General Plan is a statement of community goals written by and for the community. Thousands of work hours over decades have resulted in the General Plan. To ensure that the community-at-large, businesses, and project applicants and sponsors are familiar with the content and community vision, the city will provide educational materials, presentations, community discussions, and other services that promote its implementation.

#### 4) Process and Programs

After ratification of the General Plan, the city will begin reviewing existing plans, documents, and regulations for consistency. For example, the city will review the Zoning Ordinance to see if any changes are needed to carry out the intent of the General Plan. These changes could encompass anything from definitions to development standards.

Per the Community Involvement Element and State and local laws, community input will be a part of these implementation programs. Not all goals and policies may be implemented at one time due to the long-term nature of the plan and budgeting/funding requirements.

The tables on the following pages delineate programs that implement the General Plan. It is assumed that all programs listed will be reviewed and updated to conform to applicable General Plan elements. Other programs may be created or updated that are not listed but are still intended to implement the General Plan over its 20 year lifespan. **Timeframes are general, expected timeframes. The City Council may choose to fund or not fund programs at any time.**



**Implementation**

CHARACTER & CULTURE						
Program	Elements Implemented	Years 1-5	Years 5-10	Years 10-20	On-Going/ Periodic	Responsible Agency
Airport Part 150 Program Implementation	C, CD, EV, LU, S				■	Airport
Arts & Cultural Ordinances Update	ACC, CD, GA, LU, T	■	■			Planning & Development
Arts/Cultural Strategic/Master Planning	ACC, EV, GA, HC, LU, T	■	■		■	Contracted Agency
Character Area Plans-Prioritize/Create New	CD, GA, LU, OS	■				Planning & Development
Character Area Plan Implementation	C, CD, EV, GA, LU, OS				■	Citywide
Crime Prevention through Environmental Design (CPTED) Program	ACC, CD, CONSV, CRR, H, LU, NPR, S				■	Police, Planning & Development
Design Guidelines (e.g. Sensitive Design Principles, Visually Significant Roadways, Gateways)	ACC, C, CD, CONSV, CRR, EP, GA, HC, LU, NPR, OS, PB				■	Planning & Development
Design Standards & Policies Manual Update	CD, OS, EP, CONSV, ACC, LU, H, NPR, EV, PB, HHC, C				■	Planning & Development
Development Review Process Refinement	ACC, CD, CI, COD, CONSV, CRR, EP, EV, H, HC, LU, NPR, OS				■	Planning & Development
Downtown Infill Incentive District Evaluation/Update	ACC, CD, COD, CRR, EV, GA, LU, NPR, OS	■				Planning & Development
Historic & Archaeological Preservation Programs/Update	ACC, CD, CRR, EP, EV, GA, H, LU, NPR, OS, T	■	■		■	Planning & Development
Zoning & Related Code Updates (e.g. Land Division, Building Codes) (see Implementation Tools section for further detail)	ACC, CD, COD, CRR, EV, EP, GA, H, HC, LU, NPR, OS, S, T	■			■	Planning & Development

ELEMENT LEGEND
ACC= Arts, Culture & Creative Community
All- All Elements/ Entire General Plan
B= Bicycling
C= Circulation
CD= Character & Design
CI= Community Involvement
COD= Cost of Development
CONSV= Conservation
CRR= Conservation, Rehabilitation, & Redevelopment
E= Energy
EP= Environmental Planning
EV= Economic Vitality
GA= Growth Areas
H= Housing
HC= Healthy Community
LU= Land Use
NPR= Neighborhood Preservation & Revitalization
OS= Open Space
PB= Public Buildings
PSF= Public Services & Facilities
R= Recreation
S= Safety
T= TOURISM
WR= Water Resources

SUSTAINABILITY & ENVIRONMENT						
Program	Elements Implemented	Years 1-5	Years 5-10	Years 10-20	On-Going/ Periodic	Responsible Agency
Assured Water Supply	EV, S, WR				■	Water Resources
Drought Management Plan Updates	WR, S				■	Water Resources
Energy Efficiency & Clean Fuel Code Revisions	CD, CRR, E, EP, H, LU		■			Environmental Initiatives
Green Building Program & Code Review/Update	CD, CONSV, CRR, E, EP, GA, H, HHC, NPR, PB				■	Environmental Initiatives
Heat Island Mitigation Plan	CD, CONSV, EP	■		■		Environmental Initiatives
Infrastructure Improvements Plan	COD, CONSV, CRR, EV, GA, OS, PSF	■	■	■		Water Resources
INTEGRATED Water/Wastewater RESOURCES Master Plan Review/Update	COD, CONSV, CRR, EP, LU, WR	■		■		Water Resources
Scottsdale McDowell Sonoran Preserve Land Acquisition	ACC, EP, EV, LU, OS, R, T				■	Preservation
Scottsdale McDowell Sonoran Preserve Maintenance/Access Improvements	ACC, CONSV, HC, EP, EV, LU, NPR, OS, R, T				■	Community Services, Preservation
Native Plant Ordinance	CD, CONSV, EP, GA, H, HC, NPR, OS		■		■	Planning & Development
Net-Zero Energy Strategic Plan Creation	CRR, E, EP, LU		■	■		Environmental Initiatives
Recycling/Solid Waste Programs Review/Update	COD, CONSV, E, EP, PSF	■		■	■	Solid Waste
Stormwater Program & Master Plan Update	CRR, CONSV, EP, LU, OS, PSF, S		■		■	Stormwater
Sustainability Plan	CD, CONSV, E, EP, EV, GA, H, HC, LU, OS, PB	■	■			Environmental Initiatives
Water Conservation Program	CONSV, EP				■	Water Resources
Water Quality Reporting	CI, CONSV, EP, WR				■	Water Resources

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**Implementation**

COLLABORATION & ENGAGEMENT						
Program	Elements Implemented	Years 1-5	Years 5-10	Years 10-20	On-Going/ Periodic	Responsible Agency
Citizen Survey	CI, HC, S				■	Administration
CITY PUBLIC PARTICIPATION MANUAL	CI, HC	■			■	ADMINISTRATION
Community Outreach Programs	ACC, B, C, CD, CI, CONSV, CRR, E, EP, H, HC, NPR, OS, S				■	Citywide
Community Visioning	ACC, CD, CI, HC, T			■		Planning & Development
General Plan 5-year & Annual/Reports	CD, CI, EV, LU, T		■		■	Planning & Development
City Council Annual Priorities/ Organization Strategic Plan	All				■	Citywide
Public Involvement Plans for projects	ACC, C, CI, CRR, EP, H, HC, R				■	Private Sector
Public Notification Requirements Update	ACC, CI, CRR, H, HC, LU	■				Planning & Development, Neighborhood Services

CONNECTIVITY						
Program	Elements Implemented	Years 1-5	Years 5-10	Years 10-20	On-Going/ Periodic	Responsible Agency
Airport Master Plan Update	C, COD, EP, EV, LU, PB, PSF, S, T		■			Airport
Bicycle Program	B, C, CRR, EP, EV, GA, HC, LU, NPR, R, S, T				■	Transportation
Transit Program	B, C, CRR, EP, EV, GA, H, HC, LU, NPR, PSF, T				■	Transportation
Transportation Master Plan Update	B, C, COD, CRR, EP, EV, GA, H, HC, LU, NPR, OS, PSF, S	■				Transportation

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COMMUNITY WELL-BEING

Program	Elements Implemented	Years 1-5	Years 5-10	Years 10-20	On-Going/ Periodic	Responsible Agency
ADA Program	ACC, C, CD, CRR, H, HC, PB, PSF, R				■	Citywide
CDBG Strategic Action Plan	CRR, H, HC, LU, PSF, R				■	Human Services
Community Health Assessments	EP, H, HC, LU, NPR, PB, S		■			Healthcare Providers, Planning & Development, Neighborhood Services
Community Services Master Plan Review/Update	ACC, COD, EV, HC, LU, NPR, OS, R, PB, PSF		■			Community Services
Diversity AND INCLUSION ProgramS	ACC, CI, H, HC, PSF, R, T	■			■	Administration
Emergency Management Plan & Program	CD, HC, LU, NPR, S	■	■		■	Public Safety
Fair Housing Program	H, HHC				■	Human Services
Fire Department Strategic and Standards of Coverage Plans	CONSV, COD, CRR, EV, HC, LU, NPR, PSF, S, T				■	Fire
Gardens, Farmers Markets Code Updates	HC, LU, NPR	■				Planning & Development, Neighborhood Services
Hazardous Materials Compliance Program & Planning	CONSV, EP, PSF, S				■	Environmental Initiatives
Housing/Human Services Programs and Consolidated Plan Update	ACC, COD, CRR, EV, H, HC, NPR, PSF, R	■	■	■	■	Human Services
Housing Rehabilitation Programs Review	CRR, H, HC, NPR				■	Human Services
Parks & Recreation Master Plan Review/Update	ACC, B, C, COD, EP, EV, GA, LU, OS, NPR, PSF, R, T		■			Parks & Recreation
Police Department Strategic Plan	B, CI, COD, GRR, EV, HC, LU, NPR, PSF, S, T				■	Police
Residential Healthcare Facility/Adult Care Home Codes/Policies Update	LU, EV, H, HC	■		■		Planning & Development, Human Services
School District & Regional Safety Coordination	B, C, HC, NPR, S				■	Public Safety
Senior Services Programs	ACC, H, HC, R				■	Community Services
Strategy for Preservation & Creation of High-Quality, Safe, and Affordable Housing	ACC, CRR, H, HHC, LU		■			Human Services
Trails Master Plan Review/Update	B, C, EP, EV, GA, HC, LU, NPR, OS, R, T	■				Community Services

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**Implementation**

REVITALIZATION						
Program	Elements Implemented	Years 1-5	Years 5-10	Years 10-20	On-Going/ Periodic	Responsible Agency
Asset Management Programs	ACC, CD, CRR, E, EV, GA, NPR, PB, R				■	Public Works, Water Resources
Capital Improvement Plan/Program	ACC, C, COD, CRR, EV, HC, LU, NPR, OS, PB, PSF, R, S, T				■	Public Works
City Facilities Master Plan	ACC, HHC, PB, EV, R, C, PSF, CRR, COD, E		■			Public Works
Community-Building & Neighborhood Organization Programs	ACC, CRR, H, HC, NPR, PSF				■	Neighborhood Services
Community Policing Programs/Update	CI, HC, NPR, S		■		■	Neighborhood Services, Public Safety
Development & Demographic Forecasting/Analysis	ACC, C, CD, CI, COD, CRR, EV, GA, H, HC, LU, OS, PSF, R, T		■		■	Planning & Development
Fiscal Sustainability Analysis	ACC, CI, COD, CRR, EV, GA, LU, OS, PSF, R, T				■	Economic Development, Planning & Development
Impact & Development Fees Update	COD, H, NPR, OS, R				■	Planning & DEVELOPMENT, Water Resources, <del>Financial Services</del> CITY TREASURER'S OFFICE
Library Programs	ACC, COD, GA, HC, PSF, R, T				■	Library
Neighborhood Planning Program	ACC, H, HC, LU, NPR			■		Neighborhood Services, Planning & Development
Neighborhood Preservation & Conservation Programs Development/Update	ACC, CD, CI, CRR, EV, GA, H, HC, LU, NPR, OS	■			■	Planning & Development, Neighborhood Services
Property Maintenance Code/Code Enforcement Program Review/Update	CD, CRR, EP, H, NPR, S		■		■	Neighborhood Services
Redevelopment Plan-Updates	ACC, CD, CRR, H, LU, OS	■				Economic Development, Planning & Development

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INNOVATION & PROSPERITY						
Program	Elements Implemented	Years 1-5	Years 5-10	Years 10-20	On-Going/ Periodic	Responsible Agency
Business Attraction/ Retention Programs	ACC, C, CRR, EV, HC, LU, NPR				■	Economic Development
Competitive Position Analysis	ACC, EV, HC, LU, T	■				Economic Development
Economic Development Strategic Plan Update	ACC, CRR, EV, GA, H, HC, LU, NPR	■	■	■		Economic Development
HIGH PERFORMANCE AND INNOVATION INITIATIVE	ALL	■			■	CITY MANAGER'S OFFICE
Lodging and Visitor Statistics Studies	ACC, EV, HC, T				■	Tourism
SMART CITIES STRATEGIC ROADMAP	ALL	■	■	■	■	CITY MANAGER'S OFFICE
Tourism & Marketing Strategic Plan & Implementation	ACC, EV, HC, LU, OS, T		■		■	Tourism

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## 5) Measuring Progress

A community's ability to prepare and respond to change is an indication of its resiliency and sustainability. A regular system of review, monitoring, and adjustment will measure progress towards achieving the General Plan's short- (1-5 years), mid- (5-10 years), and long-term (10-20 years) goals and policies, and ensure that the General Plan responds to emerging trends, issues and opportunities.

### General Plan Update

The General Plan is in effect for up to ten years from the date the plan is ratified. Arizona law requires that at the end of the ten year period, the City Council will either readopt the existing General Plan or adopt a new General Plan.

### General Plan Progress Reporting

#### **Annual Assessment Report†**

As required per State Statute, an Annual Assessment Report will be compiled and provided to City Council. The report will assess the progress of the city in achieving the vision, values, goals, and policies of the General Plan. At a minimum, the Annual Assessment Report will include the following :

- Information collected at meetings with relevant city departments, public bodies, and community members that represents how the General Plan has been implemented;
- An annual listing and description of any major and minor General Plan amendments;
- Progress on the implementation of each Chapter;
- An analysis and recommendation of modifications needed to clarify or update the General Plan;
- A presentation of the Annual Assessment Report to the Planning Commission; and
- Distribution of the Annual Assessment Report to City Council.

#### **Five-Year Assessment Report**

Completion of a comprehensive five-year General Plan assessment report is recommended. This report should analyze community trends - land use changes, demographics, socio-economic projections, and other emerging issues and opportunities - so as to recommend adjustments that may be necessary to include in the State-mandated, ten-year General Plan update process.

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Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
<b>Land Use Element</b>					
1	General	Again, my major concerns are mediocre public transportation and land use. The City Council is adverse to light rail and seems to favor high rise buildings and more residential and commercial use in close areas. COVID-19 is a toxic virus and will not go away. This will affect future use of any proposed projects.	Public Comment	No action was requested by the public comment. CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/5/2020
2	General	Rural areas are being lost to incompatible upzoning that is destroying rural lifestyles and creating suburban housing tracts in rural residential areas. Rural refers to large lot sizes, not homes on less than 1 acre, and is meant to accommodate rural activities. Separate the Rural Neighborhoods Land Use category into TWO separate categories. Any change from lower rural density to a higher rural density would require a Major General Plan Amendment. This requires more Council scrutiny and the minimum of a 5-2 vote for project approval. This change would use the approved General Plan 1996-2000 Rural categories and terminology. The two categories would be: Rural Neighborhood 1-2 acre (R1-143, R1-70) Rural Neighborhood 2+ 5 acre (R1-130, R1-190) pg. 64-Land Use Patterns, Line 1: revert "can" back to "should" Line 4: reinstate "must" in wording pg. 70-Goal LU7: Reinstate original LU7.1 to protect airport operations from damaging development pg. 73 Non-Residential Land Use Categories, Commercial, lines 5-6: Reinstate the sentence, "The size and scale of neighborhood business centers should be compatible with surrounding areas." This protects neighborhoods from excessive commercial development which can drastically change neighborhood character and quality.	Public Comment	City Council previously directed staff (December 1, 2016) to not move forward with separating Rural Neighborhoods into separate Land Use Categories due to a mixed reaction of support and opposition to such a change by the community. Furthermore, creating new Land use regulations on a property owner could lead to a Prop 207 Private Property Act risk of claim against the City. Requested changes in zoning categories are subject to approval by City Council.  However, the Rural Neighborhoods definition has been updated to better-define large-lot character and preservation techniques utilized within this Land Use Category.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
3	General	We believe that the Conceptual Land Use Map should be more detailed, identifying specific boundaries between Character Types. We also believe that the Conceptual Land Use Map should be coordinated with the Zoning Ordinance and its associated zoning maps. Coordinating these maps will clarify boundaries and eliminate subjective interpretations of land use. The General Plan repeatedly addresses neighborhood protection, discussing the need to preserve and protect the uniqueness and identity of each. The General Plan states that Scottsdale is committed to maintaining existing neighborhoods. This action would help accomplish that. Over the last several years we experienced an assault on the uniqueness of our neighborhood.  The assault occurred specifically because of a non- detailed Conceptual Land Use Map. Our neighborhood is located along East Jenan Drive between Scottsdale Road and Miller Road. There is a small corner of this area at the northwest corner of Scottsdale Road and Cholla Street that is zoned R1-10, and another area at the northwest corner of Scottsdale Road and Cactus Road that is zoned R1-18. The remainder of the larger area between Scottsdale Road and Miller Road is zoned R1-35. The existing General Plan Conceptual Land Use Map shows both these northwest and southwest areas identified as Suburban, while the remaining area is identified as Rural.  The problem is that the map is not detailed to the specific streets or properties. We believe that it was the intent of the authors of this map that the two areas zoned differently from R1-35 were to be designated as Suburban while the remaining areas were to be designated Rural. If you do a direct overlay with the non-detailed Land Use Map and the neighborhood the result is that the Suburban designation extends beyond the originally intended zoning boundaries resulting in properties zoned R1-35 having split designations of Rural and Suburban.  This is clearly not the intent of the authors of the existing General Plan, but it resulted in the interpretation that all of the properties on the south side of Jenan Drive were in the Suburban designation. It did not mater that these properties were clearly Rural as identified in the General Plan. They were large-lot, single family, usually one house per acre or more, and included equestrian uses.  They were not Suburban as identified in the General Plan - medium to small lot single family, usually more than one house per acre and small lot single family. The result of this incorrect interpretation was a year and a half battle with a developer attempting to place a small gated community in a Rural neighborhood. We was able to prevail against this attempt. The amount of time expended by the City Staff, the Planning Commission, the City Council, and the neighborhood members could have been prevented if the Land Use Map and the zoning maps were coordinated.  The General Plan 2035 Draft included the Future Land Use Map, page 80. This map includes a detailed depiction of our neighborhood, including the smaller streets. The authors of this draft plan concluded our assumption that the Rural/Suburban designation should align with the zoning maps was correct. This detail and alignment should be included in the new General Plan. Thank you.	Public Comment	The 2001 General Plan is a generalized non-parcel-based map while the draft 2035 plan is parcel based. In both instances – the property described by the commenter is designated as Suburban Neighborhoods. The commentary that describes the history for their neighborhood is case specific. In that case, the leading consideration for Council to consider was the requested zoning district that would implement the General Plan and not the General Plan land use designation. Notably, the case that was submitted has since been withdrawn as described by commenter.  Working with the 2001 General Plan conceptual land use map at times can lead to questions, and can necessitate having staff make a land use determination based on the history of a property because the map is so generalized, is not parcel-based (meaning at times land use designations do not always perfectly align with property boundaries) and the map is not scale. Due to the generalized nature of the map, at times staff has to make a land use determination as to the land use designation for a property – as is the case with the Jenan property.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
4	General	I cannot figure this site out and I am looking for the parcel of land behind my home. It is located on Jenan just east of Scottsdale Rd. We defeated Camelot Homes and their desire to put four homes per acre on this parcel. All of the neighbors had signed a legal protest to stop the development and Julie Hancock knew she did not have the support of the City Council members. I have tried for hours to figure out how to enlarge that plat map and read it and for the average person I can't figure it out. Too complicated for me. I want to make sure everyone in the City knows that none of the neighbors in this area of town support the rezoning that Camelot tried to push through. We all stood up and made our voices heard. I was in the newspaper a couple of times with another neighbor Lauren Grey. We had signs all the way up 74th place south of Cactus and North of Cholla DontRezone. I wish I could figure out your site to see more detail on the colored boxes. I have a landline and it's down this morning or I would be calling someone. This was very rushed at a time of Covid and your offices are closed it makes it hard to come in and ask questions. I made three calls to people that were never returned and I was going to try again today but this morning my phone was not on. Please do not rezone on Jenan. All the neighbors are against it. That street has a church and a school and does not need the extra traffic. No one wants the noise it would bring either. When I bought my home it was zoned rural and I want to keep that. With all the big houses going into the neighborhood someone will buy it and put something nice there. Camelot homes tried to bully all of us telling us they could do whatever they wanted to do but we all stood up to them and with a legal protest they gave up for now but if they can pay off enough people they will get their way. Stand up for your citizens please against the greed of the builders to divide and conquer. Thank you. I want this comment to be put in the objection file if indeed the land is going to be rezoned. I cannot read your maps at all and don't know.	Public Comment	This appears to be a comment specific to a rezoning case and not the draft General Plan (20-ZN-2018 & 2-ZN-2020). There are no changes to the land use category along Jenan within this General Plan update.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
5	Intro Text p. 64	Restore the original text as it is clear. It is not clear what "alternative" means without automobile. Also, Land use patterns must achieve the overall vision of what the community wants their city to be.	COGS	The CRC discussed this paragraph on both 10/12/2020 and 11/16/2020 and recommended the wording as presented in the Land Use Patterns paragraph. The reasoning given by the CRC was that, since the City is currently dependent on the automobile for travel, it was unlikely that the dependence would be reduced over the next 20 years and that it was more likely that "alternative mobility options" would occur, such as enhanced pedestrian or bicycling infrastructure or transit.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020

Enclosure 9

Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
6	Intro Text p. 64	Page 64. Under "Land Use Patterns," "alternative mobility" is unclear unless it specifies what the "alternative" modes are alternatives to. Presumably, they are alternatives to automobiles, but this should be specified.	Public Comment	The CRC discussed this paragraph on both 10/12/2020 and 11/16/2020 and recommended the wording as presented in the Land Use Patterns paragraph. The reasoning given by the CRC was that, since the City is currently dependent on the automobile for travel, it was unlikely that the dependence would be reduced over the next 20 years and that it was more likely that "alternative mobility options" would occur, such as enhanced pedestrian or bicycling infrastructure or transit.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
7	Intro Text p. 64	Page 64. Under "Land Use Patterns," I don't understand the use of "can." Is this a goal? Presumably, it is. If so, the wording should revert to "should" or "encourage."	Public Comment	This paragraph is a subsection of the Introduction of the Land Use Element. It is not a goal or policy, but a statement of what may result from various land use patterns throughout the City.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
8	Intro Text p. 64	64, "PROVIDE FOR ALTERNATIVE MOBILITY OPTIONS" Does "alternative" mean "alternative to automobiles"? That meaning will not be clear once the strike-through is gone. Recommendations: "reduce the dependence on the automobile" OR "provide alternative mobility options to the automobile"	Public Comment	CRC discussed that the automobile is likely not going away, and that instead of reducing dependence the text could discuss that land use patterns could provide alternative options.  The CRC considered this comment on 11/16/2020 and did not incorporate the requested change.	11/13/2020
9	GP 2035 Land Use Mixture Table p. 65	65, Would "dwelling unit" be the more correct term here (e.g., 1 dwelling unit per 1 or more acres)?	Public Comment	The CRC considered this comment on 11/16/2020 and did not incorporate the requested change, keeping the language in the table as was previously presented.	11/13/2020
10	Land Use Mixture text p. 65	Grammar Correction  65, . . . delineate criteria that <del>is</del> ARE carefully considered . . .	Public Comment	The CRC considered this comment on 11/16/2020 and <b>incorporated the grammatical change.</b>	11/13/2020
11	Land Use Mixture Table p. 65	Land Use Mixture table: Change "dwelling" to "dwelling unit". There is a big difference. See Glossary definitions.	COGS	CRC agreed to amend the General Plan 2035 Land Use Mixture table to utilize the phrase "Dwelling <b>UNIT</b> " throughout. Furthermore, amend the Urban Neighborhoods definition on page 72 to state "Densities in Urban Neighborhoods are usually eight or more dwelling <b>UNITS</b> per acre."	12/3/2020
12	Land Use Mixture Table p. 65	On the chart on page 65, I would retain the breakout line for the Preserve.	Public Comment	The Preserve is not a land use category within the 2001 General Plan, where it is designated as Natural Open Space. During the 2014 General Plan process, there was a proposal to have it be its own Land Use, however, the Preserve is better protected under the Preserve Ordinance and charter than by giving it its own land use designation. As such, in June 2015, City Council directed the removal of the McDowell Sonoran Preserve as a proposed land use category. Consequently, the Land Use Mixture table was updated to reflect the Preserve's acreage as Natural Open Space. Furthermore, land use definitions on page 74 reflect the Preserve as a subset of the Natural Open Space category, similar to how it is treated in the existing 2001 GP.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
13	Land Use Mixture p. 65	On page 65, I would delete the paragraph regarding aggregates since it is irrelevant	Public Comment	Arizona State Statutes (9-461.05.C.1.g) requires General Plans to identify aggregates. Consequently, this statement in the draft plan is necessary and must remain.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
14	Land Use Mixture Table p. 65  Natural Open Space Land Use Category Definition p. 74  Change in Land Use Category table p. 76  Exceptions to GP Amendment Criteria p. 77	In June 2015, City Council directed the removal of the McDowell Sonoran Preserve as a land use category.	City Council	Updated:  Land Use Mixture table has been updated to reflect the Preserve's acreage as Natural Open Space. Furthermore, land use percentages and acreages across the board were updated to reflect <b>up-to-date calculations.</b>  The Preserve will be categorized as Natural Open Space in the redline just as it is in the existing 2001 GP. The existing General Plan language includes: Preserve land will remain as permanent open space with limited permanent construction. McDowell Sonoran Preserve definition has been updated to discuss Natural Open Space designation:  <del>The Natural Open Space Category includes the McDowell Sonoran Preserve. The Preserve</del> <del>The McDowell Sonoran Preserve Category</del> consists of mountain and desert land included in the Scottsdale McDowell Sonoran Preserve.  Change in Land Use Category has been updated to reflect McDowell Sonoran Preserve as Natural Open Space designation:  <del>McDowell Sonoran Preserve</del> <b>(NEW)</b>  Major Amendment Criteria has been updated to reflect McDowell Sonoran Preserve as Natural Open Space designation:  To facilitate completion of the Scottsdale McDowell Sonoran Preserve, any cityinitiated change from any General Plan Land Use Category to the <del>McDowell-Sonoran Preserve</del> <b>Natural Open Space</b> Land Use Category on city-acquired parcels for the Scottsdale McDowell Sonoran Preserve will be exempt from a General Plan Amendment process.	Pre 2020
15	Goal LU 1 p. 66	Goal LU 1. Change to read: "Enhance Scottsdale's economic viability by encouraging land uses that reinforce the city's reputation as the premier southwestern tourist destination, cultural center, economic hub and are compatible with Scottsdale's brand, character and physical appearance."	Coalition of Greater Scottsdale (COGS)	Updated:  LU 1 - <del>Enhance Scottsdale's economic viability by encouraging Encourage</del> land uses that <del>contribute to economic viability and community character to sustain and enhance Scottsdale's role as a major</del> reinforce the city's reputation as the premier southwestern tourist destination, cultural center, economic hub, and are compatible with Scottsdale's brand, character, and physical appearance <del>regional economic hub, tourist destination, and cultural center.</del>	Pre 2020

Enclosure 9

Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
16	Policy LU 1.3 p. 66	Policy LU 1.3 - Delete "destination"	Coalition of Greater Scottsdale (COGS)	Updated: LU 1.3 - Promote <del>destination</del> resorts in conjunction with recreation, retail, housing, and cultural uses that support tourism and sustain a resort-like lifestyle.	Pre 2020
17	Policy LU 1.3 p. 66	66LU 1.3 (formerly 1.4) - staff offered to bring back (bring back was not requested by CRC majority)  original: Promote development patterns that integrate with and complement surrounding uses and reinforce an area's character.  yellow highlight: Promote development patterns that integrate with and reinforce the character of an area, or establish new character where development patterns are fragmented or evolving.  The bring back was prompted by this question posed by a CRC member: "What if the character is outdated or not supported by community, residents, or leaders?" The member then requested the policy be revised to allow a developer to change a character "that is not good."  One purpose of City plans is to advise and assure those who purchase property what they can expect in the future. Residents and property owners want and need to count on those assurances. Of course, plans evolve and the City changes. To accommodate evolution and change, we have established processes.  If a developer or anyone else believes a character "is not good" and should be changed, the change must be pursued through City planning processes. Similarly, if the character of an area is "fragmented" or "evolving," the area's uses and character may be changed through the City's planning processes. After changes are made through those processes, the development patterns would then integrate with, complement and reinforce the newly-adopted uses and character, per the original LU 1.3 policy language. In other words, the original policy language, coupled with the City's established processes for change, covers the scenario in the CRC member's question.  With respect to the yellow highlighted language, a policy in the General Plan Land Use element should not promote development/redevelopment that would be incompatible with surrounding uses/character in order to establish new character.  Action requested: maintain original language	Public Comment	The CRC considered this comment on 11/16/2020 and instead updated as follows:  LU 1.3 - Promote development patterns that integrate with and reinforce <b>THE</b> character <b>OF AN AREA, OR ESTABLISH NEW CHARACTER WHERE DEVELOPMENT PATTERNS ARE FRAGMENTED OR EVOLVING.</b>	11/13/2020
18	Goal LU 1 p. 66	LU 1  Action requested: revise language as follows ... reinforce the city's reputation as the premier southwestern tourist destination, cultural center, <b>and</b> economic hub ...  Reason: The grammatical structure of the sentence is: land uses that a) reinforce the city's reputation as a tourist destination, cultural center and economic hub and b) are compatible with . . .	Public Comment	CRC agreed to amend the Goal to read as follows:  Goal LU 1 - Enhance Scottsdale's economic viability by encouraging land uses that reinforce the city's reputation as the premier southwestern tourist destination, cultural center, <b>AND</b> economic hub, and are compatible with Scottsdale's character and physical appearance.	12/4/2020
19	Policy LU 1.3 p. 66	Changing or worse developing a new character area should not be considered here. If the character is not well defined or is evolving, then change the character area and plan, <b>don't ever allow a development to change the character. Remove all references to establishing a new character.</b>	COGS	The CRC discussed this at both their 10/12/2020 and 11/16/2020 meetings, deciding to incorporate the language as represented on page 66. The reasoning regarding this particular update to the policy was that some areas of the city may not have established character or an adopted Character Area Plan.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
20	Policy LU 1.3 p. 66	Page 66. Under LU 1.3, it is not clear who is responsible for "establishing new character" areas. Any change should be made through proper processes.	Public Comment	Policy LU 1.3 discusses the character "of an area" and is not specific to adopted Character Area Plans. Land use patters are supported by the community's vision and any change to such is ultimately approved by City Council.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
21	Policy LU 3.5 p. 68	68CRC member request for a new policy LU 3.5  Staff indicated the policy might be covered by CI 1 and CI 2.1. In lieu of adding LU 3.5, a cross-reference was added to LU 3.3.  •The added cross-reference, while a positive addition, does not seem to achieve the result desired by proposed LU 3.5. The added cross-reference does not specify the goal (CI 1) and policy (CI 2.1) shown on the slide (during the 10/12/20 CRC meeting) because cross-references state only elements. Furthermore, CI 2.1 is directed to providing notification, not to collecting community input. •There are numerous places in the Plan where citizen involvement is specifically is called out (with cross-reference to the CI element). Some examples are: CD 2.3, ACC 1.2, OS 6.1, H 1.3, R 1.1, NPR 4.1, and CRR 1.3. In other words, the presence of the CI element does not preclude a call-out for citizen involvement with respect to land use matters. •If CRC reconsiders adding policy LU 3.5, I suggest a general reference to "land use" as opposed to restricting it to "residential and neighborhood" land use. In other words: "Engage the community in land use discussions."	Public Comment	The CRC considered this comment on 11/16/2020 and instead added the following NEW Policy:  <b>LU 3.5 - ENGAGE THE COMMUNITY IN RESIDENTIAL AND NEIGHBORHOOD LAND USE DISCUSSIONS.</b>	11/13/2020
22	Policies 3.1, 3.2, and 3.3 p. 68	All of these policies should be re-written to be compatible with the vision rather than opening the door to any and all land use changes. Changes must also be balanced against their fiscal impact to the city, impacts to existing residents, and impacts to tourism.	COGS	Policies 3.1, 3.2, and 3.3 were carried forward from the 2001 General Plan and include discussion that meets state statute requirements. Furthermore, and as an aspirational document, the policies support the draft Vision and Values of the draft 2035 General Plan.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
23	Policy LU 3.5 p. 68	<b>Community input should NOT be limited to residential development as all development impacts residents and neighborhoods.</b>	COGS	CRC agreed to amend the Policy to read as follows:  LU 3.5 - ENGAGE THE COMMUNITY IN <del>RESIDENTIAL AND NEIGHBORHOOD</del> <b>ALL</b> LAND USE DISCUSSIONS.	12/3/2020
24	Policy LU 3.5 p. 68	Engage the community in " All LAND USE DISCUSSIONS."	Public Comment	CRC agreed to amend the Policy to read as follows:  LU 3.5 - ENGAGE THE COMMUNITY IN <del>RESIDENTIAL AND NEIGHBORHOOD</del> <b>ALL</b> LAND USE DISCUSSIONS.	12/3/2020

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Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
25	Goal LU 4 p. 69	Goal LU 4. Change to read: "Develop context appropriate land use patterns that support a variety of compatible mobility choices and services."	Coalition of Greater Scottsdale (COGS)	Updated: LU 4 - Develop <b>context appropriate</b> land use patterns that <del>are compatible with and</del> support a variety of <b>compatible</b> mobility choices and services	Pre 2020
26	Policy LU 4.1 p. 69	69LU 4.1  original language: Integrate land uses and mobility systems to allow for shorter and fewer automobile trips and greater choices for mobility.  11/16/20 draft without redline (changes incorporated): Integrate land uses and transportation systems to allow for a variety of mobility choices.  proposed alternative with redline (compared to 11/16/20 draft language): Integrate land uses and transportation systems to <del>allow for</del> <b>FACILITATE SHORTER AND FEWER AUTOMOBILE TRIPS</b> and a variety of mobility choices.  reasons: a)CRC struck all three references to reducing automobile use (page 64 and two on page 69). This would restore one of them. b)Shorter and fewer automobile trips is one outcome of integrating land uses and transportation systems. Having a variety of mobility choices is another. Neither one includes the other. Both should be stated.	Public Comment	The CRC did not have consensus on adding the suggested public comment language. Discussion included the possible need for more localized commercial uses to facilitate what the language was suggesting.  The CRC considered this comment on 11/16/2020 and did not incorporate the requested change.	11/13/2020
27	Policy LU 4.2 p. 69	LU 4.2. Change to read: "Provide opportunities for pedestrian-oriented development, reduced parking demand and context appropriate alternative mobility choices."	Coalition of Greater Scottsdale (COGS)	Updated: LU 4.2 - <del>Support mixed use and compact development patterns, to p</del> Provide opportunities for pedestrian-oriented development, reduced parking demand, and <del>support</del> <b>context appropriate</b> alternative mobility choices.	Pre 2020
28	Policy LU 5.3 p. 69	The best environmental solution is to keep the density LOW and that should be a goal, especially under LU 5.3	COGS	The City's Environmentally Sensitive Lands (ESL) Ordinance is adopted within Appendix B of the City Code of Ordinances. The General Plan is not regulatory document; however, implementing themes of ESL are referenced throughout the General Plan, including context-appropriate development and the Rural Neighborhoods land use definition.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
29	Policy LU 6.1 p. 70	Commercial and employment uses should be compatible with the overall vision. For instance, attracting tourism related, high end specialty retail, and other such businesses that complement and build on Scottsdale's strengths should be the goal. Businesses should support the direction not detract from it. They also need to contribute to fiscal sustainability.	COGS	The Future Land Use Map, updated through November 2020, was analyzed utilizing a Land Use Model, which helps note the fiscal sustainability of land uses throughout the City. The reporting found that the land use mixture in the 2035 General Plan is projected to be fiscally sustainable through 2030.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
30	Policy LU 6.2 p. 70	LU 6.2. Change to read: "Support well-planned, clustered employment centers of Healthcare, and Research and Development."  Rationale: to capitalize Healthcare, Research and Development makes it clear that the language is referring to those as employment centers.	Coalition of Greater Scottsdale (COGS)	Updated: LU 6.2 - Support well-planned, clustered <b>employment centers of H</b> Healthcare and <del>R</del> Research and <del>D</del> Development land uses.	Pre 2020
31	Policy LU 6.2 p. 70	LU 6.2. as written seems to be too limiting. Suggest adding similar or related uses to ensure compatability of uses within a clustered, employment center.	Public Comment	Updated: LU 6.2 - Support well-planned, clustered <b>employment centers of healthcare and research and development</b> related or similar uses such as <b>Healthcare and Research and Development</b> land uses.	7/14/2020
32	Policy LU 6.3 p. 70	LU 6.3 as written seems to be too limiting. Suggest "in proximity" instead of "next to".	Public Comment	Updated: LU 6.3 - Encourage commercial land uses of similar scale and character <del>next in proximity</del> to or within medium- to high-density residential areas to promote walkable connections.	7/14/2020
33	Policy LU 6.3 p. 70	70LU 6.3 - "of similar scale and character"  This policy <b>encourages</b> commercial land uses <b>within</b> medium to high-density residential areas. Per the Glossary definitions, that collectively includes single-family, two-family, and multi-family housing with > 1 to ≥ 8 dwelling units per acre. In other words, it includes all residential land use in the City except Rural Neighborhoods (one dwelling unit on one or more acres.) Within and near residential areas, it is very reasonable and very desirable to qualify the commercial land uses being encouraged to those "of similar scale and character."  Action requested: restore "of similar scale and character."	Public Comment	The CRC considered this comment on 11/16/2020 and did not incorporate the requested change.	11/13/2020
34	Policy LU 6.3 p. 70	<b>Restore the crossed-out text "of similar scale and character"</b> as this is critical to it fitting into the residential district.	COGS	CRC agreed to amend the Policy to read as follows: LU 6.3 - Encourage commercial land uses <b>of similar scale and character</b> in proximity to or within medium- to high-density residential areas to promote walkable connections.	12/3/2020
35	Policy LU 6.3 p. 70	Page 70. I would retain the stricken language in LU 6.3. We should encourage contextual transitions. The need for transitions is referenced on the following page.	Public Comment	CRC agreed to amend the Policy to read as follows: LU 6.3 - Encourage commercial land uses <b>of similar scale and character</b> in proximity to or within medium- to high-density residential areas to promote walkable connections.	12/4/2020



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Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
36	Policy LU 7.1 p. 70	LU 7.1. Change to read: "Promote safe operation of the Scottsdale Airport by prohibiting uses that will impede safe flight operations or negatively impact airport funding sources.	Coalition of Greater Scottsdale (COGS)	Modified language included in redline, based on direction from the Airport Director. Per legal counsel, the suggested change to "prohibit" should not be included in the General Plan.  Updated:  LU 7.1 - Promote safe operation of the Scottsdale Airport by discouraging uses that will impede safe flight operations or negatively impact airport funding sources.	Pre 2020
37	Policy LU 7.1 p. 70	Add back the original 7.1 as it is crucial to keeping the airport there and active which in turn is a critical part of Scottsdale, tourism, and high-end residential uses.	COGS	This deletion was proposed by Scottsdale Airport Staff as this specific policy is implemented via the Airport Vicinity development regulations (August 2012).  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
38	Policy LU 7.1 p. 70	Page 70. Not sure why the original of LU 7.1 was stricken. It seems to make sense.	Public Comment	This deletion was proposed by Scottsdale Airport Staff as this specific policy is implemented via the Airport Vicinity development regulations (August 2012).  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
39	Land Use Map Designations p. 71	71second paragraph, last sentence CRC changed the language from "should take special care to provide" to "should provide" Legal then changed "should" to "can" Result: "Can" is much weaker than the original "should take special care" and CRC's "should"  Action requested: restore "take special care to." Without "should," the sentence would read: All development, regardless of jurisdiction, can take special care to provide adequate transitions and avoid impacts between uses with different functions and intensities.  This language is consistent with language elsewhere in the Plan. For example: page 51 ("special care is taken"), page 52 ("special care is taken" [twice]), page 71 "special care is taken", page 72 ("special care is needed"). Note: These sentences address different issues; in other words, they are not redundant.	Public Comment	The CRC considered this comment on 11/16/2020 and instead amended the final sentence of the 2nd paragraph to read as"  All development, regardless of jurisdiction, <b>ARE ENCOURAGED TO</b> provide adequate transitions and avoid impacts between uses with different functions and intensities.	11/13/2020
40	Future Land Use Map Designations p. 71	Restore crossed out text so it changes to: "regardless of jurisdiction, must provide adequate transitions..."	COGS	The CRC discussed this on 10/12/2020 and 11/16/2020. This particular text is stating that the city regularly works/cooperates with adjacent municipalities regarding land use decisions and transitions, particularly when such are adjacent to municipal boundaries. Furthermore, the General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
41	Future Land Use Map Designations p. 71	All development, regardless of jurisdiction, "MUST PROVIDE ADEQUATE TRANSITIONS."	Public Comment	The CRC discussed this on 10/12/2020 and 11/16/2020. This particular text is stating that the city regularly works/cooperates with adjacent municipalities regarding land use decisions and transitions, particularly when such are adjacent to municipal boundaries. Furthermore, the General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
42	Future Land Use Map Designations p. 71	Page 71, picky grammar point. The insertion should be "is encouraged to" since "development" is singular.	Public Comment	CRC agreed to amend the text as follows:  All development, regardless of jurisdiction, <b>IS ARE</b> encouraged to provide adequate transitions and avoid impacts between uses with different functions and intensities.	12/4/2020
43	Rural Neighborhoods Land Use Category definition p. 72	<b>WHAT DO SCOTTSDALE RESIDENTS WANT?</b> <b>Protection of Rural Neighborhoods</b> It is critically important to the residents of Rural Neighborhoods to protect the large lot character of their area. Residents bought homes and invested in a Rural Neighborhood based on the surrounding area zoning. Residents trust that their City would only promote and approve changes that would benefit their neighborhood and protect their property values and expectations, and would continue to preserve the character of the area. <b>Problem:</b> Rural Neighborhoods are quickly disappearing in Scottsdale. The 14 year old General Plan now allows all large lot residential and equestrian properties (1 to 5 acre lots) to be up-zoned to the highest gross density allowed in the Rural Neighborhood General Plan Category of 1 dwelling unit/acre. This will cause us to lose all the large lots which are necessary to support equestrian use and larger homes plus desert privacy desired by more affluent homeowners. Due to environmental constraints, this is allowing properties to be subdivided and clustered so actual lots are as small as one-quarter acre-hardly a Rural experience. This practice totally changes the rural Character Area and will eliminate equestrian uses. <b>COGS Solution:</b> Separate the Rural Neighborhoods Land Use category into three separate categories. Any change from lower rural density to a higher rural density would require a Major General Plan Amendment. This requires more Council scrutiny and the minimum of a 5-2 vote for project approval. This change would use the approved General Plan 1996-2000 Rural categories and terminology. <b>The three categories would be:</b> Rural Neighborhood 1/5 dwelling unit per acre (R1-190 zoning) Rural Neighborhood 1/3 to 1/2 dwelling unit per acre (R1-70 & R1-130) Rural Neighborhood 1 dwelling unit per acre (R 1-43)	Coalition of Greater Scottsdale (COGS)	City Council directed staff to not move forward with separating Rural Neighborhoods into separate Land Use Categories.  Rural Neighborhoods definition has been updated to better-define large-lot character and preservation techniques utilized within this Land Use Category.  Updated:  RURAL NEIGHBORHOODS: This category includes large-lot, single-family neighborhoods. Densities in Rural Neighborhoods are usually one house per one or more acres. <b>Within the Environmentally Sensitive Lands area, native desert vegetation is abundant in many Rural Neighborhoods, and special care is needed to preserve open desert character and environmental features. The goal in Rural Neighborhoods is to retain the large-lot character while preserving Clustering is encouraged to preserve</b> desert vegetation, washes, and natural features. <b>Clustering may be used to achieve this goal.</b> Rural Neighborhoods may also include equestrian or golf course <b>use character.</b>	Pre 2020

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Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
44	Urban Neighborhoods definition p. 72	Grammar Correction 72, The design of developments in this category take <del>S</del> care to provide . . .	Public Comment	The CRC considered this comment on 11/16/2020 and incorporated the grammatical change.	11/13/2020
45	Urban Neighborhoods Land Use Category Definition p. 72	72URBAN NEIGHBORHOODS re: addition of "AND BUFFER LESS DENSE RESIDENTIAL FROM HIGHLY TRAVELED ROADWAYS"  Does this mean: a) the design of developments takes care to buffer less dense residential from highly traveled roadways OR b) the design of developments takes care to provide adequate circulation to buffer less dense residential from highly traveled roadways? If "b," replace first "and" with a comma.	Public Comment	The CRC considered this comment on 11/16/2020 and updated as follows:  The design of developments in this category take <del>S</del> care to provide adequate circulation to accommodate traffic volumes, <del>and</del> provide access to transportation choices (e.g. pedestrian, bicycle, transit) and buffer less dense residential from highly traveled roadways.	11/13/2020
46	Residential Land Use Categories p. 72	<b>Suburban Neighborhoods : Add back the terrain restrictions to both. Challenging terrain requires larger lots and/or lower density.</b>	COGS	The General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department. Furthermore, the City's Environmentally Sensitive Lands (ESL) Ordinance is adopted within Appendix B of the City Code of Ordinances and provides regulatory standards regarding all development, depending on grade/slope. Providing prescriptive discussion related to slope would be too specific for the General Plan.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
47	Residential Land Use Categories - Urban Neighborhoods p. 72	Restore the crossed out text and remove the new text so it reads: " <b>The terrain should be relatively flat or gently sloping to accommodate this density.</b> " Take out: "AND BUFFER LESS DENSE RESIDENTIAL FROM HIGHLY TRAVELED ROADWAYS." As this could encourage incompatible uses as the buffer.	COGS	The General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department. Furthermore, the City's Environmentally Sensitive Lands (ESL) Ordinance is adopted within Appendix B of the City Code of Ordinances and provides regulatory standards regarding all development, depending on grade/slope. Providing prescriptive discussion related to slope would be too specific for the General Plan.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
48	Commercial Land Use Category Definition p. 73	73, COMMERCIAL stricken language: "The size and scale of <b>neighborhood</b> business centers should be compatible with surrounding areas."  I appreciate the value of the stricken sentence as someone who lives in a single-family residential neighborhood in very close proximity to a very well-designed commercial center. Please restore the sentence for these reasons: a)It was stated during the 10/12 meeting that the sentences before and after the strike-through cover it. However, the sentence before relates to locating commercial at sufficient intervals so as to reduce travel; the two sentences after relate to the type of streets that commercial use is best located on. No other sentence addresses <b>size and scale</b> . b)The paragraph very succinctly makes important distinctions between <b>neighborhood</b> and <b>community/regional</b> commercial uses. It doesn't make sense to delete one of the distinctions. c)It is very reasonable and very important that the size and scale of <b>neighborhood</b> business centers be compatible with surrounding areas. d)The stricken sentence is analogous to the sentence under OFFICE that CRC voted not to strike: "When next to residential land uses, design of offices should incorporate elements that minimize negative impacts on the neighborhood." Both sentences appropriately protect neighborhoods in close proximity to non-residential land uses.  Since Legal is deleting the word "should" throughout the Plan, the sentence could read: The size and scale of neighborhood business centers are compatible with surrounding areas.	Public Comment	The CRC considered this comment on 11/16/2020 and did not incorporate the requested change.	11/13/2020
49	Non-Residential Land Use Categories - Commercial p. 73	Commercial: Add back the text struck out as it is crucial to maintain the size and scale to be compatible.	COGS	CRC agreed to add the struck text back into the definition and amend the text to read as follows:  The size and scale of neighborhood business centers <del>should</del> <b>ARE ENCOURAGED TO</b> be compatible with surrounding areas.	12/3/2020
50	Non-Residential Land Use Categories - Commercial p. 73	Page 73. I would retain the stricken sentence in the "Commercial" paragraph.	Public Comment	CRC agreed to add the struck text back into the definition and amend the text to read as follows:  The size and scale of neighborhood business centers <del>should</del> <b>ARE ENCOURAGED TO</b> be compatible with surrounding areas.	12/4/2020
51	Non-Residential Land Use Categories - Commercial p. 73	Page 73. Commercial Paragraph. There are two uses of "typically" in this paragraph. Are these goals? If so, "encourage" or "should" should be used instead.	Public Comment	CRC agreed to amend the second and fifth sentences of the definition to read as follows:  Neighborhood-serving commercial uses are <del>TYPICALLY ENCOURAGED TO BE</del> located at frequent intervals in relationship to the density of nearby housing to reduce travel time and distance.  Community- and regional-serving commercial uses are <del>TYPICALLY ENCOURAGED TO BE</del> located on arterial streets for high visibility and traffic volumes.	12/4/2020
52	Non-Residential Land Use Categories - Cultural/Institutional p. 73	Page 73. Should "museums" be included in Cultural/Institutional or Public use?	Public Comment	CRC agreed to amend the definition to read as follows:  CULTURAL/INSTITUTIONAL OR PUBLIC USE: This category includes a variety of public and private facilities including, but not limited to, government buildings, schools, <b>MUSEUMS</b> , private and public utilities, the Scottsdale Airport, and Taliesin West.	12/4/2020

Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
53	Natural Open Space Land Use Category Definition p. 74	<p>74Lands in THE Natural Open Space Category <del>should</del> CAN remain as permanent OR REVEGETATED open space.</p> <p>1.After CRC considered this page, Legal changed "should" to "can." In this case, there is a huge difference between "should" and "can." Stronger language is needed here.</p> <p>2.The addition of "or revegetated" is confusing, especially with the change from "should" to "can." It now reads as if the open space can remain as one thing or another (permanent or revegetated).</p> <p>3.The addition of "or revegetated" is superfluous for two reasons:</p> <p>a)The term "Natural Open Space" already provides for both natural and revegetated open space. Per the Plan's Glossary definition, Natural Open Space includes areas that have maintained their natural character and areas whose character has been restored.</p> <p>b)At the planning and development stage, the distinction between virgin and revegetated Natural Open Space is relevant. But once development is complete, it's all Natural Open Space; the distinction no longer matters. This sentence is about maintaining open space as open space; it doesn't matter whether open space was virgin or revegetated to begin with.</p> <p>Action requested: One of the following or language with similar "strength."</p> <ul style="list-style-type: none"> <li>•It is the community's intent that lands in the Natural Open Space Category remain as permanent open space.<b>OR</b></li> <li>•The community desires that lands in the Natural Open Space Category remain as permanent open space.<b>OR</b></li> <li>•Lands in the Natural Open Space Category are expected to remain as permanent open space.</li> </ul>	Public Comment	<p>The CRC did not have consensus on adding the suggested public comment language. The CRC discussed that the suggested change does not account for areas that may need to be revegetated on a case by case basis. Furthermore, the CRC discussed that the suggested change changes the intent of the original definition.</p> <p>The CRC considered this comment on 11/16/2020 and did not incorporate the requested change.</p>	11/13/2020
54	Open Space Land Use Categories - Developed Open Space p. 74	Page 74. Another odd use of "typically" in the Developed Open Space paragraph. Is this a goal or not?	Public Comment	<p>CRC agreed to amend the definition to read as follows:</p> <p>Their design is <del>TYPICALLY ENCOURAGED TO BE</del> integrated with adjacent neighborhoods.</p>	12/4/2020
55	Open Space Land Use Categories - Natural Open Space p. 74	NAOS is dedicated when development happens and MUST remain as natural open space, it should not appear to be optional as the added text suggests. <b>The ordinance requirements MUST be followed and the General Plan should not allow it to become optional. Remove the added text and change "should" or "can" to "must"</b>	COGS	<p>The General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department. Furthermore, the City's Environmentally Sensitive Lands (ESL) Ordinance is adopted within Appendix B of the City Code of Ordinances and provides regulatory standards regarding all NAOS. Providing prescriptive discussion related to such would be too specific for the General Plan. Furthermore, the General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/3/2020
56	Open Space Land Use Categories - Natural Open Space p. 74	Page 74. Under Natural Area Open Space, I would retain the "should" language as opposed to "are encouraged to."	Public Comment	<p>The General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department. Furthermore, the City's Environmentally Sensitive Lands (ESL) Ordinance is adopted within Appendix B of the City Code of Ordinances and provides regulatory standards regarding all NAOS. Providing prescriptive discussion related to such would be too specific for the General Plan. Furthermore, the General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/4/2020
57	Open Space Land Use Categories - McDowell Sonoran Preserve p. 74	Page 74. The paragraph on the McDowell Sonoran Preserve should reference the limitations in the Charter imposed by Proposition 420. I am not sure why the paragraph says Preserve land may be "privately owned." Isn't Preserve land publicly owned, by definition?	Public Comment	<p>Proposition 420 was a Charter Amendment to prohibit the alteration of the McDowell Sonoran Preserve. Furthermore, the General Plan is not regulatory document therefore any reference to statements that would be considered to be prescriptive have been removed by the City's Legal Department. Consequently, the impacts of Prop. 420 enforced through the City's Preserve Ordinance and the City Charter.</p> <p>There is currently privately-owned land within or adjacent to the preserve that was previously dedicated by zoning action, dedicated easements, or other entitlements. For example, parcel 217-08-336-A is owned by DC Ranch Association Inc, but has been dedicated by zoning action within the Conservation Open Space (COS) zoning category, within the McDowell Sonoran Preserve. Such Preserve lands are outlined in the Scottsdale McDowell Sonoran Preserve Map (p. 102) as "Other Preserved lands, within RSB". This statement covers such instances "privately owned" land in the Preserve.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/4/2020
58	Infill Incentive District p. 75	This district shouldn't even exist as NO area in the city met the Arizona State Blighted criteria. Should be repealed and this removed.	COGS	<p>As stated in the Infill Incentive District definition, the existing Infill Incentive District was established by City Council as they determined it met the State Statute Requirements (ARS 9.499.10) for such. There is no sunset date for the District, and therefore any alteration or disbandment of such would necessitate consideration by City Council.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/3/2020
59	GP Amendment Criteria p. 76	<b>Rural Neighborhoods has to be split into at least 2 categories, 1 to 2 acres and more than 2 acres with it being a Major Amendment to go from the lower density to the higher density.</b> This is necessary to preserve some large lots which attract high income residents and equestrian uses, both of which support the city financially.	COGS	<p>City Council previously directed staff (December 1, 2016) to not move forward with separating Rural Neighborhoods into separate Land Use Categories due to a mixed reaction of support and opposition to such a change by the community. Furthermore, creating new Land use regulations on a property owner could lead to a Prop 207 Private Property Act risk of claim against the City. Requested changes in zoning categories are subject to approval by City Council.</p> <p>However, the Rural Neighborhoods definition has been updated to better-define large-lot character and preservation techniques utilized within this Land Use Category.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/3/2020
60	GP Amendment Criteria p. 76	With our current tax rates, most development loses the city money, and residents are very concerned with the increased heights and density with its resultant impact on traffic congestion, views, etc. In addition it can have a negative impact on tourism. So a good criteria to use is <b>if the change increases the height and/or intensity by more than 25% it should be a Major Amendment.</b>	COGS	<p>The General Plan is not regulatory document - while the Zoning Ordinance is a regulatory document. The Zoning Ordinance is an implementation tool of the General Plan. The Zoning Ordinance regulates height - not the General Plan. Directly relating General Plan amendment criteria to regulatory development standards would be too prescriptive for a General Plan.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/3/2020

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Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
61	GP Amendment Criteria p. 76	Somewhere the real cost of development to the city must be considered and it goes way beyond water and sewer which can be made up with user fees. <b>If it costs the city more than any projected income, the change should be a Major Amendment. That cost has to include maintenance not just initial cost.</b>	COGS	The draft Water/Wastewater Infrastructure Criteria (criteria brought forward from the 2001 General Plan) is in place to ensure that development proposals deemed to result in the premature increase in the size of a master planned water transmission or sewer collection facility are either paid for or constructed by an applicant.  Furthermore, page 206 of the Cost of Development Element outlines several City ordinances that require developer participation in public infrastructure improvement, as prescribed per State law.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
62	GP Amendment Criteria p. 76	Page 76. General Plan Amendment Criteria. Significant variances to zoning requirements can disrupt the carefully thought out categories of the General Plan. Therefore, I would consider incorporating certain significant zoning changes as a Major amendment. "Major" amendments are those that result in a "substantial alteration of the municipality's land use mixture or balance." A substantial variance in zoning regulations could result in such an alteration.	Public Comment	The General Plan is not regulatory document - while the Zoning Ordinance is a regulatory document. Directly relating General Plan amendment criteria to regulatory development standards would be too prescriptive for a General Plan.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
63	Character Area Criteria p. 77	General Plan Amendment Criteria, Character Area Criteria 3, Change the first sentence to read: "A change that does not clearly meet all the goals, policies, and strategies embodied within an approved Character Area Plan or that changes an existing Character Area plan, boundary or implementation."	Coalition of Greater Scottsdale (COGS)	Updated:  Character Area Criteria A change in General Plan Land Use Category that does not clearly meet the goals, <del>and</del> policies, and strategies embodied within an <b>approved adopted</b> Character Area Plan <b>or that changes an existing character area or boundary.</b>	Pre 2020
64	Change to Amendment Criteria / Land Use Definitions p. 77	General Plan Amendment Criteria, Change to Amendment Criteria 5, Change the title to read: "A Change to the Amendment Criteria and/or Land Use Category Definitions Criteria"	Coalition of Greater Scottsdale (COGS)	Updated:  5) Change to <b>the</b> Amendment Criteria and/or Land Use Category Definitions Criteria	Pre 2020
65	GP Amendment Criteria p. 77	5)Change to the Amendment Criteria and/or Land Use Category DefinitionsCriteria (NEW)  Action requested: revise language as follows A modification to the General Plan Amendment Criteria Section of the General Plan Land Use Element ( <b>pages 76-79</b> ) and/or a text change to the use, density, or intensity of the General Plan Land Use Category definitions ( <b>pages 72-75</b> ).  Reason: To make it clear what is being referred to, particularly for the definitions which, unlike the amendment criteria section, don't have a "definitions" heading.	Public Comment	CRC agreed to amend Criteria # 5 to read as follows:  A modification to the General Plan Amendment Criteria Section of the General Plan Land Use Element ( <b>PAGES 76-79</b> ) and/or a text change to the use, density, or intensity of the General Plan Land Use Category definitions ( <b>PAGES 72-75</b> ).	12/4/2020
66	GP Amendment Criteria p. 77	7)General Plan Land Use Overlay Criteria (NEW)  Action requested: replace current language with The modification or expansion of an existing General Plan Land Use Overlay Category or the creation of a new General Plan Land Use Overlay Category.  Reasons: •In the current language, there is a conflict between "creation of new" and "specifically regarding" the four overlays that already exist. The argument could be made that the criterion as written addresses only those four overlays. That was not the intent. •Because of the "specifically regarding" clause, if a new overlay was approved during the Plan's lifespan, the criterion would not cover any attempt to modify or expand it. •The proposed language clearly states the intent.	Public Comment	CRC agreed to amend Criteria # 7 to read as follows:  The modification <del>of</del> <b>OR</b> expansion of <del>and/or creation of a new</del> <b>AN EXISTING</b> General Plan Land Use Overlay Category, (specifically regarding the Regional Use Overlay, Infill Incentive District Overlay, Shea Corridor Overlay, and Mayo Support District Overlay) <b>OR THE CREATION OF A NEW GENERAL PLAN LAND USE OVERLAY CATEGORY.</b>	12/4/2020
67	Exceptions to GP Amendment Criteria, bullet 4 p. 78	General Plan Amendment Criteria, Exceptions Criteria 8, in the 4th bullet, Add:  "Proposed uses within the Shea Corridor Overlay and/or Mayo Support District Overlay areas on the General Plan Future Land Use Map that clearly meet the goals"	Coalition of Greater Scottsdale (COGS)	Updated:  Proposed uses within the Shea Corridor Overlay and/or Mayo Support District Overlay areas on the General Plan Future Land Use Map that <b>clearly</b> meet the goals, policies, and guidelines of the East Shea Area Plan/Shea Area Plan (1987/1993) are determined as already suitable uses within those areas and will be determined as minor amendments.	Pre 2020
68	Exceptions to GP Amendment Criteria, NEW bullet 5 p. 78	General Plan Amendment Criteria, Exceptions Criteria 8:  2001 General Plan included language related to allowing a project applicant to appeal the designation of a General Plan major amendment to the "Chief Planning Officer".	City Staff	Language added back in as bullet 5 under Criteria 8:  <b>If a project applicant wishes to appeal the designation of General Plan major amendment, the Planning and Development Director, or the position equivalent, will evaluate the appeal and make a major amendment determination.</b>	Pre 2020
69	Exceptions to GP Amendment Criteria, Area of Change, sub-bullet 2 p. 78	General Plan Amendment Criteria, Exceptions Criteria 8, 6th bullet (Exceptions to Criteria #2), 2nd sub-bullet:  Change "dwellings" to "dwelling units"	Coalition of Greater Scottsdale (COGS)	Updated:  A change greater than ten (10) gross acres in General Plan Land Use Category will be processed as a minor amendment if it does not increase the planned number of dwellings <b>units</b> and includes at least 30% more Natural Area Open Space (NAOS) than required by the Environmentally Sensitive Lands Ordinance (ESLO), and if the open space:	Pre 2020

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Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
70	GP Amendment Criteria p. 78	<p>bottom bullet</p> <p>Current language (with redline incorporated and new redline added)                      A change greater than ten (10) gross acres in General Plan Land Use Category to Cultural/ Institutional or Public Use with a municipal or non-profit cultural facility that is not adjacent to a Rural or Suburban Neighborhoods General Plan Land Use Category, <b>or</b> <del>#</del> does not share direct access to any street having single-family residential driveway access within one-half (1/2) mile of the proposal will be processed as a minor amendment.</p> <p>The highlighted "or" means:                      •If the subject land <b>is</b> adjacent to a Rural or Suburban Neighborhoods General Plan Land Use Category but <b>does</b> not share direct access to any street having single-family residential driveway access within one-half (1/2) mile, the change would be processed as a minor amendment.                      •If the subject land is <b>not</b> adjacent to a Rural or Suburban Neighborhoods General Plan Land Use Category but <b>does</b> share direct access to any street having single-family residential driveway access within one-half (1/2) mile, the change would be processed as a minor amendment.</p> <p>In other words, as currently worded, a proposal would qualify for the less rigorous minor amendment process if it met only one of the conditions. The language should be clear that <b>both</b> conditions are required to qualify for the less rigorous minor amendment process.</p> <p>Action requested: revise language as follows                      A change greater than ten (10) gross acres in General Plan Land Use Category to Cultural/ Institutional or Public Use with a municipal or non-profit cultural facility will be processed as a minor amendment if the subject land is not adjacent to a Rural or Suburban Neighborhoods General Plan Land Use Category and does not share direct access to any street having single-family residential driveway access within one-half (1/2) mile of the proposal.</p> <p>Note: I know staff moved "will be processed as a minor amendment" to the end of the sentence to be parallel with the other bullets on this page. However, because this bullet addresses negative conditions (is not adjacent, does not share), the sentence makes much more sense when "will be processed as a minor amendment" is placed earlier in the sentence.</p>	Public Comment	<p>The final bullet, as referenced, is directly carried over from the existing 2001 General Plan, where it also utilized the word "or" and not the word "and" as suggested by the comment. Recommend retaining the wording as presented on page 78 of the draft General Plan, where both conditions are NOT required to qualify for the minor amendment process.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/4/2020
71	Airport Influence Area Map p. 81	<p>81Airport Influence Area</p> <p>Compared to the Draft 2035 map, this version shows more context, but now the detail area is very small. Recommendation: a) switch the two maps or make them both roughly equal size, b) include the entire influence area on the detail map, and c) leave the noise contours off the context map so the underlying influence areas are visible on one of the two maps.</p>	Public Comment	<p>The Airport Influence Map - similar to all maps within the draft plan - was digitized in high-resolution, and therefore detail is not lost.</p> <p>The CRC considered this comment on 11/16/2020 and did not incorporate the requested change.</p>	11/13/2020
72	Airport Influence Map p. 81	<p>Make airport map an entire page of its own with all the referenced areas clearly shown with the underlying streets for reference. The "influence areas" are not at all clearly shown.</p>	COGS	<p>The CRC considered this map on 11/16/2020. The map was created via ArcGIS and is exported in high-resolution - retaining the detail necessary to clearly show what areas surrounding the airport are affected by the noise contours and influence areas. Furthermore, the purpose of this map is to meet state statutes, in terms of detailing airports within municipal boundaries. Finally, further detail specific to the Airport noise contours and influence areas is found in the Scottsdale Airport Part 150 Noise Compatibility Study and the Scottsdale Airport Master Plan - both of which are reference on this specific Map and in the Appendix.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/3/2020

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73	Airport Infuence Map p. 81	Page 81. What is the purpose of the "airport influence areas?" I don't see that this concept is explained or referenced in the Plan.	Public Comment	Page 70 of the draft 2035 General Plan contains policy LU 7.1, which specifically discusses the Airport Influence Map and provides guidance to maintain and follow the Airport Part 150 Noise Compatibility Program - an Airport-specific document.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
<b>Safety Element</b>					
74	General	Getting the optimal balance of effort between the numerous aspirations is not addressed. In addition do we know that the right provider of that service is always utilized.  I suggest an analysis be done as to what activities of the police can be done by other areas of Scottsdale government. For instance, police could be substituted by social services on calls for the mentally ill.  Are we spending our money in the most effective and efficient way and improving results?	Public Comment	This public comment was presented at May 15 meeting. No conversation from CRC. Subsequent conversation by CRC will result in Scottsdale Police consideraiton items on the public comment submitted at the September 8th meeting for the CRC.	6/8/2020
75	General p. 159	Page 159. Under the Safety Element, should there be more discussion of pandemic issues and communications? There is a single reference to "pandemic" but not much beyond that. Possibly this belongs under Health.	Public Comment	The Safety Element, as drafted, includes Policy S 1.2 that discusses pandemics. However, Goals S1, S2, S3, and S4 broadly cover such in terms of hazard prevention, disaster preparedness, emergency response, and recovery.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
76	Goal S 2 p. 160	Suggest including discussion regarding Citywide WiFi infrastructure that is robust, resilient, and solar or battery driven, in case of an emergency where we have power outages that affect large swaths of the city.	Scottsdale Environmental Advisory Commission	No change to document. Policy S 2.2, as written, discusses enhancing communication capabilities - which would include city WIFI infrastructure.  The CRC considered this comment on 9/8/2020 and chose not to include this new language in the document.	8/24/2020
77	Policy S 7.10 p. 163	S 7.10  current policy language: Collaborate with schools, neighborhood associations, community centers, religious and multicultural organizations to foster continuous dialogue between law enforcement and resident safety needs and concerns.  This policy would be more effective with clarification. As it reads now, the dialogue would occur between "law enforcement" and "resident safety needs and concerns" – which doesn't make sense.  Here are some alternatives:  Foster continuous dialogue between law enforcement and the community through collaboration with schools, neighborhood associations, community centers, and religious and multicultural organizations.  Collaborate with schools, neighborhood associations, community centers, and religious and multicultural organizations to foster continuous dialogue between law enforcement and residents.  Collaborate with schools, neighborhood associations, community centers, and religious and multicultural organizations to foster continuous dialogue between law enforcement and residents about safety needs and concerns.	Public Comment	CRC agreed to amend the policy to read as follows:  S 7.10 - Collaborate with schools, neighborhood associations, community centers, <b>AND</b> religious and multicultural organizations to foster continuous dialogue between law enforcement and residents <b>S ABOUT</b> safety needs and concerns.	12/10/2020
78	Policy S 8.6 p. 164	S 8.6  Action requested: replace existing language with this Expand programs for wastes that require special management to protect public health, safety and the environment such as household hazardous waste, electronics and pharmaceuticals.  Reasons: •Add pharmaceuticals to support public and private drug take-back programs, including Scottsdale Police and Fire programs. •The requested language is more inclusive than "collection" and "disposal." There are many end-of-life solutions beyond collection and disposal. A few examples: latex paint recycling, used electronics donation, manufacturer and industry take-back programs (e.g., electronics, batteries, fluorescent lamps, carpet, mattresses).	Public Comment	CRC agreed to amend the policy to read as follows:  S 8.6 - <b>EXPAND PROGRAMS FOR WASTES THAT REQUIRE SPECIAL MANAGEMENT TO PROTECT PUBLIC HEALTH, SAFETY AND THE ENVIRONMENT SUCH AS</b> household hazardous waste, electronics <b>AND PHARMACEUTICALS</b> .	12/4/2020
<b>Connectivity Chapter</b>					
79	Chapter Introduction p. 166	Chapter introduction should acknowledge the new Bicycling element.	Public Comment	Introduction currently discusses "mobility choices" which would be inclusive of Bicycling. Furthermore, the Introduction ends by stating the Elements in the chapter. Finally, the Bicycling Element is not new, it was within the 2001 General Plan, and was further highlighted as such as a result of the 2019 effort to update the General Plan to existing community conditions and furthermore meet state statute requirements.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/10/2020

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<b>Circulation Element</b>					
80	Element Introduction p. 169 Policy C 1.2 p. 170	General / Introduction to Circulation Element and Policy C 1.1 (Now C 1.2):  Incorporate transportation into other areas of the plan that address sustainability. Potentially: P. 1 Community Values In the year 2025, Scottsdale will be a community that: Demonstrates its commitment to environmental, economic, and social sustainability and measures both the short and long-term impacts of our decisions; P. 6 Seek Sustainability: Scottsdale is committed to the effective management of its finite and renewable environmental, economic, social, and technological resources to ensure that they serve future needs. P. 81 The sustainability of the city of Scottsdale's quality of life directly relies on the economic prosperity of the community balanced with the preservation of and respect for the natural environment. The policies of the Economic Vitality Element are designed to support and enhance this sustainability. To assure a high quality of life for Scottsdale residents, economic growth must be fostered.  P. 138 Related Plans and Policies: Scottsdale Sustainability Indicators Report, 2000*  P. 145 Scottsdale Values ... The range, quality, accessibility, availability, functionality, suitability, sustainability, compatibility, and affordability of Scottsdale's public service delivery operations, infrastructure and capital facilities.	Transportation Commission	These are references to the 2001 General Plan. Staff worked with the Transportation Commission to incorporate sustainability into the Introduction to the Circulation Element and Policy C 1.1 (Now C 1.2). These changes are consistent with the Transportation Master Plan adopted in July 2016.  Updated Introduction: Scottsdale's transportation system is the backbone of the city, supporting the economy and serving and influencing land use patterns. The automobile historically has been, <del>and will continue to be</del> , the predominant mode of transportation in Scottsdale. <del>While the automobile will remain an important means of travel in Scottsdale, the community must make land use decisions that strive to reduce the length and number of automobile trips. However, to match the character, needs, and lifestyle of different areas, the city will need to diversify its transportation choices. A variety of mobility choices will provide greater accessibility and connectivity, alleviate pollution and congestion, and foster community well-being and quality of life.</del> This shift will depend on Scottsdale's ability to continuously investigate, plan, and incorporate new and traditional technologies – including high capacity transit modes – into our public transit system to assure adequate access to and within our community for residents, employees, visitors, and businesses. To correspond to the character, needs, and lifestyle of different areas within Scottsdale, the City will need to diversify its transportation choices. Land use and transportation policies should emphasize a mix of uses and activities served by more efficient and accessible transportation options. These policies will reduce traffic congestion and impacts on the built and natural environments. Together land use planning, application of sustainable technology, and employer participation in trip reduction programs; can transform reliance on the automobile and reduce congestion on streets. A variety of mobility choices will provide greater accessibility and connectivity and foster community well-being and quality of life.  Updated C 1.2 - Coordinate transportation and land use planning to <del>enhance an integrated, sustainable provide a continuous and integrated</del> mobility system that promotes livable neighborhoods, economic vitality, safety, efficiency, and mode choice.	Pre 2020
81	Element Introduction p. 170	Introduction  Action requested: delete "and new" OR delete "existing and new" (which would have the effect of including both) OR replace "new" with "future"	Public Comment	Language was added by the CRC on 6-29-20.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/11/2020
82	High Capacity Transit Definition p. 170	Action requested: define "high capacity transit" in the Glossary  Reason: "High capacity transit" is not defined in the Glossary of this Plan; nor is it defined in the Transportation Master Plan 2016. As multiple CRC members expressed in the 6-29-20 discussion, this is a long-range planning document and options should not be excluded. There was strong support within CRC to include "high capacity transit" in all its forms and the term should be defined accordingly in the Glossary.	Public Comment	CRC agreed to add the following new term to the glossary, pending Transportation Commission approval:  <b>HIGH-CAPACITY TRANSIT – ANY TRANSIT TECHNOLOGY THAT OPERATES OR FUNCTIONS TO MOVE A LARGE NUMBER OF PASSENGERS.</b>	12/11/2020
83	NEW Policy C 1.1 p. 170 NEW Policy C 1.6 p.170	Add new policies to address the following:  - Add general number regarding transit ridership (i.e. double). The plan needs to address high-capacity transit, especially rail.  - Would like to see additional language for transit beyond bus service since there is a changing mood in the community regarding transit. Adding this now would be beneficial since construction is unlikely for 20-30 years.  - Would like to see goal that recognizes support for transit, for example, "Support the Scottsdale public transit system to assure adequate access to and within our community for residents, employees, and tourists through the use of existing and future technologies that incorporate traditional and high capacity transit modes."	Transportation Commission	Staff worked with the Transportation Commission to prepare additional draft goal/policies regarding transit that were reviewed by the Transportation Commission in 8/15 and modified as needed based on the Transportation Master Plan approved in 7/16.  Added New Policies:  C 1.1 - Support the Scottsdale public transit system to assure adequate access to and within our community for citizens, employees, visitors, and businesses through the use of existing and future technologies that incorporate traditional and high capacity transit modes, excluding all forms of rail.  C 1.6 - Assess high capacity transit alternative, excluding all forms of rail.	Pre 2020
84	Policy C 1.2 p. 170	C 1.1 (Now C 1.2) -  - The city has done a fantastic job with sustainability in the natural environment, needs to expand in the built environment. Add "sustainable" to C 1.1  - Incorporate sustainability and policy C 1.1 as follows: Coordinate transportation planning to provide an integrated, sustainable mobility system that promotes livable neighborhoods, economic vitality, safety, efficiency and mode choice.  - Was sustainability in the 2001 General Plan? Would like to see it in this plan and have it address ways that land use and transportation could be more sustainable. Great to see the separate bike element and would like to see transit beefed up, possibly in its own element.	Transportation Commission	Updated:  C 1.2 - Coordinate transportation and land use planning to <del>enhance an integrated, sustainable provide a continuous and integrated</del> mobility system that promotes livable neighborhoods, economic vitality, safety, efficiency, and mode choice.	Pre 2020
85	Policy C 1.3 p. 170	C 1.3  Current Reduce conflict points between various transportation choices.  Action requested: clarify the policy with the following revision Reduce conflict points between various modes of travel, for example, where the paths of vehicles and bicycles, pedestrians or equestrians, cross, diverge or merge.  Reasons: •CRC member Iacovo proposed examples to clarify the policy's meaning. In listening to the meeting, it sounded like CRC had agreed to add examples. But they were not added. •"Transportation choices" is confusing in this context. Transportation Master Plan 2008 (TMP 2008) uses the term "modes" when referring to conflict points. •The proposed language is a blend of examples from CRC member Iacovo and TMP 2008.	Public Comment	CRC agreed to amend the policy to read as follows:  C 1.3 - Reduce conflict points between various <b>MODES OF TRAVEL, FOR EXAMPLE, WHERE THE PATHS OF VEHICLES AND BICYCLES, PEDESTRIANS OR EQUESTRIANS, CROSS, DIVERGE OR MERGE.</b>	12/11/2020

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Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
86	Policy C1.5 p. 170	C 1.4 (Now C1.5) - Change C 1.4 to: Incorporate strategies that efficiently move people, improve road capacity utilization, enhance mobility choices and assess high capacity transit alternatives.	Transportation Commission	Staff worked with the Transportation Commission to prepare additional draft goal/policies regarding transit that were reviewed by the Transportation Commission in 8/15. Updated: C 1.5 - <del>Use and manage technologies</del> Incorporate strategies that efficiently move people, <del>increase the carrying</del> improve transportation system capacity <del>of roads</del> , and enhance mobility choices.	Pre 2020
87	Policy C 1.6 p. 170	C 1.6 Alternatives to what? Action requested: delete "alternatives" OR replace "alternatives" with "options."	Public Comment	CRC agreed to amend the policy to read as follows: C 1.6 - Assess <b>ALTERNATIVES TO</b> high capacity <b>REGIONAL</b> transit <del>alternatives</del> .	12/11/2020
88	Policy C 2.2 p. 171	C 2.2 Action requested: delete "as appropriate"  Reasons: For reasons discussed by CRC at various times, the qualifier "as appropriate" has drawbacks, as it does here. Moreover, the qualifier is not necessary here. A policy promoting a variety of choices is not a <b>mandate</b> to integrate <b>every</b> mobility option into <b>every</b> corridor. It's understood that in the implementation of the policy, options that are appropriate to a given corridor would be considered.	Public Comment	CRC agreed to delete the phrase "as appropriate".	12/11/2020
89	Policy C 2.3 p. 171	C 2.3 Action requested: delete "in order to provide travelers with effective choices to improve travel reliability"  Reasons: •This policy is under a goal about reducing automobile trips. "Improv(ing) travel reliability" seems like a different topic. •How would telecommuting, alternative work schedules and transit/bicycling incentives improve <b>travel reliability</b> ?	Public Comment	"Reducing traffic congestion" is stated in the goal, if that goal were to be implemented, travel reliability would be one result of such.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/11/2020
90	Policy C 3.8 p. 172	C 3.8  At the 9/8/20 meeting, CRC reviewed this language brought back by staff: Incorporate dedicated pedestrian zones, or periods of non-motorized pedestrian traffic, into areas of the city that have the greatest intensity of development.  There was discussion about the meaning of "periods of non-motorized pedestrian traffic." An example was provided of roads being closed to vehicular traffic once-a-year for the TPC (Waste Management Phoenix Open). In addition, concern was expressed about use of the word "non-motorized." "Pedestrian-only" was suggested as an alternative. Based on the discussion, it sounded like CRC wished to change "periods of non-motorized pedestrian traffic" to "periods of pedestrian-only traffic." However, when Adam read the language just before the vote, he said: "areas of pedestrian-only traffic." So the current language is: Incorporate dedicated pedestrian zones, or areas of pedestrian-only traffic, into areas of the city that have the greatest intensity of development.  Action requested: replace first "areas" with "periods" Incorporate dedicated pedestrian zones or periods of pedestrian-only traffic into areas of the city that have the greatest intensity of development.  Reason: •This makes sense because there is no difference between "dedicated pedestrian zones" and "areas of pedestrian-only traffic." Both are permanent. With "periods" you are providing for both permanent and transient pedestrian areas.	Public Comment	Policy C 3.8, as written, does not specifically state that pedestrian zones are permanent, it states that they may be incorporated - as necessary. Thus, the policy does not preclude utilizing pedestrian-only zones for temporary events, such as the Waste Management Open.  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020



Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
91	Policy C 3.5 p. 172	<p>C 3.5</p> <p>Original language                      Increase accessibility to transit through non-motorized and other transit connections, such as, neighborhood circulators or Dial-A-Ride type services.</p> <p>Current language with all changes incorporated                      Increase accessibility to transit options through non-motorized and other transit connections, such as, neighborhood circulators; on-demand transportation network companies; improved/enhanced sidewalks, multimodal paths, bicycle lanes; and, non-motorized options for the final mile to reach public transit services.</p> <p>Action requested: revise language as follows                      Increase accessibility to transit <del>options</del> through non-motorized and other transit connections, such as, neighborhood circulators; <b>paratransit services</b>; on-demand transportation network companies; improved/<del>enhanced</del> sidewalks, multimodal paths, <b>and</b> bicycle lanes; and, non-motorized options for the final mile to reach public transit services.</p> <p>Reasons:                      •This policy specifically addresses increasing accessibility to transit through connections, rather than increasing accessibility to options.                      •CRC intended “on-demand transportation network companies” (TNCs) to include Dial-a-Ride (now known as Valley Metro Paratransit). But because Valley Metro is a public transportation agency, not a company, readers of the Plan may not make that connection. Although Paratransit Services employs a variety of means, including TNCs, it should be called out separately. (www.scottsdaleaz.gov/transportation/transit at Paratransit Services)                      •“Improved/enhanced” seems redundant.</p>	Public Comment	<p>On-demand transportation network companies was included by the CRC to be encompassing of paratransit as well as ride-share. Furthermore, the policy utilizes “such as” to note one such possible option.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/11/2020
92	Policy C 4.5 p. 173	<p>C 4.5</p> <p>Original language                      Explore partnerships or privatization to provide more mobility choices and address gaps in the mobility system.</p> <p>Current language with all changes incorporated                      Advance partnerships and privatization to provide additional choices to addressing gaps in the city’s mobility system.</p> <p>Action requested: CRC clarifications                      a)The original policy speaks to providing more choices and addressing gaps. The revised policy speaks to providing more choices solely to address gaps. Is it CRC’s intention to limit the policy to addressing gaps?                      b)CRC added “CITY.” Is this City or city? The difference could be relevant here in a few ways. Delete?</p>	Public Comment	CRC agreed to delete the word "city's" from the policy.	12/11/2020
93	Policy C7.2 p. 175	<p>10/19/20 draft without redline (all changes incorporated including legal):                      (NEW) Work with schools to minimize the impacts of student drop-off and pick-up activity on neighborhoods by providing adequate school parking, drop-off, pickup, and queuing space on school property, providing convenient access to public transit, and encouraging non-motorized transportation to school.</p> <p>proposed alternative with redline (compared to above):                      (NEW) Work with schools to minimize the impacts of <del>student drop-off and pick-up activity</del> <b>SCHOOL TRANSPORTATION</b> on neighborhoods by providing adequate <del>school</del> parking, drop-off, pickup, and queuing space on school property; <b>FACILITATING RIDE-SHARING</b>; providing convenient access to public transit; and encouraging non-motorized transportation to school.</p> <p>proposed alternative without redline (this is how it would read):                      (NEW) Work with schools to minimize the impacts of school transportation on neighborhoods by providing adequate parking, drop-off, pickup, and queuing space on school property; facilitating ride-sharing; providing convenient access to public transit; and encouraging non-motorized transportation to school.</p> <p>reasons:                      a)These strategies extend benefits to neighborhoods beyond minimizing the impacts of drop-off and pick-up activity (e.g., on-site [off-street] parking).                      b)After the 2014 General Plan process, EQAB, through the Mayor’s Office, worked with SUSD on these issues. Facilitating ride-sharing was one of the solutions.                      c)Semi-colons provide greater clarity.                      d)Parking is understood to be school parking in this context.</p>	Public Comment	<p>The CRC considered these comments on 10/19/2020 and incorporated as follows:</p> <p><b>COLLABORATE WITH SCHOOLS TO</b> minimize the impacts of <b>SCHOOL TRANSPORTATION</b> on neighborhoods by <b>FACILITATING RIDE-SHARING</b>, providing convenient access to public transit, and encouraging non-motorized transportation to school.</p>	10/19/2020

Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
94	Policy C 7.2 p. 175	<p>C 7.2</p> <p>Current language (with all changes incorporated) Collaborate with schools to minimize the impacts of school transportation on neighborhoods by facilitating ride-sharing, providing convenient access to public transit, and encouraging non-motorized transportation to school.</p> <p>Action requested: revise as follows Collaborate with schools to minimize the impacts of <del>school transportation student drop-off and pick-up activity</del> on neighborhoods by facilitating ride-sharing, providing convenient access to public transit, and encouraging non-motorized transportation to school.</p> <p>Reason: I originally proposed replacing “student drop-off and pick-up activity” with “school transportation,” which CRC accepted. However, because of other changes CRC made to the policy, it is now better to revert back to “student drop-off and pick-up activity.”</p>	Public Comment	<p>Noted for CRC consideration." School transportation" is inclusive of "student drop-off/pickup", but may also include buses, etc.</p> <p>CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.</p>	12/11/2020
95	Street Classification Map P. 176	<p>Street Classification Map -</p> <p>Cactus is shown as a collector from 124th to 128th. This is a legacy depiction that changed with the acquisition of the Preserve.</p>	Transportation Commission	Updated, staff has made this adjustment on the map.	Pre 2020
96	Street Classification Map P. 176  Bikeways and Crossings Map p. 82	<p>Street Classification &amp; Bikeways and Crossings Maps -</p> <p>In addition to the citywide map, provide separate maps for south of Indian Bend Road, Indian Bend Road to Pinnacle Peak Road, and north of Pinnacle Peak Road. These provide much greater clarity for specific locations.</p>	City Staff	Updated, maps have been added to the Appendix to provide further detail.	Pre 2020
<b>Innovation &amp; Prosperity Chapter</b>					
97	Chapter Introduction p. 222	<p>Suggest updating second paragraph of Chapter Introduction:</p> <p>Tourism <del>and high-paying as well as</del> industries, such as healthcare and bio/life sciences, are integral to Scottsdale's identity and serve as the community's core economic engines. In some respects, the tourism and healthcare markets in Scottsdale overlap, with both influenced by the climate, natural and built environments, and the diversity, quality, and character of the community. <del>Whereas While</del> tourism is more of an <del>elastic cyclical</del> industry, <del>healthcare and other significant industries in Scottsdale balance the</del> it contributes extensively to the economy, provides a <del>stable diverse</del> range of employment <del>base opportunities</del>, and contributes to community well-being <del>through diverse jobs and community services</del>. Preservation and enhancement of Scottsdale's core economic sectors is critical to continuous economic health.</p>	Experience Scottsdale	<p>The CRC considered this comment on 9/8/2020 and updated as follows:</p> <p>Tourism <del>and high-paying as well as</del> industries, such as healthcare and bio/life sciences, are integral to Scottsdale's identity and serve as the community's core economic engines. In some respects, the tourism and healthcare markets in Scottsdale overlap, with both influenced by the climate, natural and built environments, and the diversity, quality, and character of the community. <del>Whereas While</del> tourism is more of an <del>elastic SEASONAL,</del> cyclical industry, <del>healthcare and other significant industries in Scottsdale balance the</del> it contributes extensively to the economy, provides a <del>stable diverse</del> range of employment <del>base opportunities</del>, and contributes to community well-being <del>through diverse jobs and community services</del>. Preservation and enhancement of Scottsdale's core economic sectors is critical to continuous economic health.</p>	7/28/2020

Enclosure 9

Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/Legislative Edits	Comment Source	CRC Action / Notes	Date Submitted
<b>Economic Vitality Element</b>					
98	Introduction p. 225	Put back paragraph on tourism which is critical to Scottsdale fiscal sustainability.	Public Comment	Paragraph was retained, but moved to the NEW Tourism Element Introduction. CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/11/2020
99	General	While tourism is our major economic engine there should be a greater emphasis on attracting multiple clean industries to better grow and balance the source of revenues. We have been significantly overtaken by other communities in this regard.	Public Comment	The public comment is addressed by EV2. The City's Economic Development Department has reviewed the Economic Vitality Element and found the language to sufficient for the General Plan. More specifics would be outlined in the Economic Development Strategic Plan. CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	11/23/2020
100	Element Introduction p. 225	<b>Put back the paragraph on tourism as it is a prime economic driver and must remain so.</b> Tourism and the health industry are the two staples that should be not only maintained but strengthened. Add to that high-tech industries that supply jobs Scottsdale residents can fill. We should be encouraging tourism, health, and high-tech related businesses, specialty retail, and other similar businesses that build on Scottsdale's strengths, and will survive the shift to internet purchasing. Other businesses can cost the city more than any income they bring in, so that needs to be evaluated when trying to attract businesses to the city. The keys are income generated for the city, support of other income generators (tourism and health care related), and/or a majority of high paying jobs. Otherwise they will likely generate a net loss to the city.	COGS	The text has been moved to the introduction text of the Tourism Element. CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
101	Goal EV 1 p. 226	Restore all of the original EV 1 or refer to where it is located.	COGS	The text has been moved to the Tourism Element. CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
102	Policy EV 1.9 p. 227	EV 1.9, Change to: "Preserve <b>and enhance</b> historic and <b>archaeological sites that support tourism.</b> "	Historic Preservation Commission	Updated: EV 1.9 - Preserve <b>and enhance</b> historical and <b>archaeological sites that support tourism</b> <del>tourist destination attractions.</del>	Pre 2020
103	Policy EV 1.1 p. 228	Page 228. Under EV 1.1, I would add "and character" at the end of the second bullet point.	Public Comment	The CRC agreed that Character is utilized to more meaningful intent elsewhere in the draft plan. CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
104	Policy EV 2.4 p. 228	Update policy EV 2.4 to state: Recruit and retain <del>environmentally sensitive</del> companies <del>at the forefront of economic and technological change</del> , with core values that include both <b>innovation and environmental stewardship</b> and encourage partnerships between these companies and research institutions.	Scottsdale Environmental Advisory Commission	Updated: EV 2.4 - Recruit and retain <del>environmentally sensitive</del> companies <del>at the forefront of economic and technological change</del> , with core values that include <b>both innovation and environmental stewardship</b> and encourage partnerships between these companies and research institutions.	8/26/2020
105	Policy EV 2.4 p. 229	This goal is not necessarily good for the city. If done most of the employees will NOT LIVE IN SCOTTSDALE which means the business can cost the city more than any income it may bring in, plus they won't provide jobs for Scottsdale residents.	COGS	No action was requested by the public comment. CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
106	Goal EV 4 p. 230	Update Goal EV 4 to state: Sensitively manage land uses to provide and enhance economic development, fiscal health and job growth, while simultaneously protecting <b>both the environment and</b> the integrity and lifestyle of neighborhoods.	Scottsdale Environmental Advisory Commission	The CRC agreed that this additional text was redundant with other text within the draft document, including Goal LU 5 and Policy CD 1.1. The CRC considered this comment on 9/8/2020 and chose not to include the suggested text.	8/26/2020
107	Goal EV 4 p. 231	<b>This is a good section and should actually be part of the Major Amendment criteria.</b> (see earlier comment)	COGS	When developments propose a change to a sites General Plan land use designation, land use impact modeling is utilized to estimate the socioeconomic, development, and fiscal impacts associated with a change from one General Plan land use designation to another over a 20-year time period (i.e., 2020-2040). From a fiscal standpoint, the model assesses both revenues generated from development (initial construction expenditures, yearly sales tax generation, etc.) as well as City expenditures (public safety, infrastructure maintenance, etc.), as a means to estimate how much value a project adds (or does not add) over time – or Net Present Value (NPV).  This is built into major amendment criteria #1 - protects economic drivers (i.e. change from commercial to residential).  CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/3/2020
108	Policy EV 4.1 p. 230	Update policy EV 4.1 to state: <del>Support Plan for</del> and enhance, where necessary, major street, freeway, and telecommunications access to key employment and regional retail centers.	Scottsdale Environmental Advisory Commission	The CRC agreed that this additional text was redundant with other text within the draft document, including Goal LU 5 and Policy CD 1.1. The CRC considered this comment on 9/8/2020 and chose not to include the suggested text.	8/26/2020
109	Policy EV 4.5 p. 230	Update policy EV 4.5 to state: Ensure neighborhoods <b>and sensitive environmental habitats</b> are adequately protected from major development through design sensitivity, buffering, and traffic management.	Scottsdale Environmental Advisory Commission	The CRC agreed that this additional text was redundant with other text within the draft document, including Goal LU 5 and Policy CD 1.1. The CRC considered this comment on 9/8/2020 and chose not to include the suggested text.	8/26/2020
110	Policy EV 4.7 p. 230	Update policy EV 4.7 to state: Identify and promote opportunities for infill development, and ensure that infill development projects sensitively integrate into the <b>environmental and</b> neighborhood setting	Scottsdale Environmental Advisory Commission	Updated: EV 4.7 - Identify and promote opportunities for infill development, and ensure that infill development projects sensitively integrate into the <b>environmental and neighborhood setting</b>	8/26/2020
111	Policy EV 4.7 p. 231	Page 231. I would keep the stricken language in EV 4.7.	Public Comment	The amended text was stricken so as to be broader and not limited. All Land Use decisions are ultimately subject to City Council approval. CRC considered the comment on 12/14/2020 and did not recommend making any changes to the draft plan.	12/4/2020
112	Policy EV 4.8 p. 230	Update policy EV 4.8 to state: Promote orderly, planned growth to reduce service costs, maximize use of existing and proposed public facilities, <b>ensure adequate infrastructure resources</b> , and enhance available revenues	Scottsdale Environmental Advisory Commission	The CRC agreed that this additional text was redundant with other text within the draft document, including Goal LU 5 and Policy CD 1.1. The CRC considered this comment on 9/8/2020 and chose not to include the suggested text.	8/26/2020

Enclosure 9

Comment Number	Applicable Goal/Policy/Page Number	Citizen Outreach Comment/Suggestion/ <b>Legislative Edits</b>	Comment Source	CRC Action / Notes	Date Submitted
113	Policy EV 4.9 p. 230	Update policy EV 4.9: Suggest deleting "when appropriate for the city's fiscal health". The end of the policy already states to expand uses that "contribute to the socio-economic prosperity of our residents", so the statement above is unnecessary.	Public Comment	The CRC did not have consensus on adding the suggested language, with no further discussion regarding such. The CRC considered this comment on 9/8/2020 and chose not to include the suggested text.	7/14/2020
114	Policy EV 5.6 p. 231	Update policy EV 5.6 to state: <del>Explore new options</del> Promote efficiencies gained from comprehensive energy management to supplement the city's existing resources devoted to the operation and maintenance of infrastructure and service delivery.	Scottsdale Environmental Advisory Commission	Updated: EV 5.6 - <del>Explore new options</del> Promote efficiencies gained from comprehensive energy management to supplement the city's existing resources devoted to the operation and maintenance of infrastructure and service delivery.	8/26/2020
115	Policy EV 5.7 p. 231	Update policy EV 5.7 to be more broad and encompassing: Carefully consider the fiscal implications of land use decisions <del>that result in service expansions to avoid significant negative fiscal impacts, unless necessary to achieve other critical community objectives.</del>	Public Comment	Updated EV 5.7 - Carefully consider the fiscal implications of land use decisions <del>that result in service expansions to avoid significant negative fiscal impacts, unless necessary to achieve other critical community objectives.</del>	7/14/2020



## COMMISSION ACTION REPORT

Five-Year Airport Capital Improvement Program for  
Fiscal Years 21/22 through 25/26

**Agenda Item No.:** 3

**Meeting Date:** 02/17/21

**Staff Contact:** Carmen Williams,  
Finance & Administration Manager

**Phone:** (480) 312-8475

### ACTION

Discussion and possible action to approve the Five-Year Airport Capital Improvement Program for Fiscal Years 21/22 through FY25/26.

### PURPOSE

The Aviation Department's Five-Year Capital Improvement Program provides a basis for controlling expenditures for the acquisition of significant aviation assets and construction of capital items at Scottsdale Airport. A presentation of the proposed program to the Airport Advisory Commission is part of the annual budget approval process. Following this action, the program will be incorporated into the overall City budget process before final adoption by the City Council. The program is developed and updated annually.

### KEY CONSIDERATIONS

- All recommended aviation capital improvement projects would be funded by the Aviation Enterprise Fund, federal and/or state grants which will result in no impact to the General Fund.
- If grant funding is not realized for a project it will be pulled for future consideration or pushed to the following year.
- The proposed adoption date by the City Council is June 2021 for implementation on July 1, 2021.

Attachments: 1. Proposed Five-Year Airport Capital Improvement Program FY21/22 through 25/26

Action taken:



# Proposed Five-Year Capital Improvement Program

## FY 2021/22 – 2025/26 Sorted by Division

Application: Capital Planning; For C&ED – Airport

Req Type	Project #	Project Name	Funding Source	FY2020/21 *Approved	ITD Exp @12/2020	FY2020/21 Remaining Budget	FY2021/22 Requested	Total FY2021/22 Requested	Net FY2021/22 Requested	FY2022/23 Forecast	FY2023/24 Forecast	FY2024/25 Forecast	FY2025/26 Forecast	Future	Total Remaining and Forecasted Budget	Total Budget
<b>C&amp;ED - Airport</b>																
Airport Pavement Preservation Program																
E	AB52		642 - Aviation Funds	620,568.0	(399,703.5)	220,864.5	-	620,568.0	220,864.5	-	100,000.0	100,000.0	-	-	420,864.5	820,568.0
			AVF - Aviation Funds	569,432.0	(569,431.5)	0.5	-	569,432.0	0.5	-	-	-	-	-	0.5	569,432.0
A	AB52 (1)	Closing out	642 - Aviation Funds	-	-	-	-	-	-	-	-100,000.0	-100,000.0	-	-	-200,000.0	-200,000.0
			<b>SubTotals for AB52</b>	<b>1,190,000.0</b>	<b>(969,135.0)</b>	<b>220,865.0</b>	<b>-</b>	<b>1,190,000.0</b>	<b>220,865.0</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>220,865.0</b>	<b>1,190,000.0</b>
Design/Construct North General Aviation Box																
E	AF01		642 - Aviation Funds	9,547,000.0	(4,160,729.2)	5,386,270.8	-	9,547,000.0	5,386,270.8	-	-	-	-	-	5,386,270.8	9,547,000.0
Kilo Ramp Rehabilitation																
E	AG02		642 - Aviation Funds	8,046.0	-	8,046.0	87,357.0	95,403.0	95,403.0	-	-	-	-	-	95,403.0	95,403.0
			644 - Grant	163,908.0	-	163,908.0	1,779,591.0	1,943,499.0	1,943,499.0	-	-	-	-	-	1,943,499.0	1,943,499.0
			645 - Grant	8,046.0	-	8,046.0	87,357.0	95,403.0	95,403.0	-	-	-	-	-	95,403.0	95,403.0
A	AG02 (1)	Increase Costs	642 - Aviation Funds	-	-	-	43,656.0	43,656.0	43,656.0	-	-	-	-	-	43,656.0	43,656.0
			644 - Grant	-	-	-	889,333.0	889,333.0	889,333.0	-	-	-	-	-	889,333.0	889,333.0
			645 - Grant	-	-	-	43,656.0	43,656.0	43,656.0	-	-	-	-	-	43,656.0	43,656.0
			<b>SubTotals for AG02</b>	<b>180,000.0</b>	<b>-</b>	<b>180,000.0</b>	<b>2,930,950.0</b>	<b>3,110,950.0</b>	<b>3,110,950.0</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>3,110,950.0</b>	<b>3,110,950.0</b>
Delta Apron Improvements Phase II																
E	AG03		642 - Aviation Funds	41,596.0	(63,075.8)	(21,479.8)	-	41,596.0	-21,479.8	-	-	-	-	-	-21,479.8	41,596.0
			644 - Grant	847,378.0	(107,464.6)	739,913.4	-	847,378.0	739,913.4	-	-	-	-	-	739,913.4	847,378.0
			645 - Grant	41,596.0	-	41,596.0	-	41,596.0	41,596.0	-	-	-	-	-	41,596.0	41,596.0
			<b>SubTotals for AG03</b>	<b>930,570.0</b>	<b>(170,540.4)</b>	<b>760,029.6</b>	<b>-</b>	<b>930,570.0</b>	<b>760,029.6</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>760,029.6</b>	<b>930,570.0</b>
Runway 03/21 Rehabilitation/Reconstruction																
E	AG04		642 - Aviation Funds	518,520.0	(381,835.8)	136,684.2	-	518,520.0	136,684.2	-	-	-	-	-	136,684.2	518,520.0
			644 - Grant	10,562,960.0	-	10,562,960.0	-	10,562,960.0	10,562,960.0	-	-	-	-	-	10,562,960.0	10,562,960.0
			645 - Grant	518,520.0	-	518,520.0	-	518,520.0	518,520.0	-	-	-	-	-	518,520.0	518,520.0
			<b>SubTotals for AG04</b>	<b>11,600,000.0</b>	<b>(381,835.8)</b>	<b>11,218,164.2</b>	<b>-</b>	<b>11,600,000.0</b>	<b>11,218,164.2</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>11,218,164.2</b>	<b>11,600,000.0</b>
<del>Perimeter Service Road Rehabilitation</del>																
E	AG05		642 - Aviation Funds	74,311.0	-	74,311.0	-	74,311.0	74,311.0	-	-	-	-	-	74,311.0	74,311.0
			645 - Grant	668,799.0	-	668,799.0	-	668,799.0	668,799.0	-	-	-	-	-	668,799.0	668,799.0
			<del>SubTotals for AG05</del>	<del>743,110.0</del>	<del>-</del>	<del>743,110.0</del>	<del>-</del>	<del>743,110.0</del>	<del>743,110.0</del>	<del>-</del>	<del>-</del>	<del>-</del>	<del>-</del>	<del>-</del>	<del>743,110.0</del>	<del>743,110.0</del>
Airport Security Fencing Improvements																
E	AG06		642 - Aviation Funds	33,540.0	-	33,540.0	-	<del>33,540.0</del>	<del>33,540.0</del>	-	-	-	-	-	33,540.0	33,540.0
			645 - Grant	301,860.0	-	301,860.0	-	<del>301,860.0</del>	<del>301,860.0</del>	-	-	-	-	-	301,860.0	301,860.0
			<b>SubTotals for AG06</b>	<b>335,400.0</b>	<b>-</b>	<b>335,400.0</b>	<b>-</b>	<del>335,400.0</del>	<del>335,400.0</del>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>335,400.0</b>	<b>335,400.0</b>
<del>Ceiling Improvements at Airport OPS Building</del>																
E	AG07		642 - Aviation Funds	40,000.0	(8,423.7)	31,576.3	-	40,000.0	31,576.3	-	-	-	-	-	31,576.3	40,000.0
Environmental Assessment for Land Acquisition																

\* Approved = Budget inclusive of changes subsequent to Adoption.

]Project was partially or wholly funded with reallocated bond proceeds from MPC Series 2013 per Resolution 10335 and MPC2016-01.



# Proposed Five-Year Capital Improvement Program

## FY 2021/22 – 2025/26 Sorted by Division

Application: Capital Planning; For C&E – Airport

Req Type	Project #	Project Name	Funding Source	FY2020/21 *Approved	ITD Exp @12/2020	FY2020/21 Remaining Budget	FY2021/22 Requested	Total FY2021/22 Requested	FY2022/23 Forecast	FY2023/24 Forecast	FY2024/25 Forecast	FY2025/26 Forecast	Future	Total Remaining and Forecasted Budget	Total Budget
E	AH03		642 - Aviation Funds	8,940.0	-	8,940.0	-	8,940.0	8,940.0	-	-	-	-	8,940.0	8,940.0
			644 - Grant	182,120.0	-	182,120.0	-	182,120.0	182,120.0	-	-	-	-	182,120.0	182,120.0
			645 - Grant	8,940.0	-	8,940.0	-	8,940.0	-	-	-	-	-	8,940.0	8,940.0
			<b>SubTotals for AH03</b>	<b>200,000.0</b>	-	<b>200,000.0</b>	-	<b>200,000.0</b>	-	-	-	-	-	<b>200,000.0</b>	<b>200,000.0</b>
			Land Acquisition 4.0 Acres												
E	TEMP1882-F		642 - Aviation Funds	-	-	-	-	-	1.0	-	-	-	-	1.0	1.0
A	TEMP1882-F	Pushing Out	642 - Aviation Funds	-	-	-	-	-	-1.0	-	1.0	-	-	-	-
			<b>SubTotals for TEMP1882-F</b>	-	-	-	-	-	-	-	<b>1.0</b>	-	-	<b>1.0</b>	<b>1.0</b>
			Exit Taxiways B2, B9 and B15												
E	TEMP2084-F		642 - Aviation Funds	-	-	-	8,940.0	8,940.0	8,940.0	75,804.0	-	-	-	84,744.0	84,744.0
			644 - Grant	-	-	-	182,120.0	182,120.0	182,120.0	1,544,232.0	-	-	-	1,726,352.0	1,726,352.0
			645 - Grant	-	-	-	8,940.0	8,940.0	8,940.0	75,804.0	-	-	-	84,744.0	84,744.0
A	TEMP2084-F	Pushing Out	642 - Aviation Funds	-	-	-	-8,940.0	-8,940.0	-8,940.0	-75,804.0	8,940.0	87,190.0	-	11,386.0	11,386.0
			644 - Grant	-	-	-	-182,120.0	-182,120.0	-182,120.0	-1,544,232.0	182,120.0	1,776,170.0	-	231,938.0	231,938.0
			645 - Grant	-	-	-	-8,940.0	-8,940.0	-8,940.0	-75,804.0	8,940.0	87,190.0	-	11,386.0	11,386.0
			<b>SubTotals for TEMP2084-F</b>	-	-	-	-	-	-	<b>200,000.0</b>	<b>1,950,550.0</b>	-	-	<b>2,150,550.0</b>	<b>2,150,550.0</b>
			Airport Master Plan Update												
E	TEMP2274-F		642 - Aviation Funds	-	-	-	-	-	-	17,880.0	-	-	-	17,880.0	17,880.0
			644 - Grant	-	-	-	-	-	-	364,240.0	-	-	-	364,240.0	364,240.0
			645 - Grant	-	-	-	-	-	-	17,880.0	-	-	-	17,880.0	17,880.0
			<b>SubTotals for TEMP2274-F</b>	-	-	-	-	-	-	<b>400,000.0</b>	-	-	-	<b>400,000.0</b>	<b>400,000.0</b>
			Rehabilitate/Geometry Improvements - Partial												
E	TEMP2374-F		642 - Aviation Funds	-	-	-	-	-	-	-	189,175.0	-	-	189,175.0	189,175.0
			644 - Grant	-	-	-	-	-	-	-	3,853,760.0	-	-	3,853,760.0	3,853,760.0
			645 - Grant	-	-	-	-	-	-	-	189,175.0	-	-	189,175.0	189,175.0
A	TEMP2374-F	Combine	642 - Aviation Funds	-	-	-	23,667.0	23,667.0	23,667.0	213,002.0	-	-	-	47,494.0	47,494.0
			644 - Grant	-	-	-	482,128.0	482,128.0	482,128.0	4,339,149.0	-	-	-	4,632,102.0	4,632,102.0
			645 - Grant	-	-	-	23,667.0	23,667.0	23,667.0	213,002.0	-	-	-	-3,617,091.0	-3,617,091.0
			<b>SubTotals for TEMP2374-F</b>	-	-	-	<b>529,462.0</b>	<b>529,462.0</b>	<b>529,462.0</b>	<b>4,765,153.0</b>	-	-	-	<b>5,294,615.0</b>	<b>5,294,615.0</b>
			Rehabilitate Connector Taxiway A4 - Design and												
A	TEMP2376-F	Version 1 -	642 - Aviation Funds	-	-	-	-	-	-	-	-22,777.0	-	-	-22,777.0	-22,777.0
			644 - Grant	-	-	-	-	-	-	-	-464,006.0	-	-	-464,006.0	-464,006.0
			645 - Grant	-	-	-	-	-	-	-	-22,777.0	-	-	-22,777.0	-22,777.0
			<b>SubTotals for TEMP2376-F</b>	-	-	-	-	-	-	-	<b>-509,560.0</b>	-	-	<b>-509,560.0</b>	<b>-509,560.0</b>
			Rehabilitate Airport Drive												
N	TEMP2497		642 - Aviation Funds	-	-	-	-	-	84,678.0	-	-	-	-	84,678.0	84,678.0
			645 - Grant	-	-	-	-	-	762,102.0	-	-	-	-	762,102.0	762,102.0
			<b>SubTotals for TEMP2497</b>	-	-	-	-	-	<b>846,780.0</b>	-	-	-	-	<b>846,780.0</b>	<b>846,780.0</b>
			Reconstruct Greenway Hangar Taxilane and												

\* Approved = Budget inclusive of changes subsequent to Adoption.

jProject was partially or wholly funded with reallocated bond proceeds from MPC Series 2013 per Resolution 10335 and MPC2016-01.



# Proposed Five-Year Capital Improvement Program

## FY 2021/22 – 2025/26 Sorted by Division

Application: Capital Planning; For C&ED – Airport

Req Type	Project #	Project Name	Funding Source	FY2020/21 *Approved	ITD Exp @12/2020	FY2020/21 Remaining Budget	FY2021/22 Requested	Total FY2021/22 Requested	Net FY2021/22 Requested	FY2022/23 Forecast	FY2023/24 Forecast	FY2024/25 Forecast	FY2025/26 Forecast	Future	Total Remaining and Forecasted Budget	Total Budget
N	TEMP2498		642 - Aviation Funds	-	-	-	-	-	-	4,605.0	41,444.0	-	-	-	46,049.0	46,049.0
			644 - Grant	-	-	-	-	-	-	93,809.0	844,283.0	-	-	-	938,092.0	938,092.0
			645 - Grant	-	-	-	-	-	-	4,605.0	41,444.0	-	-	-	46,049.0	46,049.0
			<b>SubTotals for TEMP2498</b>	-	-	-	-	-	-	<b>103,019.0</b>	<b>927,171.0</b>	-	-	-	<b>1,030,190.0</b>	<b>1,030,190.0</b>
		Airport Drainage Master Plan Update														
N	TEMP2499		642 - Aviation Funds	-	-	-	-	-	-	-	17,880.0	-	-	-	17,880.0	17,880.0
			644 - Grant	-	-	-	-	-	-	-	364,240.0	-	-	-	364,240.0	364,240.0
			645 - Grant	-	-	-	-	-	-	-	17,880.0	-	-	-	17,880.0	17,880.0
			<b>SubTotals for TEMP2499</b>	-	-	-	-	-	-	-	<b>400,000.0</b>	-	-	-	<b>400,000.0</b>	<b>400,000.0</b>
		Reconfigure Aircraft Run-up Hold Apron RY21														
N	TEMP2503		642 - Aviation Funds	-	-	-	-	-	-	-	-	5,637.0	50,732.0	-	56,369.0	56,369.0
			644 - Grant	-	-	-	-	-	-	-	-	114,829.0	1,033,473.0	-	1,148,302.0	1,148,302.0
			645 - Grant	-	-	-	-	-	-	-	-	5,637.0	50,732.0	-	56,369.0	56,369.0
			<b>SubTotals for TEMP2503</b>	-	-	-	-	-	-	-	-	<b>126,103.0</b>	<b>1,134,937.0</b>	-	<b>1,261,040.0</b>	<b>1,261,040.0</b>
		Aviation Annual Pavement Preservation														
N	TEMP2530		642 - Aviation Funds	-	-	-	200,000.0	200,000.0	200,000.0	200,000.0	200,000.0	200,000.0	200,000.0	-	1,000,000.0	1,000,000.0
		Aviation Match Contingency														
E	ZB52		642 - Aviation Funds	-	-	-	250,000.0	250,000.0	250,000.0	250,000.0	250,000.0	250,000.0	-	-	1,000,000.0	1,000,000.0
A	ZB52 (1)	Restore	642 - Aviation Funds	-	-	-	-	-	-	-	-	-	250,000.0	-	250,000.0	250,000.0
			<b>SubTotals for ZB52</b>	-	-	-	<b>250,000.0</b>	<b>250,000.0</b>	<b>250,000.0</b>	<b>250,000.0</b>	<b>250,000.0</b>	<b>250,000.0</b>	<b>250,000.0</b>	-	<b>1,250,000.0</b>	<b>1,250,000.0</b>
		Airport Future Grants Contingency														
E	ZB53		646 - Grant	-	-	-	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	-	-	13,200,000.0	13,200,000.0
A	ZB53 (1)	Restore	646 - Grant	-	-	-	-	-	-	-	-	-	3,300,000.0	-	3,300,000.0	3,300,000.0
			<b>SubTotals for ZB53</b>	-	-	-	<b>3,300,000.0</b>	<b>3,300,000.0</b>	<b>3,300,000.0</b>	<b>3,300,000.0</b>	<b>3,300,000.0</b>	<b>3,300,000.0</b>	<b>3,300,000.0</b>	-	<b>16,500,000.0</b>	<b>16,500,000.0</b>
			<b>SubTotals for C&amp;ED - Airport</b>	<b>24,766,080.0</b>	<b>(5,690,664.1)</b>	<b>19,075,415.9</b>	<b>7,210,412.0</b>	<b>31,976,492.0</b>	<b>26,285,827.9</b>	<b>9,464,952.0</b>	<b>5,677,171.0</b>	<b>5,317,094.0</b>	<b>4,884,937.0</b>	-	<b>51,629,981.9</b>	<b>57,320,646.0</b>
			<b>Expended Prior Years Budget (through 12/2020)</b>	<b>(5,690,664.1)</b>				<b>(5,690,664.1)</b>								<b>(5,690,664.1)</b>
				<b>19,075,415.0</b>				<b>26,285,827.9</b>								<b>51,629,981.0</b>
			642 - Aviation Funds	10,892,521.0	(5,013,768.0)	5,878,753.0	604,680.0	11,497,201.0	6,483,433.0	752,285.0	536,144.0	520,051.0	500,732.0	-	8,792,645.0	13,806,413.0
			644 - Grant	11,756,366.0	(107,464.6)	11,648,901.4	3,151,052.0	14,907,418.0	14,799,953.4	4,432,958.0	1,754,883.0	5,091,578.0	1,033,473.0	-	27,112,845.4	27,220,310.0
			645 - Grant	1,547,761.0	-	1,547,761.0	154,680.0	1,702,441.0	1,702,441.0	979,709.0	86,144.0	-3,594,535.0	50,732.0	-	-775,509.0	-775,509.0
			646 - Grant	-	-	-	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	-	16,500,000.0	16,500,000.0
			AVF - Aviation Funds	569,432.0	(569,431.5)	0.5	-	569,432.0	0.5	-	-	-	-	-	0.5	569,432.0
			<b>Total Capital Budgets:</b>	<b>24,766,080.0</b>	<b>(5,690,664.1)</b>	<b>19,075,415.9</b>	<b>7,210,412.0</b>	<b>31,976,492.0</b>	<b>26,285,827.9</b>	<b>9,464,952.0</b>	<b>5,677,171.0</b>	<b>5,317,094.0</b>	<b>4,884,937.0</b>	-	<b>51,629,981.9</b>	<b>57,320,646.0</b>
			<b>Expended Prior Years Budget (through 12/2020)</b>	<b>(5,690,664.1)</b>				<b>(5,690,664.1)</b>								<b>(5,690,664.1)</b>
				<b>19,075,415.0</b>				<b>26,285,827.9</b>								<b>51,629,981.0</b>

\* Approved = Budget inclusive of changes subsequent to Adoption.

Project was partially or wholly funded with reallocated bond proceeds from MPC Series 2013 per Resolution 10335 and MPC2016-01.





# Proposed Five-Year Capital Improvement Program

## FY 2021/22 – 2025/26 Sorted by Division

Application: Capital Planning; For C&ED – Airport

Total Summary by Funding Source	FY2020/21 *Approved	ITD Exp+Com @12/2020	FY2020/21	FY2021/22 Requested	Total FY2021/22 Requested	Net FY2021/22 Requested	FY2022/23 Forecast	FY2023/24 Forecast	FY2024/25 Forecast	FY2025/26 Forecast	Future	Total	Total
			Remaining Budget									Remaining and Forecasted Budget	
642 - Aviation Funds	10,892,521.	(5,013,768.0)	5,878,753.0	604,680.0	11,497,201.0	6,483,433.0	752,285.0	536,144.0	520,051.0	500,732.0	-	8,792,645.0	13,806,413.
644 - Grant	11,756,366.	(107,464.6)	11,648,901.4	3,151,052.0	14,907,418.0	14,799,953.4	4,432,958.0	1,754,883.0	5,091,578.0	1,033,473.0	-	27,112,845.4	27,220,310.
645 - Grant	1,547,761.0	-	1,547,761.0	154,680.0	1,702,441.0	1,702,441.0	979,709.0	86,144.0	-3,594,535.0	50,732.0	-	-775,509.0	-775,509.0
646 - Grant	-	-	-	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	3,300,000.0	-	16,500,000.0	16,500,000.
AVF - Aviation Funds	569,432.0	(569,431.5)	0.5	-	569,432.0	0.5	-	-	-	-	-	0.5	569,432.0
<b>Grand Total</b>	<b>24,766,080.</b>	<b>(5,690,664.1)</b>	<b>19,075,415.9</b>	<b>7,210,412.0</b>	<b>31,976,492.0</b>	<b>26,285,827.9</b>	<b>9,464,952.0</b>	<b>5,677,171.0</b>	<b>5,317,094.0</b>	<b>4,884,937.0</b>	<b>-</b>	<b>51,629,981.9</b>	<b>57,320,646.</b>

\* Approved = Budget inclusive of changes subsequent to Adoption.

jProject was partially or wholly funded with reallocated bond proceeds from MPC Series 2013 per Resolution 10335 and MPC2016-01.



## COMMISSION ACTION REPORT

Discussion and Possible Action to modify the Airport Rules and Regulations, Article 1. Definitions, Article 2 Section 2.6. Aircraft Parking, Section 2.9. Aircraft shades and tiedowns, Section 2.15. Aviation Business Center and Airpark Rules and Regulations, Section 301. Landing and takeoff of helicopters.

**Agenda Item No.:** 4

**Meeting Date:** 02/17/21

**Staff Contact:** Gary P. Mascaro,  
Aviation Director

**Phone:** (480) 312-7735

### ACTION

Discussion and Possible Action to modify the Airport Rules and Regulations, Article 1. Definitions, Article 2 Section 2.6. Aircraft Parking, Section 2.9. Aircraft shades and tiedowns, Section 2.15. Aviation Business Center and Airpark Rules and Regulations, Section 301. Landing and takeoff of helicopters.

### PURPOSE

As part of the Aviation Department overall review of our policies, procedures and regulatory documents, staff evaluated and are recommending updates to the above referenced sections of our primary guiding documents for the Scottsdale Airport. The goal of this process is to evaluate the documents to help foster the long term economic health, enhance the safe operating environment at the airport/airpark and provide clarity of language.

### KEY CONSIDERATIONS

- All changes are mostly “clean-up” and clarification within the primary guiding documents.
- Some areas have been deleted within the documents as there is no longer a need for certain sections.

Attachment(s):

1. Redlined changes to the primary guiding documents to specific sections.

NOTE: In accordance with Scottsdale Revised Code, Chapter 5-202, copies of the attachment above will available for review during normal business hours (8:00 a.m. – 5:00 p.m., Monday-Friday, except holidays) at the Scottsdale Airport Administration Offices located at 15000 North Airport Drive, Suite 100, Scottsdale, AZ 85260.

Action taken:

## Primary Guiding Documents – Revisions

### **Airport Rules and Regulations**

Based means an aircraft: (1) which the owner physically locates at the Airport the intent and purpose to remain for an undetermined period; (2) which, whenever absent from the Airport, its owner intends to return to the Airport for permanent storage; and (3) whose presence on the Airport is not transitory in nature. Any aircraft that is parked at the Airport in a yellow painted tiedown for more than 21 days in two (2) consecutive calendar months is presumed to be based at the Airport Based includes including an aircraft that are is located at the Airport or a Airpark for a limited or seasonal duration. Aircraft temporarily parked at the Airport to have maintenance performed on it by an authorized Aeronautical Business Permit holder with the intention to leave when the maintenance is complete are not considered based. Once an aircraft is based at the Airport, it shall continue to be considered a based aircraft for six (6) months or as long as the aircraft is parked at the Airport, whichever is longer.

#### **2-6. Aircraft parking.**

- (a) No person shall park, leave parked, or allow to remain stationary any aircraft at the Airport except within an aircraft parking and storage area. Any aircraft based at the Airport shall be registered as such with the Aviation Department.
- (b) No person shall park an aircraft in a City owned aircraft parking and storage area without an Aircraft Storage Permit.
- (c) No person shall park an aircraft at the Gemini Air Group Facility, ~~Air Commerce Center or Greenway Shades/Hangars~~ aircraft parking and storage area without an Aircraft Storage Agreement.
- (d) Any person who parks an aircraft in an aircraft parking and storage area shall remit to the City all applicable transient parking fees until such time as the person applies for and obtains an Aircraft Storage Permit/Agreement.
- (e) A person with an Aircraft Storage Permit/Agreement is prohibited from subleasing an aircraft parking and storage area or basing more than one (1) aircraft at a time in designated a tie down or shade without the prior written approval of the Aviation Director.
- (f) The storage of damaged/dismantled aircraft in aircraft shades, tiedowns or other unscreened areas of the Airport shall not exceed thirty (30) calendar days after written notification from the Aviation Director. The aircraft owner is required to diligently correct such condition as soon as possible to prevent an unsightly Airport appearance.
- (g) If any aircraft is parked in violation of this section or, in the determination of the Aviation Director, presents an operational or safety concern in any area of the Airport, the Aviation Director may cause the aircraft, at the owner's/operator's expense, to be moved by a representative of a Fixed Base Operator. The City shall not be liable for any damages which may result from the relocation of the aircraft.
- (h) The Aviation Director may immobilize an aircraft by installing on the aircraft a propeller lock or by such other suitable means under any of the following circumstances:

1. Failure to apply for and obtain an Aircraft Storage Permit/ Agreement, and until such time as the aircraft owner/operator applies for and obtains the Aircraft Storage Permit/Agreement and remits all fees and charges due the City in accordance with Section 5-116 of the Scottsdale Revised Code. ~~Under these circumstances, the aircraft owner shall be provided a hearing as soon as reasonably practical, but in no event shall said hearing be delayed more than seventy-two (72) hours after the immobilization of the aircraft, except upon mutual agreement of the Aviation Director and the aircraft owner. The hearing and any subsequent appeal shall be conducted generally in accordance with the procedures set forth in Section 5-501 et seq. of the Scottsdale Revised Code.~~
  2. Upon revocation of an Aircraft Storage Permit/Agreement pursuant to Section 5-701 et seq. of the Scottsdale Revised Code, and until such time as the aircraft owner/operator remits all fees and charges due the City in accordance with Section 5-116 of the Scottsdale Revised Code.
  3. If, in the determination of the Aviation Director, the aircraft presents an operational safety concern in any area of the Airport, or otherwise constitutes a danger to the health, safety, or welfare of any individual or the public in general, and until such time as the aircraft no longer presents such health, safety or welfare concerns. ~~Under these circumstances, the aircraft owner shall be provided a hearing as soon as reasonably practical, but in no event shall said hearing be delayed more than seventy-two (72) hours after the immobilization of the aircraft, except upon mutual agreement of the Aviation Director and the aircraft owner. The hearing and any subsequent appeal shall be conducted generally in accordance with the procedures set forth in Section 5-501 et seq. of the Scottsdale Revised Code.~~
- (i) If an aircraft is immobilized by the Aviation Director pursuant to subsection (h), the aircraft owner shall be provided a hearing as soon as reasonably practical, but in no event shall said hearing be delayed more than seventy-two (72) hours after the immobilization of the aircraft, except upon mutual agreement of the Aviation Director and the aircraft owner. The hearing and any subsequent appeal shall be conducted generally in accordance with the procedures set forth in Section 5-501 et seq. of the Scottsdale Revised Code.

**Section 2-9. Aircraft shades and tiedowns.**

- (a) Aircraft shades and tiedowns shall only be used for the following purposes:
1. Storage and parking of the based aircraft listed on the Aircraft Storage Permit/Agreement for that shade or tiedown and parked in a manner so as to be completely contained within the aircraft parking space and not obstruct adjacent aircraft parking and storage areas or taxilanes, except for purposes of immediate and temporary staging and fueling of such aircraft.
  2. Conducting preventive aircraft maintenance on the based aircraft at the based location in accordance with applicable regulations.
  3. Parking of vehicles in accordance with Section 4-2.
  4. Aircraft equipment and supplies contained in a storage device with prior written approval from the Aviation Director.

(b) Use of aircraft shades and tie-downs shall be subject to the following restrictions:

1. Tie-downs painted white are reserved tie-downs and shall not be used without the user first receiving an Aircraft Storage Permit.
2. Tie-downs painted yellow are transient tie-downs for the purposes of short-term or overnight parking. No person shall park an aircraft in a transient tie-down for a period greater than fourteen (14) ~~consecutive calendar days~~ per month, unless identified by a Fixed Base Operator's checks. No person shall park a based aircraft in a transient tie-down. If any aircraft is parked in violation of this section, or, in the determination of the Aviation Director, presents an operational or safety concern in any area of the Airport, the Aviation Director may cause said aircraft, at the owner's/operator's expense and without liability for damage which may result in the course of such moving, to be delivered into the care of a representative of a Fixed Base Operator or specialty service shop authorized to do business on the Airport.

**Section 2-15. ~~(Reserved)~~ Aviation Business Center.**

Commercial activity at the Aviation Business Center ("ABC") is limited to City personnel and the tenants of the ABC. The ABC is not available for use as a passenger boarding lounge or any other commercial activity by persons other than ABC tenants and their invited guests unless a specific permit for that purpose has been issued by the City.

**Airpark Rules and Regulations**

**Section 301. Landing and takeoff of helicopters.**

No helicopter landing or taking-off in the airpark shall obstruct the taxiway safety area at any location where the exhaust or rotor down-wash may cause injury to persons or do damage to property or spread debris on an airpark taxiway safety area, aircraft parking and/or staging area. Hover taxiing of helicopters is prohibited in the Airpark.

course of such moving, to be delivered into the care of a representative of a Fixed Base Operator or specialty service shop authorized to do business on the Airport.

**Section 2-15. Aviation Business Center.**

*Commercial activity at the Aviation Business Center (“ABC”) is limited to City personnel and the tenants of the ABC. The ABC is not available for use as a passenger boarding lounge or any other commercial activity by persons other than ABC tenants and their invited guests unless a specific permit for that purpose has been issued by the City.*

**Airpark Rules and Regulations**

**Section 301. Landing and takeoff of helicopters.**

No helicopter landing or taking-off in the airpark shall obstruct the taxiway safety area at any location where the exhaust or rotor down-wash may cause injury to persons or do damage to property or spread debris on an airpark taxiway safety area, aircraft parking and/or staging area.

*Hover taxiing of helicopters is prohibited in the Airpark.*



## COMMISSION ACTION REPORT

Discussion and input regarding Airport and Airpark Aeronautical Business Permit Additions, Cancellations, and Revocations

**Agenda Item No.:** 5

**Meeting Date:** 02/17/21

**Staff Contact:** Carmen Williams,  
Aviation Finance & Administration  
Manager

**Phone:** (480) 312-8475

### INFORMATION

Review of Airport and Airpark permittees and major tenant Aeronautical Business Permit additions, cancellations, and revocations.

### PURPOSE

Per the request of the Airport Advisory Commission, a report will be provided as needed indicating additions, cancellations, and revocations of Aeronautical Business Permits.

### KEY CONSIDERATIONS

- Attached are the current lists of Airport and Airpark permittees.
- List provides what type of aeronautical activity the business is conducting and the contact information.
- Any additions, cancellations, suspensions, and revocations will be highlighted on the list.

Attachment(s):  
1. Current Airport Permittee List by Category  
2. Current Airpark Permittee List by Category

**AIRPORT AERONAUTICAL BUSINESS PERMITS & TENANTS**

FEBRUARY 2021

<b>AIRCRAFT CHARTER, SALES &amp; MANAGEMENT</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
AMERICAN FLIGHT SUPPORT, LLC	AIRCRAFT CHARTER BROKERAGE	RASC	BEN MOKE	888-245-4017	701-540-0234
ASI CHARTER INC. dba PEAK MEDEVAC INTL	AIRCRAFT CHARTER	RASC	EUGENE HAGGAN	720-649-0600	720-649-0800
AVIATION RESOURCE GROUP dba AERODYNE FLIGHT CENTER	AIRCRAFT SALES	ACC	DOUG COX	480-359-7979	
BUSINESS AIRCRAFT MANAGEMENT dba EXECUTIVE AIRCRAFT SERVICES	AIRCRAFT CHARTER/SALES/MANAGEMENT	SFS	GORDON JOHNSON	480-905-8659	480-905-9365
ELITE FLIGHT JETS	AIRCRAFT CHARTER	SFS	CHARLES LAPMARDO	480-305-0911	
FLY DENALI dba ALASKAZONA ADVENTURES	AIRCRAFT CHARTER	RA	ERIC ROVEY	623-203-7580	
GEMINI AIR GROUP	AIRCRAFT CHARTER/MANAGEMENT	GEM	TIMOTHY CARPAY	480-991-5387	480-991-3373
G.G.R. AVIATION	AIRCRAFT MANAGEMENT	SFS	GUY MILANOVITS	480-614-1166	
GRANDVIEW AVIATION, LLC	AIRCRAFT CHARTER	SFS	DARRELL BONEBRAKE	888-573-9426	
J&S AVIATION	AIRCRAFT MANAGEMENT	MOBILE	SEAN FOWLER	480-241-9437	623-780-8484
JET LINX SCOTTSDALE	AIRCRAFT CHARTER/MANAGEMENT	RASC	JON HULBURD	866-538-5469	888-398-3189
JET FLEET, LLC	AIRCRAFT SALES	SFS	STEVE GAGE	480-286-0029	
JET PROS, LLC	AIRCRAFT CHARTER/BROKERAGE/MANAGEMENT	SFS	MARGARET PIONTEK	480-444-2452	480-575-9920
JOHN HOPKINSON & ASSOCIATES	AIRCRAFT SALES	SFS	CHRISTINA HOPKINSON	403-637-2250	
MAINE AVIATION AIRCRAFT CHARTER, LLC dba MAC JET CENTER	AIRCRAFT CHARTER	GEM	ALYSAN CARUSO	207-780-1811	
ROSS AVIATION - CHARTER	AIRCRAFT CHARTER	RASC	RICK WIELEBSKI	480-948-2400	480-948-3874
PACIFIC AIR CENTER	AIRCRAFT SALES	RASC	BRIAN MACKIN	562-513-5222	
SAWYER CHARTER SERVICE	AIRCRAFT CHARTER/SALES	RANC	CHAD VERDAGLIO	480-922-2723	480-922-5653
SCOTT AIR, LLC dba ISLAND AIR EXPRESS	AIRCRAFT CHARTER	RASC	SCOTT CURRIER	602-274-4370	602-285-9295
SET JET	AIRCRAFT CHARTER/BROKERAGE	GEM	WILLIAM SMITH	480-264-6500	
VERNON AVIATION	AIRCRAFT CHARTER	RASC	CHRIS RILEY	505-564-9464	

<b>AIRCRAFT RENTAL, LEASING &amp; FLIGHT TRAINING</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
AMERICAN FLYERS, INC.	FLIGHT TRAINING	RANC	STEVEN DAUN	954-784-2122	
AVIATION RESOURCE GROUP (AERODYNE)	AIRCRAFT RENTAL/FLIGHT TRAINING	ACC	DOUG COX	480-359-7979	
ELITE FLIGHT TRAINING	AIRCRAFT RENTAL/LEASING/FLIGHT TRAINING	SFS	CHARLES LAPMARDO	480-305-0911	
LEGACY FLIGHT TRAINING	FLIGHT TRAINING	ACC	WILLIAM INGLIS	772-539-0420	
NO DELAY LLC dba CLIMB 250	AIRCRAFT RENTAL/FLIGHT TRAINING	SFS	NATALIE MITCHELL	480-534-3789	
PLUS 5 SPORT AERO	FLIGHT TRAINING	RANC	BUD DAVIDSON	602-971-3991	602-971-3896
RAINTREE AVIATION DBA LEOPARD AVIATION	AIRCRAFT RENTAL/FLIGHT TRAINING	RASC	THOMAS NOON	760-419-2252	
SAWYER AVIATION	AIRCRAFT RENTAL/FLIGHT TRAINING	RANC	CHAD & MARY VERDAGLIO	480-922-5221	480-922-5653
SCOTTSDALE EXECUTIVE FLIGHT TRAINING	AIRCRAFT RENTAL/FLIGHT TRAINING	SFS	GUY MILANOVITS	480-614-1166	
SDL HOLDINGS - ATP	FLIGHT TRAINING	RASC	JIM KOZIARSKI	904-273-3018	904-273-1511
SIERRA CHARLIE AVIATION	AIRCRAFT RENTAL/FLIGHT TRAINING	SFS	SCOTT CAMPBELL	480-390-2346	
SOUTHWEST FLIGHT CENTER	AIRCRAFT RENTAL/FLIGHT TRAINING	ACC	GARY LEWIN	480-991-2880	480-991-2968



UNIVERSAL HELICOPTERS, INC.	FLIGHT TRAINING/LEASING/PHOTOGRAPHY	RASC	GORDON JIROUX	480-951-6283	480-951-6285
VERTICAL WORKS	FLIGHT TRAINING	RANC	CHARLES CHADWICK	732-865-1610	

<b>AIRCRAFT MAINTENANCE &amp; REPAIR</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
ACROPRO LLC	MOBILE AIRCRAFT MAINTENANCE	MOBILE	PIKE KELLY	805-268-4962	
ARIZONA AIRCRAFT INTERIOR DESIGN	SPECIALIZED AIRCRAFT REPAIR	SFS	MICHAEL BRYANT	480-832-1330	480-832-1186
AZ JET SERVICES	AIRCRAFT MAINTENANCE	SFS	DAVE FERNEAU	602-380-5555	
CESSNA AIRCRAFT COMPANY	AIRCRAFT MAINTENANCE	SFS	RANDALL SOUTIERE	480-840-9430	
CONSTANT AVIATION, LLC	AIRCRAFT MAINTENANCE	RA	NATHAN ROMNEY	469-323-4081	
DALLAS AIRMOTIVE	AIRCRAFT MAINTENANCE	SFS	DAVID HUTCHISON	214-477-9033	
DIRECTMX AVIATION LLC	AIRCRAFT MAINTENANCE	SFS	VAN NGUYEN	520-409-7860	
DUNCAN AVIONICS	AIRCRAFT MAINTENANCE	SFS	JIM DAVIS	480-922-3575	480-951-9234
GEMINI AIR GROUP	AIRCRAFT MAINTENANCE	GEM	TIMOTHY CARPAY	480-991-5387	480-991-3373
G.G.R. AVIATION	MOBILE AIRCRAFT MAINTENANCE	SFS	GUY MILANOVITS	480-614-1166	
JET EAST AVIATION	AIRCRAFT MAINTENANCE	SFS	SHAWN GEORGE	216-212-8056	
LEARJET/BOMBARDIER INC.	AIRCRAFT MAINTENANCE	SFS	SEBASTIAN MOORE	520-746-5100	
PDR SERVICES	SPECIALIZED AIRCRAFT REPAIR	SFS	PHILIP CHAPMAN	480-202-2908	
PREMIER AIR CENTER dba WEST STAR AVIATION	AIRCRAFT MAINTENANCE	SFS	RODGER RENAUD	618-258-8020	618-259-0809
SAWYER MX, LLC	AIRCRAFT MAINTENANCE	RA	CHAD VERDAGLIO	480-922-5221	480-922-5653
SOUTHWEST FLIGHT CENTER	MOBILE AIRCRAFT MAINTENANCE	ACC	GARY LEWIN	480-991-2880	480-991-2968

<b>AIRCRAFT WASHING &amp; DETAILING</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
AERO PANACHE	AIRCRAFT WASHING	MOBILE	TODD PUCKETT	602-531-5505	
APPEARANCE GROUP	AIRCRAFT WASHING	MOBILE	DONALD HENRY	480-580-1658	
CLASSIC AIR AVIATION	AIRCRAFT WASHING	MOBILE	JON MARPLE	602-574-5376	440-664-3568
JB'S EXECUTIVE DETAILING	AIRCRAFT WASHING	MOBILE	JEFFREY BURROWS	480-808-4229	
TIME FOR SALE	AIRCRAFT WASHING	MOBILE	CAROLYN NELSON	602-295-7181	
WEST COAST WASH STATION	AIRCRAFT WASHING	MOBILE	MIKE ADAMS	480-443-7320	

<b>AUTO RENTAL COMPANIES</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
ALAMO/NATIONAL CAR RENTAL	OFF-AIRPORT RENTAL CAR	OFF	MIKE ROLLINS	480-948-4884	480-948-7444
AVIS RENT-A-CAR SYSTEMS	OFF-AIRPORT RENTAL CAR	OFF	PETER SERENA	480-948-4993	602-273-3215
ENTERPRISE RENT-A-CAR	RENTAL CAR	SFS	ERIC BULLIS	480-315-8051	480-315-1938
GO RENTALS	RENTAL CAR	RA	KAVOUS GITIBIN	480-991-0117	949-222-1909
HERTZ RENT-A-CAR	OFF-AIRPORT RENTAL CAR	OFF	STEPHEN BLUM	239-301-7794	

<b>FIXED BASE OPERATORS</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
JET AVIATION OF AMERICA	FIXED BASE OPERATOR	JA	TIMOTHY VALLOWE		
ROSS AVIATION	FIXED BASE OPERATOR	RA	RICK WIELEBSKI	480-948-2400	480-948-3874
SIGNATURE FLIGHT SUPPORT	FIXED BASE OPERATOR	SFS	GREG GIBSON	480-951-2525	

<b>HANGAR, SHADE &amp; OFFICE LEASING SERVICES</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
GEMINI AIR GROUP	HANGAR LEASING	GEM	TIMOTHY CARPAY	480-991-5387	480-991-3373
ROSS AVIATION SOUTH COMPLEX	HANGAR/OFFICE LEASING	RASC	RICK WIELEBSKI	480-948-2400	480-948-3874

<b>IN-FLIGHT CATERING SERVICES</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
AIR CULINAIRE WORLDWIDE, LLC	IN-FLIGHT CATERING	MOBILE	CHRIS EVANS	1-800-247-2433	
BASHAS INC. dba AJ'S FINE FOODS	IN-FLIGHT CATERING	MOBILE	MICHAEL BASHA	480-940-6731	480-940-2245
BRUNCH CAFÉ	IN-FLIGHT CATERING	MOBILE	KEVIN RASMUS	480-398-7174	
EMILY'S EVENTS LLC	IN-FLIGHT CATERING	MOBILE	EMILY GARNER	480-361-1800	
LIVE RICH PROJECT, LLC dba JETFARE CATERING	IN-FLIGHT CATERING	MOBILE	NORA SNELLING	1-800-353-3327	
VOLANTI PRIVATE JET CATERING	IN-FLIGHT CATERING	MOBILE	DEE DEE MAZA	480-636-1722	

<b>U.S. GOVERNMENT</b>					
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>LOCATION</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
FAA CONTROL TOWER	SDL AIR TRAFFIC CONTROL	TOWER	STEVE RAULSTON	480-609-7585	480-922-4982
U.S. CUSTOMS	U.S. CUSTOMS	OPS	OFF. KENNEDY/ARVIZU	480-312-8483	480-312-8485

**LEGEND:**

Green = New Permit
Yellow = Recently Cancelled Permit
Orange = Suspension/Pending Revocation
Red = Permit Revoked

**ACC** = Air Commerce Center; 14605 N. Airport Drive, Scottsdale, AZ 85260

**JA** = Jet Aviation; 14650 N. Airport Drive, Scottsdale, AZ 85260

**RA** = Ross Aviation; 14600 N. Airport Drive, Scottsdale, AZ 85260

**RASC** = Ross Aviation South Complex; 14700 N. Airport Drive, Scottsdale, AZ 85260

**SFS** = Signature Flight Support; 15290 N. 78th Way, Scottsdale, AZ 85260

**RANC** = Ross Aviation North Complex; 15115 N. Airport Drive, Scottsdale, AZ 85260

**TOWER** = FAA Air Traffic Control Tower; 14960 N. 78th Way, Scottsdale, AZ 85260

**GRNWY** = Greenway Hangars and Shades; 15135 N. Airport Drive, Scottsdale, AZ 85260

## AIRPARK AERONAUTICAL BUSINESS PERMITS & TENANTS

FEBRUARY 2021

<b>AIRCRAFT CHARTER, SALES &amp; MANAGEMENT &amp; SPECIALTY SERVICES</b>				
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
ALANTE AIR CHARTER	AIRCRAFT CHARTER/MANAGEMENT	RYAN HAMILTON		605-593-8960
COPPER STATE TURBINE ENGINE CO.	AIRCRAFT ENGINE OVERHAUL SERVICES	JIM NORDSTROM	480-500-6677	480-991-3067
DELTA PRIVATE JETS	AIRCRAFT CHARTER/MANAGEMENT	REX BEVINS	859-534-4314	859-334-6547
FRESH AIRCRAFT SALES, LLC	AIRCRAFT SALES	JOHN CALHOUN	602-717-2336	480-820-2566
EXECUTIVE JET MANAGEMENT	AIRCRAFT MANAGEMENT	CHRISTINE LEBER	513-979-6709	
GEMINI AIR GROUP	AIRCRAFT MANAGEMENT	TIMOTHY CARPAY	480-991-5387	480-991-3373

PINNACLE AIR GROUP	AIRCRAFT CHARTER/MANAGEMENT/SALES	SCOTT GUETTI	480-998-8989	480-998-7993
PRIME JET	AIRCRAFT CHARTER/MANAGEMENT	CHERYL JANKE	310-486-2088	303-648-4685
SCOTTSDALE HANGAR ONE	AIRCRAFT MANAGEMENT	ANDY SHAFER	480-624-9000	480-659-6051
THE COFFMAN COMPANIES	AIRCRAFT MANAGEMENT	JEFF COFFMAN	480-393-0770	480-393-7774

<b>HELICOPTER RENTAL, LEASING &amp; FLIGHT TRAINING</b>				
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
SUNSTATE HELICOPTERS	SPECIAL COMMERCIAL FLYING	CHRIS DOBKINS	602-469-3182	
WESTERN SKY HELICOPTERS	SPECIAL COMMERCIAL FLYING	VANESSA CLIFTON	480-416-6415	

<b>HANGAR, SHADE &amp; OFFICE LEASING SERVICES</b>				
<b>BUSINESS NAME</b>	<b>ACTIVITY</b>	<b>CONTACT</b>	<b>TELEPHONE</b>	<b>FAX</b>
15220 N. 75TH ST., LLC	HANGAR/SHADE LEASING	JOHN CAUVIN	480-435-3163	
7345 ACOMA LLC	HANGAR/SHADE LEASING	MOSHE BAR	480-483-8107	480-483-8172
AIRPARK LAND, LLC	HANGAR/SHADE LEASING	CRAIG JACKSON	480-421-6694	
ASTOR AIRPARK HOLDINGS	HANGAR/SHADE LEASING	REG COOPER	480-483-1999	480-443-7776
AVALON ONE	HANGAR/SHADE LEASING	SAMIR KANUGA	480-718-2412	
BATES FAMILY TRUST	HANGAR/SHADE LEASING	AUSTIN BATES	480-443-8287	480-443-8385
BECK LANE HANGARS LLC	HANGAR/SHADE LEASING	RYAN HAMILTON		
BCO	HANGAR/SHADE LEASING	LYNN BABCOCK	480-922-0490	480-922-0839
BUILDING D	HANGAR/SHADE LEASING	SCOTT LYON	480-367-6200	
CENTRAL IMPLEMENT	HANGAR/SHADE LEASING	PERRY CASE	480-998-8989	
CC OFFICE LLC	HANGAR/SHADE LEASING	JOSEPH ODDO	480-998-1444	480-951-1392
CWIE MANAGEMENT RESOURCES	HANGAR/SHADE LEASING	FRANK CADWELL	480-449-7751	480-449-8814

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DEVELOPMENT SERVICES OF AMERICA	HANGAR/SHADE LEASING	RICHARD WILSON	480-927-4888	480-927-4889
GRAYSTAR CORPORATION	HANGAR/SHADE LEASING	JOHN MEYER	480-483-1985	480-483-1726
GREAT AMERICAN HANGAR	HANGAR/SHADE LEASING	MARK BOSCO	916-391-5000	916-391-5001
HANGAR THREE	HANGAR/SHADE LEASING	JIM KEELEY	480-596-9000	480-948-0502
JET LINX SCOTTSDALE	HANGAR/SHADE LEASING	JON HULBURD	866-538-5469	888-398-3189
JON VESELY REVOCABLE TRUST	HANGAR/SHADE LEASING	JOHN MEYER	480-483-1985	480-483-1726
LARRY COFFEY	HANGAR/SHADE LEASING	LARRY COFFEY	480-607-0140	
LOOKOUT PEAK, LLC	HANGAR/SHADE LEASING	MOSHE BAR	480-483-8107	480-483-8172
PACIFIC MARINE MANAGEMENT	HANGAR/SHADE LEASING	AL CHITTENDEN	360-653-4266	360-659-4216
ROSS AVIATION	HANGAR/SHADE LEASING	RICK WIELEBSKI	480-948-2400	480-443-7227
SCOTTSDALE HANGAR ONE	HANGAR/SHADE LEASING	ANDY SHAFER	480-624-9000	480-659-6051
SKY HARBOR LEASING, LLC	HANGAR/SHADE LEASING	REG COOPER	480-483-1999	
SOUTHWEST JET CENTER	HANGAR/SHADE LEASING	GARY DAICHENDT	949-254-3027	
WALLACE HOLDINGS	HANGAR/SHADE LEASING	BOB WALLACE	480-998-8861	480-998-0388
WATTS INVESTMENTS, LLC	HANGAR/SHADE LEASING	CHRIS NUTE	602-761-4571	302-275-3346
7689, LLC	HANGAR/SHADE LEASING	JOHN MEYER	480-289-5715	480-751-1559

**LEGEND:**

Green = New Permit
Yellow = Recently Cancelled Permit
Orange = Suspension/Pending Revocation
Red = Permit Revoked



## COMMISSION INFORMATION REPORT

Discussion and Input Regarding Monthly  
Financial Reports for December 2020

Agenda Item No.: 6

Meeting Date: 02/17/21

Staff Contact: Carmen Williams,  
Aviation Finance & Administration Manager

Phone: (480) 312-8475

### AVIATION OPERATING BUDGET FISCAL YEAR 2020/21

	FY 2020/21		FY 2020/21 Year to Date (through December 2020)			
	Adopted Budget	FY 2020/21 Approved Budget	Approved Budget	Actual	Dollar Variance	% Variance
Revenue	\$5,743,088	\$5,743,088	\$2,393,901	\$3,007,924	\$614,023	26%
Expenses	\$2,547,579	\$2,315,796	\$1,246,946	\$1,228,532	-\$18,414	-1%
Net	\$3,195,509	\$3,427,292	\$1,146,955	\$1,779,392	\$632,437	

### AVIATION FUND CASH BALANCE

	Operating	CIP Funds	Total
As of 12/31/20	\$4,061,036	\$2,026,481	\$6,087,517
As of 12/31/19	\$9,727,124	-\$486,281	\$9,240,843

### MONTHLY REVENUE AND EXPENDITURE COMPARISON (ACTUALS)

	December		Dollar Variance	% Variance
	2019	2020		
Revenue	\$495,008	\$745,097	\$250,089	51%
Expenses	\$193,103	\$321,534	\$128,431	67%
Net	\$301,905	\$423,563	\$121,658	

### ACCOUNTS RECEIVABLE AGING REPORT

Aging Report Data as of 1/4/2021

		Current	1-30 Days	31-60 Days	61-90 Days	91-120 Days	>120 Days	Total Amt Due
All Accounts	Total	165,448.74	-574.31	-1.24	0.00	-56.24	-292.50	164,524.45

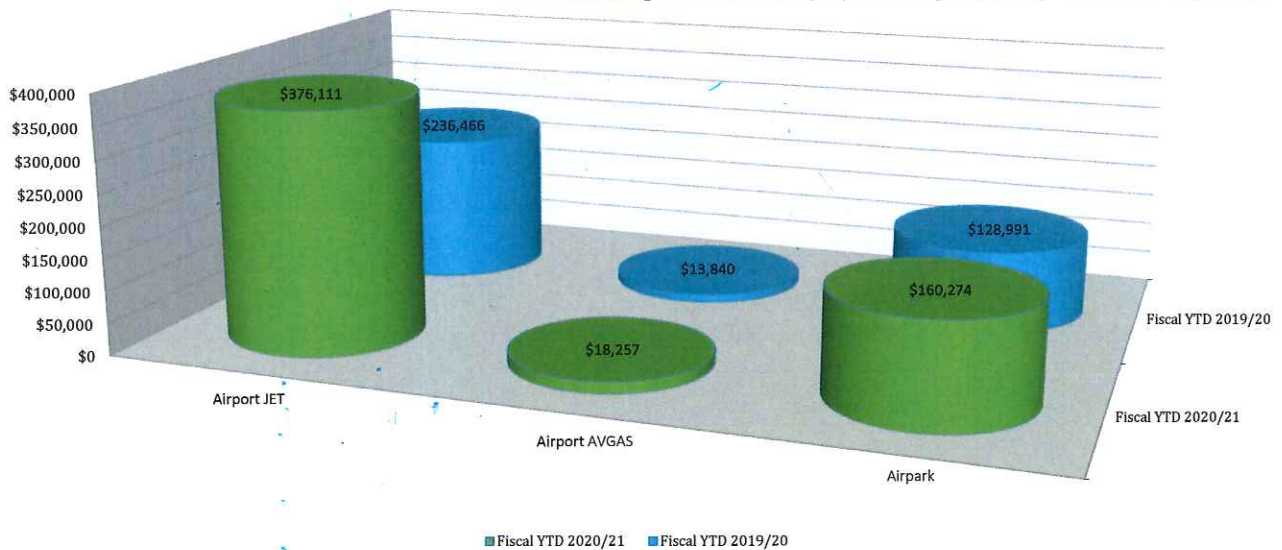
### Fuel Flowage (@ \$0.10 per gallon)

	December 2019			December 2020			% Change From Last Yr	
	Revenue	Gallons	% Total	Revenue	Gallons	% Total		
Airport JET	\$56,795	709,937	71.9%	\$94,743	947,427	74.9%	33.5%	Gal
Airport AVGAS	\$2,416	30,197	3.1%	\$3,501	35,006	2.8%	15.9%	Gal
Airpark	\$19,763	247,034	25.0%	\$28,236	282,358	22.3%	14.3%	Gal
	<b>\$78,973</b>	<b>987,167</b>	<b>100.0%</b>	<b>\$126,479</b>	<b>1,264,790</b>	<b>100.0%</b>	<b>28.1%</b>	Gal

	Fiscal YTD 2019/20			Fiscal YTD 2020/21			% Change From Last Yr	
	Revenue	Gallons	% Total	Revenue	Gallons	% Total		
Airport JET	\$236,466	2,955,828	62.3%	\$376,111	3,761,114	67.8%	27.2%	Gal
Airport AVGAS	\$13,840	172,995	3.6%	\$18,257	182,573	3.3%	5.5%	Gal
Airpark	\$128,991	1,612,383	34.0%	\$160,274	1,602,738	28.9%	-0.6%	Gal
	<b>\$379,296</b>	<b>4,741,206</b>	<b>100.0%</b>	<b>\$554,642</b>	<b>5,546,425</b>	<b>100.0%</b>	<b>17.0%</b>	Gal

Scottsdale Airport Fuel Flowage (@ \$0.10 per gallon) - Fiscal Year-to-Date





**COMMISSION INFORMATION REPORT**

Discussion and input regarding the Monthly Airport Construction Report for February 2021

**Agenda Item No.:** 7

**Meeting Date:** 2/17/21

**Staff Contact:** Chris Read,  
Asst. Aviation Director-Operations

**Phone:** (480) 312-2674

**INFORMATION**

Airport Construction Update for February 2021

**PURPOSE**

The purpose of this item is to keep the Airport Advisory Commission informed as to the status of all construction activity at the City's airport.

**PROJECT(S) CURRENTLY UNDERWAY**

**North GA Box Hangar Project Phase 2 - (\$3,972,571.00)  
December 2020 to October 2021**

<b>% Complete</b>	<b>Completed Work - January</b>	<b>Anticipated Work - February</b>	<b>Operational Impacts</b>
1%	None	Pavement Demolition, underground utilities, building footers	Small section of Kilo ramp closed



**FUTURE PROJECTS**

<b>Runway Rehabilitation Pre-Construction Phase Project</b>				
<b><u>Description</u></b>	<b><u>Contractor Pre-Construc. Cost</u></b>	<b><u>Status</u></b>	<b><u>Start Date</u></b>	<b><u>Estimated Completion Date</u></b>
Design, value engineering, bidding of the Runway Rehabilitation Project	\$112,539.00	Pre-construction phase underway	August 2020	March 2021

<b>Runway Rehabilitation Construction Phase Project</b>				
<b><u>Description</u></b>	<b><u>Approximate Cost</u></b>	<b><u>Status</u></b>	<b><u>Estimated Start Date</u></b>	<b><u>Estimated Completion Date</u></b>
Construction of runway, shoulders, blast pads and lighting	\$12,000,000.00	Waiting for design phase to be completed	July 2021	August 2021



## COMMISSION INFORMATION REPORT

Discussion and input regarding Operations Report for January 2021

**Agenda Item No.:** 8

**Meeting Date:** 2/17/21

**Staff Contact:** Chris Read,  
Asst. Aviation Director-Operations

**Phone:** (480) 312-2674

### INFORMATION

Airport Monthly Operations Update for January 2021.

### PURPOSE

The purpose of this item is to keep the Airport Advisory Commission informed as to the operational status of the Airport.

#### BASED AIRCRAFT

	<u>Helicopter</u>	<u>Single Engine</u>	<u>Twin Engine</u>	<u>Jet</u>	<u>Total</u>
Current Month	28	213	33	173	447
January 2020	30	234	38	176	478

#### OPERATIONS

	<u>January 2020</u>	<u>January 2021</u>	<u>% Δ</u>	<u>2020 YTD</u>	<u>2021 YTD</u>	<u>% Δ</u>
Total	17,355	15,715	-9.5	17,355	15,715	-9.5
IFR	5,206	5,138	-1.3	5,206	5,138	-1.3

#### ALERTS

<u>Date</u>	<u>Type</u>	<u>Description</u>
1/01/21	1	Cirrus SR-22, fuel flow indicator problem
1/14/21	2	Cessna 180, rough running engine
1/16/21	1	Cessna 310, electrical problems

#### INCIDENTS

<u>Date</u>	<u>Description</u>
1/04/21	Cessna 210, flat main tire after landing
1/05/21	Cessna 172, flat main tire during takeoff
1/07/21	Israel Aircraft Industries 1124A, fuel leak
1/07/21	Oil spill, Jet Aviation ramp
1/08/21	Beechcraft E33, flat nose tire during taxi
1/12/21	Oil spill, Ross Aviation ramp
1/24/21	Tow truck hit parking lot shade canopy
1/27/21	Cessna 172, fuel leak during start up



**ENFORCEMENT ACTIONS**

<u>Date</u>	<u>Violation</u>	<u>Enforcement Method Used</u>	<u>Comments</u>
1/04/21	Abandoned vehicle in parking lot	Vehicle towed	1 <sup>st</sup> Violation

**U.S. CUSTOMS**

<u>*Revenue (Month)</u>	<u>Total Uses Month</u>	<u>U.S. Visit Uses (flights/current month)</u>	<u>Comments</u>
\$95,225	171	32	<b>U.S. Visit Summary</b> 98 Mexican, 5 Italian, 3 British, 3 Netherlander, 9 Swiss, 2 Italian, 1 Thailander, 1 Icelander, 1 Belgian, 2 Canadian

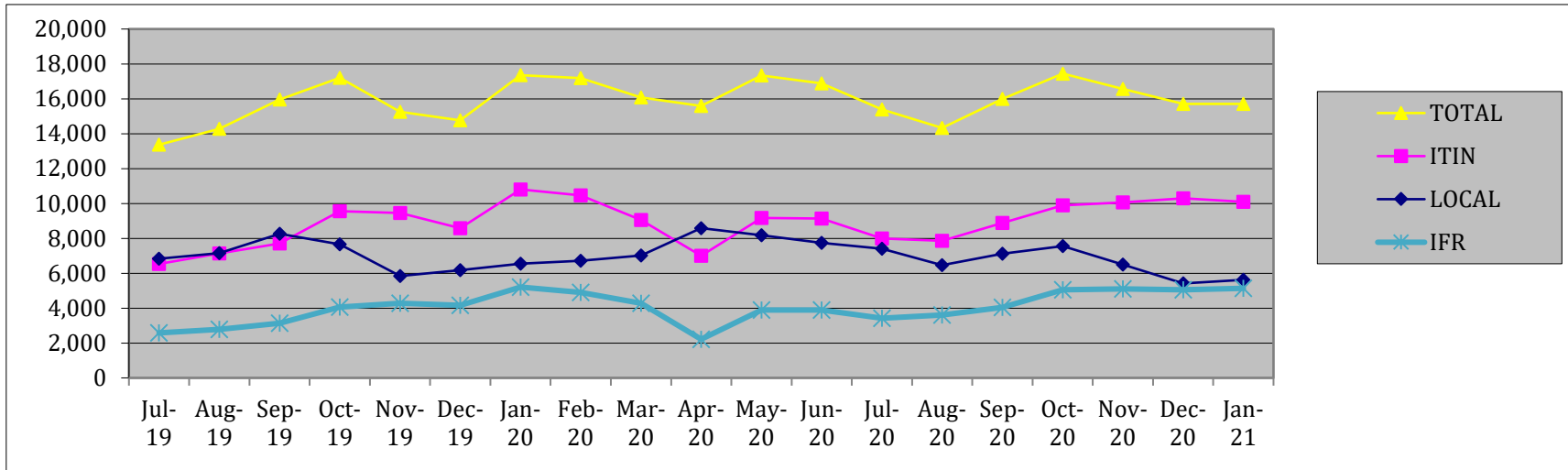
<u>*Revenue (FYTD)</u>		<u>Total Uses Month</u>		<u>Total Uses (FYTD)</u>	
2020/2021	\$402,075	JANUARY 2021	171	2020/2021	721
2019/2020	\$340,950	JANUARY 2020	138	2019/2020	626

\*Revenue = User Fees and Overtime Fees Charged to Users  
 75,000 lbs. + PPR = 5 (calendar year 2021)

# SCOTTSDALE AIRPORT OPERATIONS 2019-2021



	Jul-19	Aug-19	Sep-19	Oct-19	Nov-19	Dec-19	Jan-20	Feb-20	Mar-20	Apr-20	May-20	Jun-20	Jul-20	Aug-20	Sep-20	Oct-20	Nov-20	Dec-20	Jan-21
ITIN	6,535	7,133	7,707	9,548	9,452	8,580	10,804	10,467	9,054	7,005	9,164	9,140	7,996	7,854	8,886	9,897	10,062	10,298	10,089
LOCAL	6,831	7,145	8,255	7,660	5,839	6,181	6,551	6,720	7,017	8,587	8,169	7,740	7,398	6,469	7,114	7,548	6,496	5,416	5,626
IFR	2,579	2,777	3,133	4,064	4,271	4,159	5,206	4,897	4,284	2,215	3,885	3,896	3,429	3,607	4,046	5,057	5,094	5,059	5,138
TOTAL	13,366	14,278	15,962	17,208	15,261	14,761	17,355	17,187	16,071	15,592	17,333	16,880	15,394	14,323	16,000	17,445	16,558	15,714	15,715





**COMMISSION INFORMATION REPORT**

Discussion and Input Regarding  
Public Outreach Programs and Planning Projects

**Agenda Item No:** 9

**Meeting Date:** 02/17/21

**Staff Contact:** Sarah Ferrara,  
Aviation Planning & Outreach  
Coordinator

**Phone:** (480) 312-8482

**INFORMATION**

Monthly update of the marketing, community, planning and pilot outreach programs at Scottsdale Airport.

**PURPOSE**

The purpose of this item is to keep the Airport Advisory Commission informed of the airport’s marketing, outreach and planning projects efforts.

Noise Program Outreach		
Description	Purpose	Status
Next Gen/Phoenix Metroplex	City Council has filed a petition for review of the FAA action related to flight routes.	In progress
Noise Outreach	Will conduct noise outreach as necessary.	Completed
Pilot Outreach		
Description	Purpose	Status
Pilot Briefing & Outreach	The next FAA’s quarterly Scottsdale Flight Schools workgroup meeting has not been rescheduled yet.	Completed
Voluntary Curfew Outreach (10:00 p.m. – 6:00 a.m.)	The Voluntary Curfew Program is designed to respond to a complaint received for an operation between 10 p.m. and 6 a.m. If a flight can be confirmed, a letter is sent out to the operator to ask them for their cooperation in flying outside these hours when possible. <b>There were seven voluntary curfew letters sent out in January.</b>	<b>Completed</b>
Planning Projects		
Description	Purpose	Status
Monitor property development through the Planning Department	Working with the Planning Department to protect the airspace and development uses near Scottsdale Airport. <b>The Planning and Zoning reports listed four projects within the Airport Influence Area in January.</b>	<b>Completed</b>

<b>Community Outreach and Marketing</b>		
<b>Description</b>	<b>Purpose</b>	<b>Status</b>
<b>Media, social media, &amp; list serves</b>	Continue to post photos and information on social media, trying to increase presence. Sent list serve on Runway Rehabilitation project announcing start date and included reminder for tenants to find temporary accommodations.	Completed
<b>Brochures, flyers, other print materials, webpages &amp; videos</b>	Continuing to update Runway Rehabilitation webpage. With opening of Jet Aviation, updated Scottsdale Airport brochure (now available) and the Pilot Guide is being printed.	In process
<b>Community outreach, presentations and events</b>	Created a public communications and outreach plan for the upcoming Runway Rehabilitation project to inform users and tenants about the upcoming project, timeline, and impacts.	Completed



## COMMISSION INFORMATION REPORT

Administrative report from the Aviation Director, or designee, regarding the status of pending aviation-related items

**Agenda Item No:** 10

**Meeting Date:** 02/17/21

**Staff Contact:** Gary P. Mascaro,  
Aviation Director

**Phone:** (480) 312-7735

### INFORMATION

Discussion regarding status of the Airport Advisory Commission items to City Council, and aviation-related items approved by Planning Commission, Design Review Board or City Council.

- Attachment(s):
1. Airport Advisory Commission Items to City Council
  2. Aviation-related items to Planning Commission, Design Review Board, or City Council
  3. City Council Meeting Calendar

**AIRPORT ADVISORY COMMISSION AVIATION ITEMS TO CITY COUNCIL  
2021**

AIRPORT COMMISSION DATE	APPROVED	ITEM DESCRIPTION	CITY COUNCIL DATE	APPROVED
01/20/21	6-0	<p><b>Airport Lease Agreement</b> Discussion and Possible Action to Recommend Adoption of Resolution No. 12038, Authorizing Lease Agreement with Pantheon Global Realty for the lease of North General Aviation Executive Box Hangar Space at the Scottsdale Airport. Staff contact: Carmen Williams, Aviation Finance &amp; Administration Manager, 480-312-8475, cawilliams@scottsdaleaz.gov</p>	02/02/21	Approved on consent
01/20/21	6-0	<p><b>Aviation Real Estate Brokerage Consulting Services Contract</b> Discussion and Possible Action to Recommend Adoption of Resolution No. 12044, Authorizing Contract No. 2021-015-COS with DMD Real Estate Group, LLC for Aviation Real Estate Brokerage Consulting Services. Staff contact: Carmen Williams, Aviation Finance &amp; Administration Manager, 480-312-8475, cawilliams@scottsdaleaz.gov</p>	02/02/21	Approved on consent
01/20/21	6-0	<p><b>Contract Amendment for On-Call Aviation Department Engineering Service</b> Discussion and Possible Action to Recommend that City Council Adopt Resolution 12045 authorizing On-Call Engineering Services Contract Amendment 2018-055-COS-A1 with Mead &amp; Hunt, Inc. increasing the annual contract expenditure limit from \$1,000,000 to \$1,500,000 for design and construction administration of various Aviation Department projects. Staff contact: Chris Read, Assistant Aviation Director-Operations, 480-312-2674, cread@scottsaleaz.gov</p>	02/02/21	Approved on consent
02/17/21	?	<p><b>Airport Lease Agreement</b> Discussion and Possible Action to Recommend Adoption of Resolution No. 12075, Authorizing Lease Agreement with 3R Ranch, Inc. (2021-022-COS) for the lease of the North General Aviation Executive Box Hangar Space at the Scottsdale Airport.</p>	03/02/21	?

**AVIATION-RELATED ITEMS TO PLANNING COMMISSION, DESIGN REVIEW BOARD OR CITY COUNCIL  
(Projects that may be on airport, have taxi lane access, have height implications, or have sensitive noise uses)  
2021**

AIRPORT COMMISSION DATE	APPROVED	ITEM DESCRIPTION	PLANNING, DRB, OR CITY COUNCIL AGENDA DATE	APPROVED
?	?	<p><b>Ina Levine Jewish Community Campus</b></p> <p>Request by applicant for a zoning district map amendment form a Single-family Residential (R1-35) to Special Campus with Planned Shared District (SC PSD) including a development plan with amended development standards for building setbacks, building setbacks and landscape buffers on a +/- 28-acre site. Includes residential health care facility. 11-ZN-2020</p>	?	?
?	?	<p><b>Greystar</b></p> <p>Request by owner for approval of a Zoning District Map Amendment from Single-family Residential (R1-35) to Commercial Office (C-O) zoning on a +/-4.5-acre site located approximately 1,000 feet northwest of the East Raintree Drive and North 90th Street intersection for residential health care facility. 16-ZN-2020</p>	?	?
N/A	N/A	<p><b>Raintree Drive Residential</b></p> <p>Request approval of the site plan, landscape plan, and building elevations for a new 5-story residential development with 109 units and 218,419 square feet of building area on a +/- 3.12-acre site located at 8555 E. Raintree Drive. 1-DR-2021</p>	?	?

## 2021 City Council Meeting Calendar

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**Key**

	Council Meetings
	Optional Additional Mtg and/or Study Session
	General Plan
	Events
	City of Scottsdale Holidays
	Election
	No meetings will be scheduled
	Strategic Planning Workshop

- Jan: 14 - MLK Dinner
- Feb:
- Mar: 7-10 - NLC Conference
- Apr: xx - State of the City Address
- May: 18 - Tentative Budget Adoption
- June: 8 - Final Budget Adoption  
15 - Charter Officer Reviews

- Aug: 31 - AZ League Conference (Arizona Biltmore)
- Sep: 1-3 - AZ League Conference (Arizona Biltmore)
- Oct:
- Nov: 2 - General Election  
17-20 - NLC Summit
- Dec: 7-8 - Major General Plan Amendments and Reg Council Mtgs  
9 - Optional Meeting Date for Regular and GP items, if needed





## COMMISSION ACTION REPORT

Discussion and possible action to modify the Airport Advisory Commission meeting schedule and Commission item calendar

**Agenda Item No.:** 11

**Meeting Date:** 02/17/21

**Staff Contact:** Gary P. Mascaro,  
Aviation Director

**Phone:** (480) 312-7735

### ACTION

Review Airport Advisory Commission meeting schedule.

### PURPOSE

Pursuant to By-Laws of the Scottsdale Airport Advisory Commission, Section 202, *“Regular meetings of the Commission shall be held on the third Wednesday of each month immediately following the study session, unless otherwise scheduled by majority vote of its members. In the event the Commission desires not to hold the preceding study session, the regular meeting shall begin at 5:00 p.m., unless otherwise scheduled by majority vote of its members.”*

Attachment(s): 1. Airport Advisory Commission meeting schedule

Action taken:

**AIRPORT ADVISORY COMMISSION SCHEDULE OF MEETINGS - 2021**  
(Including anticipated topics and timeline for discussion)

JANUARY						
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- Election of Officers
- By-Laws Review
- Quarterly Noise Program Update

FEBRUARY						
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- Quarterly Noise Program Update
- Aviation Enterprise Fund Five-Year Financial Plan

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- Risk Management Insurance Update

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- Chamber Update

JULY						
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- Quarterly Noise Program Update

AUGUST						
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- Experience Scottsdale Update

OCTOBER						
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- Quarterly Noise Program Update

NOVEMBER						
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- AZ Business Aviation Assn. Update

DECEMBER						
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