

Draft



Via Linda
Streetscape Guidelines

Scottsdale, Arizona

June 14, 1994



Via Linda Streetscape Guidelines Scottsdale, Arizona

Table of Contents

Introduction.....	3
Background.....	3
Circulation Element Reference.....	4
Environmental Design Element Reference.....	4
Eagle Ridge Area Plan Reference.....	4
Applicability.....	4
Scenic Corridor Designation.....	4
Streetscape Guidelines Components.....	5
Landscape Concept.....	5
Via Linda Symbol Graphic.....	6
Entry Monument Concept.....	6
Gateway Concept.....	7
Wall Design.....	8
Light Pole Color Scheme.....	8
Building And Wall Height/ Setbacks.....	8
Sidewalks.....	9
Special Streetscape Treatment for Via Linda And 136th Street Intersection.....	9

INTRODUCTION

Via Linda is the northernmost major east-west thoroughfare through the Shea Corridor from its present intersection with Shea Boulevard to its future connection with Palisades Boulevard at the eastern boundary between the city of Scottsdale and the Town of Fountain Hills. The views of the Sonoran Desert for travelers on this roadway are as fine as any in the community. Within 25 years, an average of 29,000 vehicles will be traveling this segment of Via Linda on any weekday. The mountain and desert vistas their passengers will see depends on the effectiveness of streetscape guidelines that are established today.

In June, 1993, the Scottsdale City Council directed staff to prepare a set of guidelines for the development of a unified streetscape image for

Shea that would help preserve and enhance mountain and valley views from that thoroughfare.

Subsequently, the Council directed the city staff to prepare a similar set of guidelines for Via Linda from the Central Arizona Project Canal crossing to its eastern terminus, as shown in Figure 1.

A team of staff members from Planning and Community Development, Transportation Planning, Capital Project Management, Field Services, and Parks Maintenance has assisted in the development and review of these guidelines.

BACKGROUND

When the East Shea area is developed to the full extent of Scottsdale's General Plan, the segment of Via Linda north of Shea Boulevard will provide access to two elementary schools, a middle school and high school, two neighborhood parks, a specialty park and community park, several neighborhood retail centers, the Mayo Clinic campus, the primary office/commercial/research core of the

Mayo Clinic Support District at 136th Street, several high density multi-family residential developments, and a broad spectrum of single family residential neighborhoods.

The existing and scheduled right-of-way width for Via Linda from the CAP Canal crossing to 136th Street is 55' from the centerline on both sides for a total width of 110' and is 50' from the centerline on both sides for a total width of 100' from 136th Street eastward to the Palisades Boulevard intersection.

The location of the centerline for Via Linda has not yet been established from the 140th Street alignment to Palisades Boulevard, and the roadway is shown conceptually in all General Plan elements and area studies. At present, the terrain through which this segment of Via Linda will pass is

pristine.

There will be a total of about 59,000 linear feet of frontage along both sides of the 5.5 miles of Via Linda from the CAP Canal to Palisades. Presently, less than 27% of that frontage is either developed or actively under design for development; however, it is anticipated that all of the frontage

will be built out within the next 20 years. In the last four years, the East Shea area has absorbed about 65% of all new residential construction completed in the entire city-more than 5,000 dwelling units.

From the CAP Canal to the 140th Street alignment, Via Linda rises about 230 feet in elevation on a gradual grade of less than 2%. Over the next mile, the grade increases dramatically to more than 6% as the roadway enters a valley in the southern slopes of the McDowell Mountains and rises to an elevation of 2,100 feet, offering east-bound travelers their closest visual experience with one of the community's finest natural assets. The

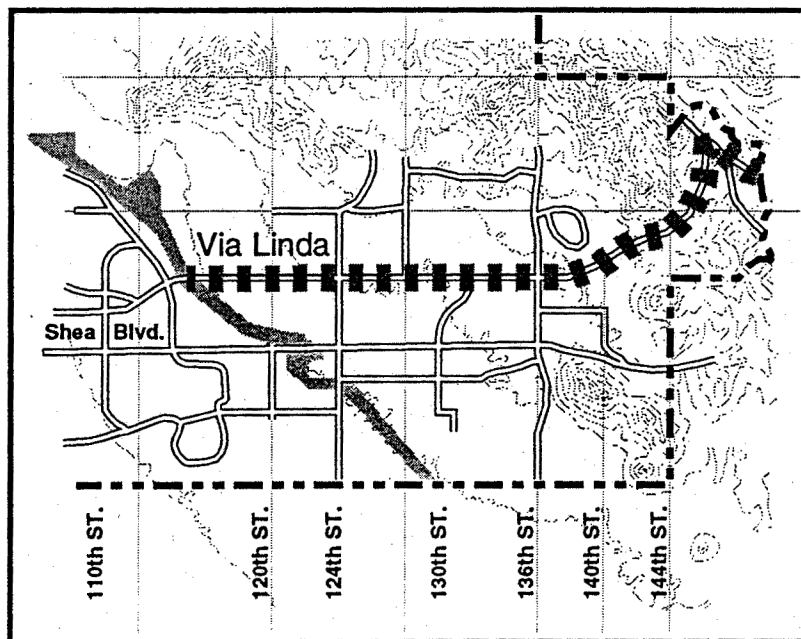


Figure 1.

elevation of the roadway remains near the 2,100-foot level for another mile until its intersection with Palisades Boulevard.

Circulation Element Reference

- Via Linda is classified in the Circulation Element of Scottsdale's General Plan as a "minor arterial" street from the CAP Canal to 136th Street and as a "major collector" street from 136th to its intersection with Palisades Boulevard. The thoroughfare runs through areas calling for both rural and mountain cross-section design and development standards.

- The cross-section for both minor arterials and major collectors in rural areas includes 2 traffic lanes, a bike lane, and a sidewalk in back of a vertical curb on both sides of a planted median.

- The cross-section for major collectors in mountain areas includes 2 traffic lanes, a bike lane, a rolled curb and no sidewalks on both sides of a planted median.

Environmental Design Element Reference

- The Environmental Design Element of Scottsdale's General Plan shows Via Linda to be in the Desert Suburban Character District and advises preservation and protection of scenic views and mountain vistas, urban landmarks and natural features such as washes and areas of dense vegetation.

- Streetscape is defined as the combination of individual design elements that give character to street frontages, including landscaping, street furniture, lighting, and sidewalks.

- Via Linda is located in the "Natural" streetscape designation where plant selection should be those that are native to the desert, and densities of planting areas should be similar to natural conditions.

- Three major wash corridors cross Via Linda between 128th Street and 140th Street.

- A "Scenic Corridor" designation should be applied where views to mountains and natural or man-made features will be enhanced.

- A gateway entry monument is planned at the intersection of Via Linda and Palisades. Entry monuments should consist of a module with design flexibility that will allow the incorporation of a custom feature patterned after a unique local condition. Landscaping should compliment the entry monument design.

Eagle Ridge Area Plan Reference

- With the exception of several rough jeep trails, Eagle Ridge is entirely in its natural state.

- Future development of Eagle Ridge must be sensitive to environmental constraints that limit road access...to the approximate alignments of the old jeep trails.

- The natural desert environment should be protected.

- Street patterns should emphasize environmental sensitivity.

- Collector street patterns should accommodate future connection(s) to Scottsdale through adjacent parcels when they develop.

APPLICABILITY

This document presents the staff team's recommended guidelines that should be addressed and applied in all development projects that will abut Via Linda and all city projects within the Via Linda right-of-way from the Central Arizona Project Canal crossing to the Palisades Boulevard intersection.

SCENIC CORRIDOR DESIGNATION

The importance of preserving and protecting mountain and natural desert views through the East Shea area, the significance of Via Linda as a major thoroughfare, and the likelihood of development along its frontage intensifying and accelerating during the next 20 years are all considerations that combined to motivate the staff team to recommend the designation of Via Linda as a "scenic corridor" from the CAP Canal to its terminus at Scottsdale's eastern boundary.

Staff has received numerous letters from three home owners associations and individual residents in the East Shea area who are encouraging and recommending a "scenic corridor" designation for Via Linda. The three associations are: Rio Montana, North East Scottsdale Property Owners Association, and Sonoran Heights.

Although some segments of the Via Linda frontage are either developed or pending development to an extent that would not allow the provision of full scenic setbacks, nearly 80% of the existing and proposed roadway frontage is either undeveloped or developed in a manner providing an acceptable setback which satisfies scenic corridor guidelines.

Within the 3-mile stretch of Via Linda from the CAP Canal crossing to 140th Street alignment,

there are about 19,000 linear feet of undeveloped frontage that could be included in a scenic corridor; 9,400 linear feet that presently provide some amount of clear setback area by virtue of scenic easements, natural area open space (N.A.O.S.) requirements, or equestrian easements; and only 3,600 linear feet that are developed with no setback for this segment of Via Linda be an average of 40' from the right-of-way line with a minimum setback of 30', resulting, therefore, in an average 190' width for the scenic corridor, including the 110' right-of-way for the thoroughfare.

The 2.5-mile segment of Via Linda proposed from 140th alignment of Palisades Boulevard, traversing the Eagle Ridge area, is undeveloped, and there are not formal development plans of record for the proximal properties; therefore, all of the 27,000 linear feet of future roadway frontage is considered to be available for the designation of a scenic corridor. Because of the special terrain features of this segment of Via Linda and its current pristine state, the staff team recommends that the scenic setback an average of 80' from the right-of-way line with a minimum setback of 60', resulting, therefore, in an average 260' width for the scenic corridor, including the 100' right-of-way for the thoroughfare. Although the 100'-wide right-of-way for the roadway may be aligned wherever future development plans and the physical condition so the terrain dictate within the 260' wide corridor, a minimum 60' offset from the edge of the corridor should be provided, as illustrated in Figure 2 below. The staff team further recommends that a stabilized path be included within this segment of the corridor.

Formalizing a "scenic corridor" designation for Via Linda from the CAP Canal crossing to its Palisades Boulevard intersection will require an amendment to the Environmental Design Element of Scottsdale's General Plan. Pending that action, application of the following streetscape guidelines recommended by the staff team in this document will help preserve and protect mountain and natural desert views along the developing frontage of the thoroughfare by preventing the construction of structures, parking areas, perimeter walls or frontage roadways within the setback area.

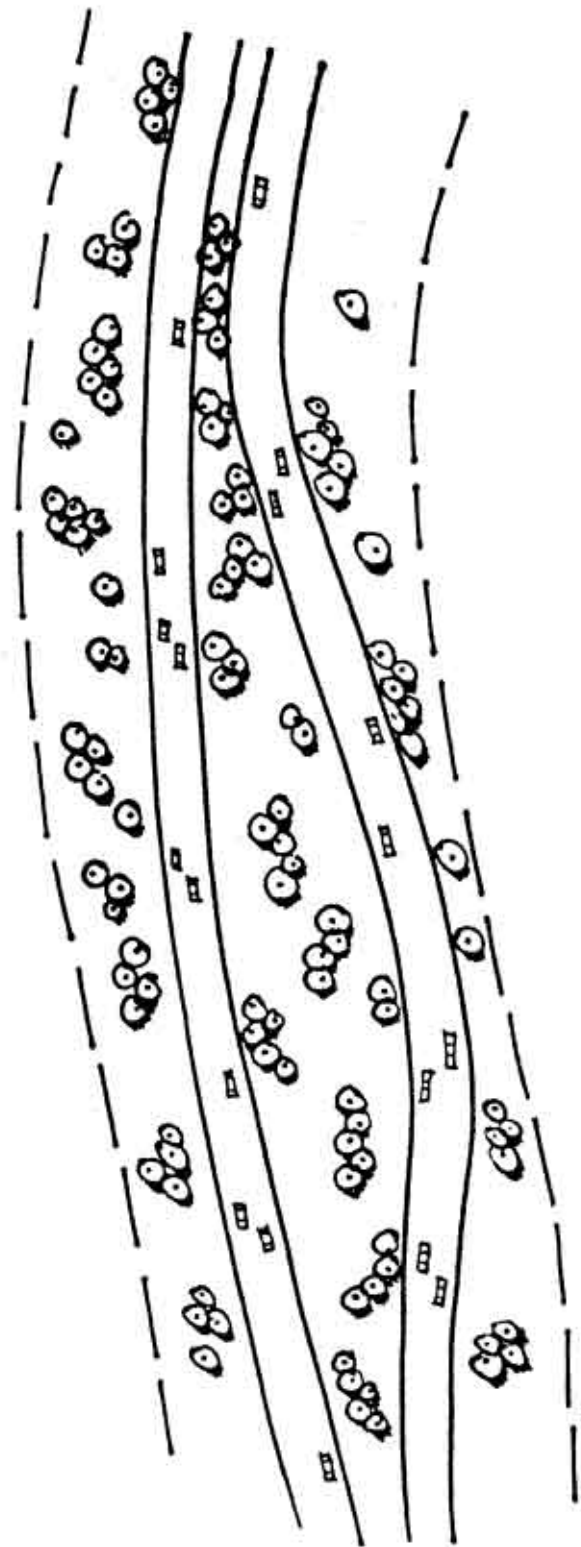


Figure 2.

STREETSCAPE GUIDELINES COMPONENTS

Guidelines for the following components are recommended by the staff team to create and enhanced streetscape appearance:

- Landscape concept
- Via Linda symbol graphic
- Entry monument concept
- Gateway concept
- Wall design
- Light pole color scheme
- Building and wall height/setbacks
- Sidewalks
- Special streetscape treatment for Via Linda and 136th Street intersection
- Maintenance
- Implementation

Landscape Concept

The Environmental Design Element of the General Plan has identified that plant material for medians and landscaped right-of-way areas should consist of naturally occurring vegetation; therefore, the scenic corridor and median areas should portray a natural appearance in plant types and densities as shown in Figure 3. The conceptual landscape design is illustrated in Figure 4 on the following page.

Where revegetation is needed for the scenic corridor and where landscape is proposed in medians or parts of rights-of-way, the plant palette should follow these criteria:

1. Plant material should reflect what is currently established in the surrounding landscape or be native plant material. All plant materials should be included in the Arizona Department of Water Resources approved plant list. Plant materials that are desert adapted but exhibit tall growth characteristics or are out of character with the natural landscape should be avoided.

2. Trees to be used as consistent unifying elements include: Foothill Palo Verde (*Cercidium microphyllum*), Velvet Mesquite (*Prosopis velutina*), and Ironwood (*Olneya tesota*.)

3. There should be trees planted at a minimum of one for every 1,000 square feet of landscaped area.

4. Shrubs should be planted at minimum rate of one for every 230 square feet of landscaped area. Trees and shrubs may be grouped into appropriate locations.

5. Plant material with overlapping flowering seasons should be used.

6. Ground covers and native seed mixes are encouraged for planting between trees and shrubs.

7. Succulents should be planted at the following rates: Saguaro, Barrel Cactus, and Ocotillo- 1 per 5,000 square feet. Cholla, Prickly Pear, and Hedgehog- 1 per 250 square feet.

8. Where streets intersect with Via Linda, proper sight lines should be preserved. Conflicts between these guidelines and needed sight distances should be evaluated on a case-by-case basis with safety being a priority. Refer to the City of Scottsdale's "Street Geometrics" for further information of sight line requirements.

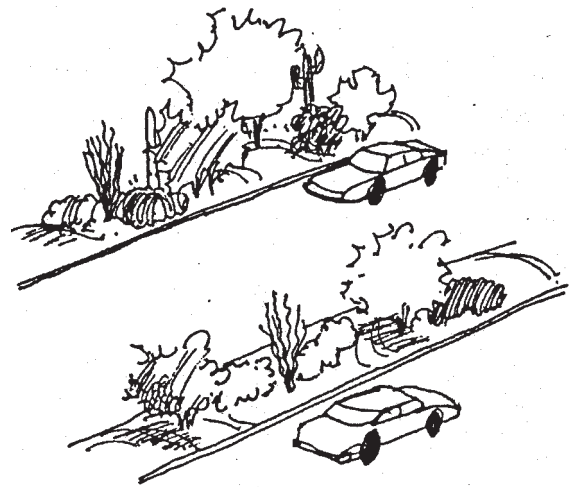


Figure 3.

Via Linda Symbol Graphic

A sense of identity for the Via Linda area can be achieved through a common graphic or symbol that is used repeatedly throughout the area. For this purpose, the symbol in Figure 5 on the following page can be used in a variety of applications such as railings, wall imprints, signs, monuments, and other graphic details.

Entry Monument Concept

To reinforce the sense of entry into Scottsdale from the town of Fountain Hills, an entry monument should be located on the north side of Via Linda west of its intersection with Palisades Boulevard. The standard city entry monument design should include an additional planter wall with the Boulevard Symbol Graphic incorporated as part of reveal, railing or other accent as shown in Figure 6 on the following page.

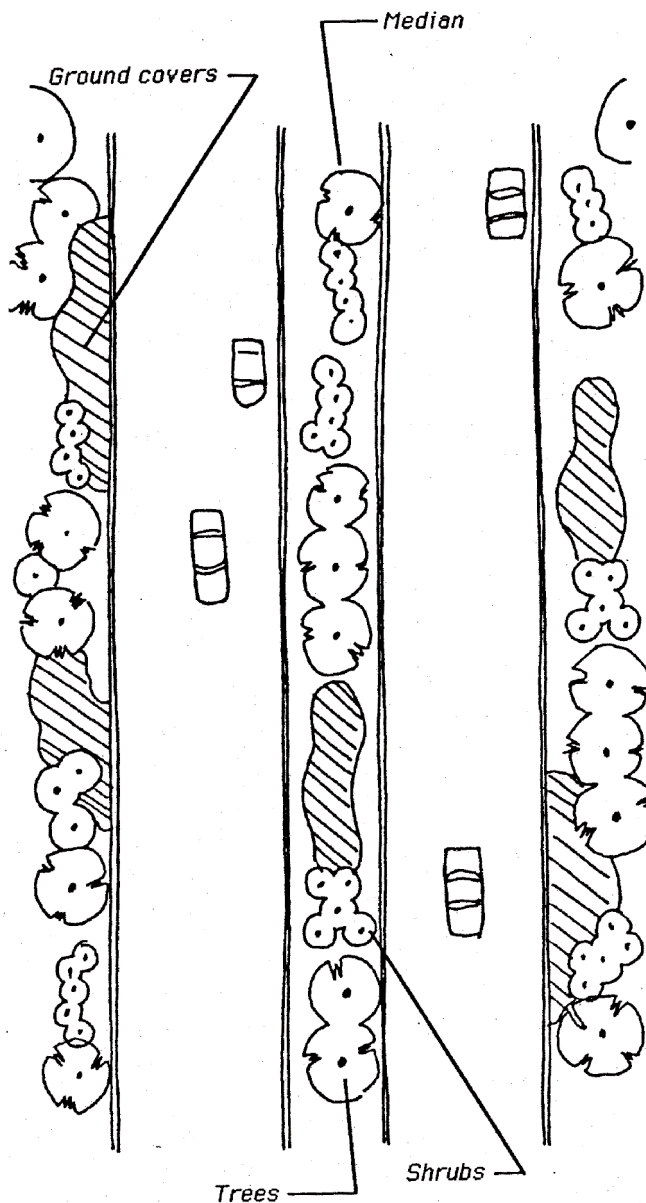


Figure 4.



Figure 5.

Gateway Concept

In addition to an entry monument, the area of Via Linda west of the Palisades Boulevard intersection lends itself well to the inclusion of a gateway feature. The Environmental Design Element of the General Plan proposes such as gateway feature on Via Linda near the Palisades intersection.

The gateway could include various elements such as sculptures, a scenic overlook, or a cultural/historical point of interest. An example of this type of project occurs at the McDowell Road and 64th Street intersection where the historic significance of the canals, desert heritage, and the summer solstice is presented in an environmental public art format. This example is not intended to specify what the gateway feature for Via Linda should be but is an idea of how to approach a potential theme. The following criteria should be considered for the gateway feature:

1. Funding of approximately \$150,000 should be appropriated for a gateway feature to be built in conjunction with the construction of Via Linda between 140th Street and Palisades Boulevard.
2. The gateway should be positioned to appropriately depict a sense of entry into the city.
3. Final concept and design of gateway should result from a competitive process involving a field of artists with experience in outdoor applications.

Wall Design

Wherever possible, development perimeter walls should be avoided so that views to the natural environment will be enhanced. In the event that walls are necessary, special attention should be given to ensure that the wall reflects the natural surroundings. General guidelines that should be followed in wall design are:

1. Muted desert tones (per ESLO design guidelines) and soft undulations in the wall
2. Vertical and horizontal breaks in wall alignment to minimize height and mass of the wall and provide visual relief
3. Special niches for plant material integrated into the wall

Examples of design elements that should be incorporated when considering wall construction are illustrated in Figure 7.

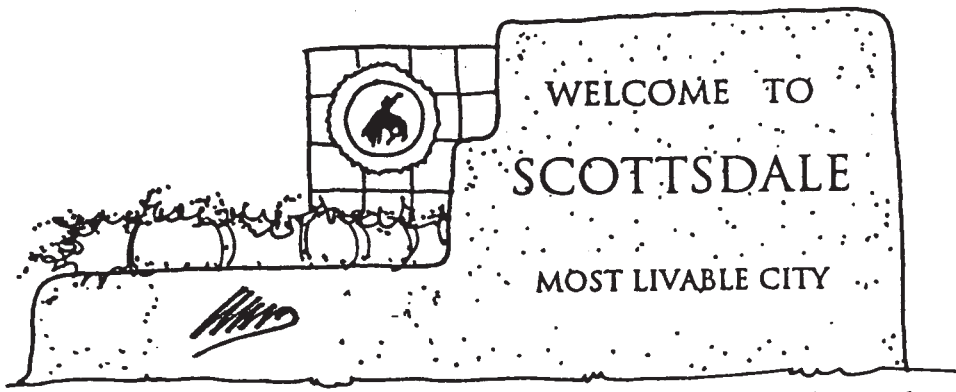


Figure 6.

Light Pole Color Scheme

The McDowell Mountains and lush desert vegetation along the Via Linda corridor provide the basis for which a light pole color is selected. Colors should be tested prior to selecting one for painting on all traffic signal, street light, and power poles erected east of the CAP Canal. This guideline should be implemented in conjunction with all Via Linda improvements. The following are potential colors that have tints reflected in the surrounding natural landscape viewed along Via Linda:

Frazer or approved equal:

4704D-Clover

5324D- Dust Storm

4334D- Terra Cotta

Building and Wall Height/Setbacks

Much of the scenic quality of the Via Linda area can be attributed to well preserved views of the McDowell Mountains and the wide Salt River Valley. It is vitally important that new development provide adequate setbacks from the scenic corridor and innovative solutions for screen/retaining walls and for building where more than one story is proposed. The overall effect on views should be judged as to whether a screen/retaining wall or a building with two or more stories can be built and retain an adequate level of view lines and corridors. To this end, these guidelines should be used as follows:

1. Natural open space should be preserved within the scenic corridor setback area.

2. Building height at the required setback line will not exceed 1 story or 24' above finished grade for any residential building constructed on property abutting Via Linda or for any non-residential building constructed within 120' of a Via Linda right-of-way line.

3. Where more than one story is proposed for a building, subsequent stories should be stepped back from the first. This guideline also applies where the building is within 100 feet of a wash or vista corridor that intersects with Via Linda.

4. Fills should not exceed 4' within 100' of a Via Linda right-of-way line.

5. The exposed height of screen walls, including any retaining wall sections, should not exceed 8' above natural grade at the scenic corridor setback line.

6. Setback distances from the scenic corridor should increase as wall and building heights increase.

Figure 8 is illustrated of these guidelines.

Sidewalks

Sidewalks should be provided in accord with the cross-section standards of the Circulation

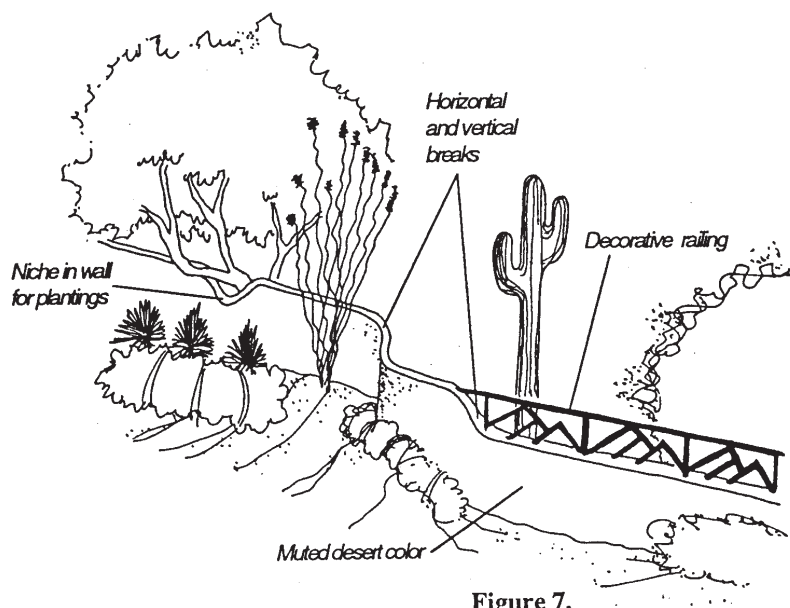


Figure 7.

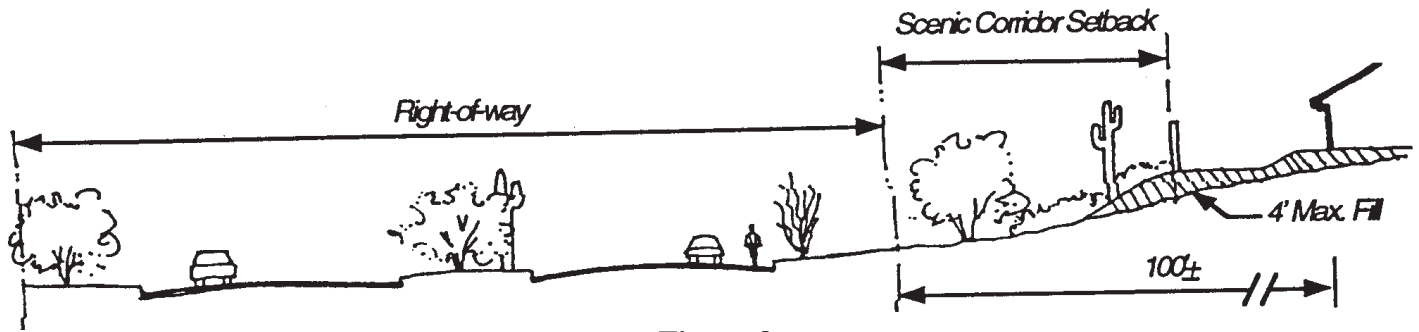


Figure 8.

Element of Scottsdale's General Plan and should be set back from the curb as far as possible with returns to the curb only at intersections and drive-ways.

Special Streetscape Treatment for Via Linda and 136th Street Intersection

The intersection of Via Linda and 136th Street will be the crossroads of the Mayo Clinic Support District, and the surrounding properties will be developed with a significant core of; medical/office/retail/hospitality uses that will be built in response to a variety of support and service demands generated by the operations of what is expected to become one of the largest out-patient clinics and medical research centers in the nation. The extensive structural development and intensive vehicular and pedestrian traffic that will occur around this intersection can be framed in an attractive, open setting that will enhance the visual experience of visitors and preserve the mountain and desert views that characterize the surrounding area. An example is shown in Figure 9 on the following page.

Maintenance

Responsibility for maintenance of landscape improvements behind the curb on both sides of Via Linda lies with the property owner or the assigned maintenance organization. The city of Scottsdale will provide maintenance for medians.

Implementation

The majority of the area affected by these guidelines is undeveloped at this time. The portions that are developed and have buildings, walls, sidewalks, roadways, and landscape in place should not be required to undergo a retrofit program to comply with these guidelines. New development projects will be required to implement these guide-

lines within their area of responsibility for the Via Linda frontage. The city of Scottsdale will be responsible for addressing the following items in addition to determining whether their inclusion in a capital improvements project is necessary:

1. Installation of median landscape
2. Entry monument signage
3. Artist designed gateway feature
4. Coordination of the selection of a paint color to used on traffic signal, street light, and power poles along the thoroughfare
5. Design and installation of special streetscape features for the Via Linda and 136th Street intersection.

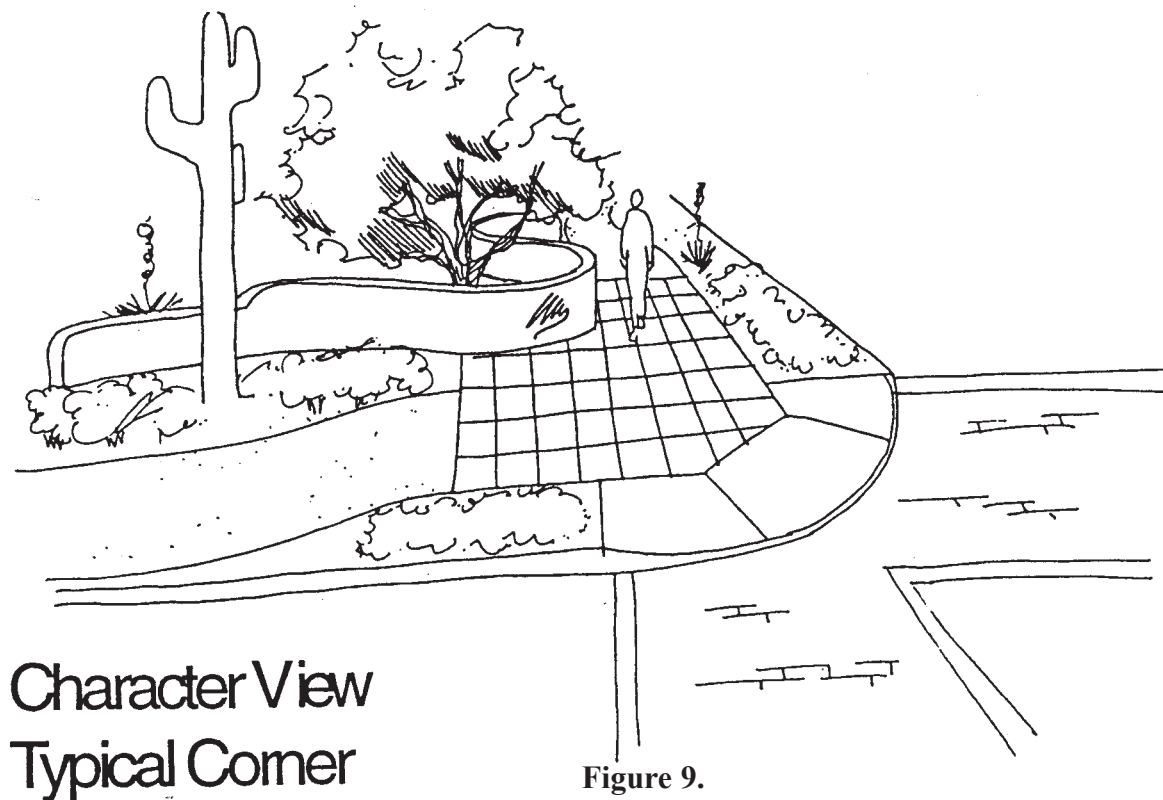
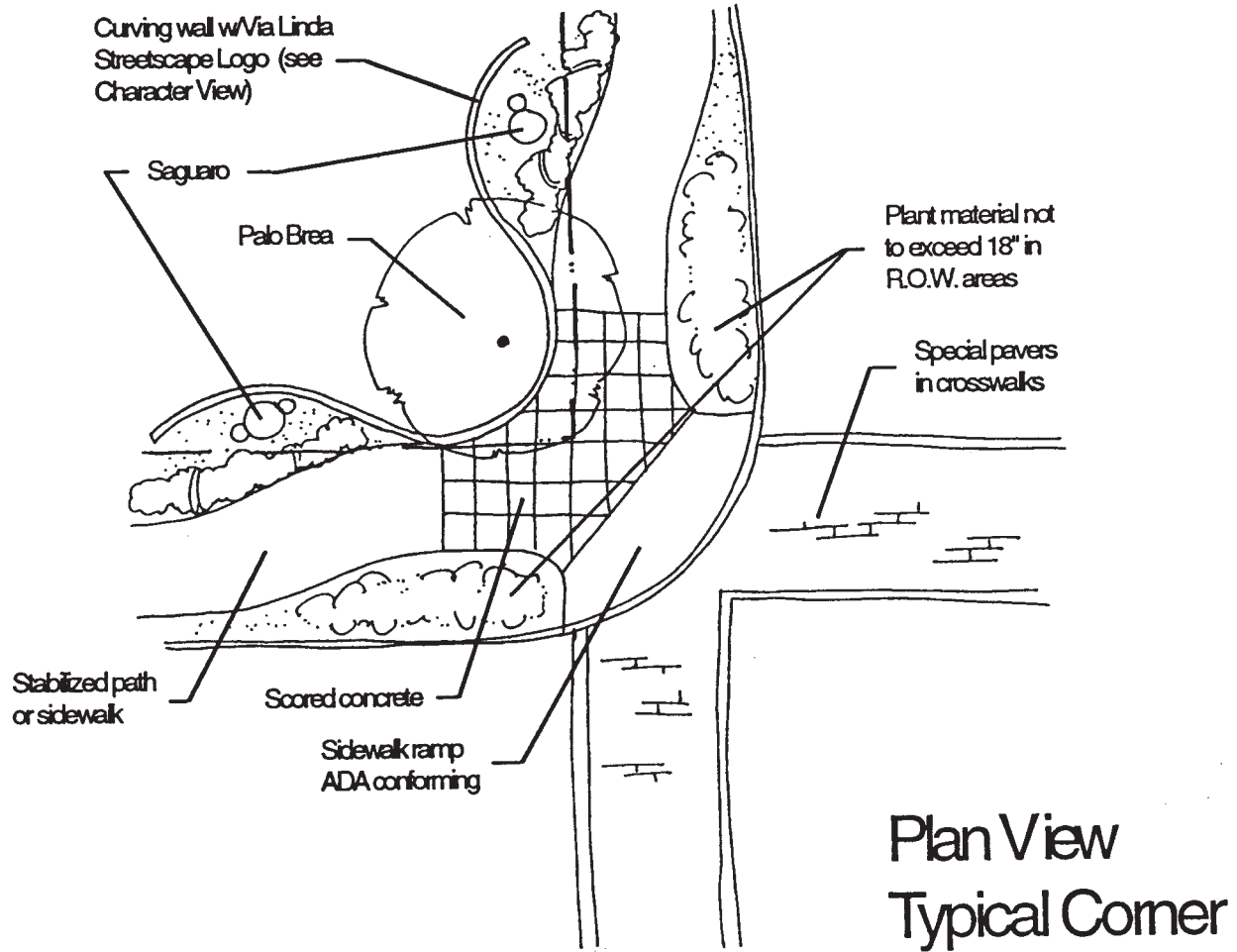


Figure 9.